

**COMMISSION ACTION FORM**

**REQUEST: MAJOR SITE DEVELOPMENT PLAN FOR 2135 COTTONWOOD ROAD**

**BACKGROUND:**

Hunziker Development Company, LLC is requesting approval of a Major Site Development Plan for a 12-unit apartment building on the property located at 2135 Cottonwood Road, (Lot 4 of Village Park Subdivision). Lot 4 includes 32,929.15 square feet (0.74 acres), and is zoned as "FS-RM" (Suburban Residential Medium Density), (See *Attachment A: Location & Zoning Map & Attachment B: Major Site Development Plan.*) The site is located west of University Boulevard, across from the Iowa State University Research Park, and south of the Wessex apartment complex. Land to the west and south of the Village Park Subdivision is outside the Ames corporate limits, in rural Story County. Attachment G shows the overall layout of the Village Park Subdivision with the open space landscaping.

A three-story, 12-unit, apartment building is proposed, including four (1-bedroom units), and eight (2-bedroom) units for a total of twenty bedrooms, with an average of 1.67 bedrooms per unit (See *Attachment E: Floor Plans*). Development of the property is required to be consistent with the rezoning Master Plan agreement for the property. The Master Plan for this property, originally identified as 3535 S. 530<sup>th</sup> Avenue (Village Park Subdivision), includes FS-RM and RH (Residential High Density). The Final Plat for Village Park Subdivision includes eleven lots for development in the FS-RM zone, and one lot for development in the "RH" (Residential High Density) zone (See *Attachment F: Village Park Subdivision Final Plat*). Approximately 100 to 120 units, and an approximate density of thirteen to seventeen units per acre is shown on the approved Master Plan (See *Attachment H: Master Plan for Village Park Subdivision*) for the FS-RM areas of the Master Plan.

The FS-RM lots are planned for a single 12-unit, three-story, apartment building on Lots 1, 2, 3 and 4, and a 12-unit, two-story apartment building on Lot 5. One 8-unit, two-story building is planned for each lot along the west perimeter of the subdivision, on Lots 6 through 11. Apartment buildings in the RH portion of the subdivision, will be located south of Cottonwood Road and east of Aurora Avenue, and are planned to include four, 36-unit buildings, three stories in height. The RH apartment buildings require City staff approval of Minor Site Development Plans for development of the entire 5.52-acre site (Lot 12).

The proposed development of 12-unit apartment buildings, along the north side of Cottonwood Road, follow a pattern and orientation that features the buildings facing the street, with surface parking along one side of the building, and behind the building in the rear yard, and garages along the north property line of each lot. The front door of the building faces Cottonwood. The proposed development on this particular lot, includes two garage buildings along the north property line, with six parking stalls in each garage building. Surface parking spaces are located along the west property line, and behind

the apartment building. The parking spaces behind the building share an access aisle with the garage parking stalls. Access to the site for parking is a shared access easement centered on the property line between Lot 4 (2135 Cottonwood Road), and Lot 5 (2209 Cottonwood Road). Notably, due to the temporary deadend status of Cottonwood, the developer is responsible for secondary emergency vehicle accessway that is to connect north through the existing Wessex apartments on Lot 3 that abuts this site to the east and there is condition for its construction related to the apartment development.

The proposed design is for 3-story buildings with gabled roofs. The individual units are accessed from an internal corridor with an exit to the front and rear of the building. Exterior building materials include primarily vinyl lap siding on the second and third stories, and in the gables facing east and west. Cultured stone veneer is limited to the first floor level, except for the south façade where the cultured stone material extends to the roofline in one area, and a point between the second and third stories in another area of the south facade. The primary entry to the building, facing the street is also covered with the cultured stone veneer. Each façade includes a cultured stone veneer element to provide design continuity and interest on all facades. Cultured stone pillars support the decks and roofs above the decks, and above the primary entry to the building. Each deck will be constructed with treated lumber decking and aluminum guardrails. Sliding glass doors will provide access to the decks and patios outside the apartment units. (*See Attachment C: Building Elevations*).

The FS-RM zone includes requirements for common open space equaling 10% of the overall FS-RM area in a development. The common open space that serves the tenants of the proposed apartment building, and all lots in the FS-RM portion of Village Park Subdivision, is located within Outlets. Outlots A and D (*See Attachment D: Final Plat for Village Park Subdivision, Attachment G: Open Space Landscape Plan*). Outlot D includes a 10-foot wide shared use path along the edge of the outlot. The shared use path for Outlot A is located in the right-of-way for Cottonwood Road. Ponds for storm water treatment and detention are located in both outlots. A 25-foot wide existing wooded buffer follows the majority of the southeast boundary of Outlot D along the eastern edge of the detention pond. The open space has essentially been design to accommodate stormwater detention. (*See Landscape Plan for Outlots A & D*). Landscape Plans for Outlots A and D will be considered by the Commission at the November 2, 2016 meeting a separate agenda item.

Landscaping is provided to meet front yard apartment foundation and parking lots screening requirements. The developer has proposed parking lot landscaping as a combination of L1 and L2 parking lots screening elements. No landscaping has been proposed or required along the north property line behind the proposed garages due to existing landscaping along the Wessex development. There is potential for an open area on the east side of the building to blend into open area of the apartment proposed to the on Lot 4 to create a relatively flat space for informal use by residents.

Off-street parking has been provided to meet the minimum parking requirements for apartment dwellings in the FS-RM zone. Access is shared with the adjoining property as required with the subdivision plat. No lighting has been proposed with the site

development plan.

Further details on the overall plans are found in the Addendum of this report.

**ALTERNATIVES:**

1. The Planning and Zoning Commission can recommend that the City Council approve the Major Site Development Plan for the proposed 12-unit apartment building, located at 2135 Cottonwood Road, as proposed, with the following condition:
  - A. That the landscaping, and site improvements, as shown on the Landscape Plan for Outlots A & D, be installed, prior to occupancy of the first FS-RM apartment building in the Village Park Subdivision;
  - B. Prior to approval of the Major Site Development Plan by the City Council, a Minor Amendment to the Planned Residence District Plan for the Wessex Apartment Development, to include the temporary emergency vehicle access, must be approved by City staff;
  - C. Prior to occupancy of the first FS-RM apartment building in the Village Park Subdivision, construction of a temporary emergency vehicle access to the Village Park Subdivision must be constructed to the specification of the Fire Department with a minimum capability of accommodating vehicles with a gross vehicle weight of up to 80,000 pounds;
  - D. Prior to consideration by the City Council, gates must be added to the Major Site Development Plan, for screening of the garbage collection areas, in order to meet the requirements of Section 29.408(3) of the *Municipal Code*.
  - E. Prior to occupancy of the apartment building at 2135 Cottonwood Road, shrubs used as screening for mechanical units must be installed to meet zoning standards at a minimum height that is six inches higher (at the time of planting) than the mechanical equipment the shrubs are screening.
  - F. Compliance of all exterior lighting, with the Outdoor Lighting Code, Section 29.411 of the *Municipal Code*, and approval by the Planning & Housing Department, is required prior to occupancy.
  - G. Six additional shrubs are required to meet the landscaping requirements for the front yard of apartment buildings in the FS-RM zone.
2. The Planning and Zoning Commission can recommend that the City Council approve the Major Site Development Plan for the proposed 12-unit apartment building, located at 2135 Cottonwood Road, with modifications.
3. The Planning and Zoning Commission can recommend that the City Council deny the Major Site Development Plan for the proposed 12-unit apartment building, if it finds the project does not meet the Major site Development Plan criteria.
4. Action on this request can be postponed and referred back to City staff and/or the applicant for additional information.

## **RECOMMENDED ACTION:**

The Major Site Development Plan review is to determine conformance with development standards, and appropriate arrangement and design of the use of the site. FS-RM zoning is intended to implement the LUPP vision of landscaped suburban style development that provides for desirable apartment housing choices. The proposed development project is consistent with the Master Plan for use limitations and meets a community interest for providing apartment types of various floor plans from one to two bedrooms that will have wide appeal. The specific criteria for site development plan approval are discussed in greater detail in the addendum.

The proposed building is one of multiple small apartment building sites that will be developed in the Village Park subdivision as separate sites. At the time the Master Plan and subdivision were approved the primary discussion was on how to situate the smaller apartment buildings around the perimeter of the site and to buffer the central RH apartment building site from the homeowners to along University Boulevard to the east. The other major issues were the provision of open space and the extension of the trail to the south and the configuration of Cottonwood as a street that will eventually extend west and connect to the Ringennberg Subdivision as residential collector street. The intent for development of the area was to provide for apartments design to be desirable to working professionals and ideally to support employment in the ISU Research Park that is immediately east of the area.

The proposed building design is consistent with the common expectations of apartment homes marketed above an entry level product in that it includes some covered parking and private spaces for individual and the units are sized for smaller living situations. As a small site there is not unique amenities or characteristics to the project that have been included with recent FS-RM approvals on Mortenson or commitments for amenities on larger RH sites on South Duff.

The architectural appearance is similar to other designs that have been developed in Ames and Ankeny. The building design has a traditional apartment aesthetic in terms of exterior materials and architectural features. The overall massing of the proposed apartment buildings in this location is significantly smaller than the 36-unit apartment buildings proposed across the street in the RH zone portion of Village Park, and also smaller in scale than the 24-unit and 36-unit apartment buildings, close to this site, in the Wessex development to the north. Staff has worked with the applicant to provide landscaping on-site to meet front yard, and perimeter parking lot landscaping requirements. The arrangement of the site has a residential appearance and with the required street trees and apartment foundation plantings the development of all the sites along Cottonwood will create a substantially landscaped corridor.

**Therefore, it is the recommendation of the Department of Planning and Housing that the Planning and Zoning Commission act in accordance with Alternative #1, recommending that the City Council approve the Major Site Development Plan with the conditions listed.**



## **ADDENDUM**

**Project Description.** Hunziker Development Company, LLC is requesting approval of a Major Site Development Plan for a 12-unit apartment building on the property located at 2135 Cottonwood Road, (Lot 4 of Village Park Subdivision). Lot 4 includes 32,329.15 square feet (0.74 acres), is zoned as “FS-RM” (Suburban Residential Medium Density), and will have a public sidewalk (5 feet wide) for the entire frontage of the lot along Cottonwood Road (*See Attachment A: Location & Zoning Map & Attachment F: Village Park Subdivision Final Plat*).

Density limitations in the FS-RM zone requires 7,000 square feet for the first two units, and 1,800 square feet for each additional unit. This lot could accommodate 16 dwelling units, provided all other site plan requirements are met. The FS-RM zone allows a maximum of 12 units in each building, which is what is proposed for the entire lot. The proposed density of 12 units on 0.74 acres is the equivalent of 16 dwelling units per acre, which is within the density range on the approved Master Plan for Village Park Subdivision (*See Attachment H: Village Park Subdivision Master Plan*). The proposed 12-unit building consists of four (1-bedroom units), and eight (2-bedroom units), for a total of twenty bedrooms in the entire building.

### **Parking.**

The Zoning regulations require a minimum of one parking space per bedroom for units of two bedrooms, or more, and one and one half parking spaces for one-bedroom units. The minimum number of parking spaces required for this site is twenty-two spaces. The developer is proposing a total of 33 parking spaces, including handicap-accessible spaces and garage stalls. Off-street parking on the site is configured as five standard (9'x19') surface parking spaces in the side yard, west of the building, plus two van-accessible handicap spaces, as required, and fourteen standard surface parking spaces, in the rear yard behind the building. Across the drive aisle from the surface parking spaces in the rear yard, are two (6-stall) garages approximately 11.58 feet from the north property line, and running parallel to the north perimeter of the site. The parking spaces, and garage stalls, are served by a single access from Cottonwood Road, centered on the property line that separates Lots 4 and 5 in a 25-foot wide access easement. The shared access between Lots 4 and 5, must be paved at a minimum width of 24 feet, to serve Lot 4, even though development of Lot 5 is not proposed at this time. In addition, the required perimeter parking lot landscaping along the west edge of the drive aisle on Lot 5, must be included on the Landscape Plan to meet minimum landscape requirements (*See Attachment B: Master Plan, Parking Layout & Landscaping Sheets*). At the time Lot 5 is developed, the landscaping along the drive aisle may be modified on Lot 5.

### **Layout.**

The building footprint of the apartment building occupies 7,900 square feet of the site. Garages footprints cover 3,168 square feet, and the parking spaces, driveways, drive aisles, and sidewalks cover an additional 8,329.49 square feet for a total of 19,397.49 square feet of the site covered by impervious surfaces, which is 60% of the total site area. The remainder of the lot is landscaped area, which amounts to 40% of the total site.

In addition, the minimum requirement of 10% of the gross area of the subdivision to be devoted to common open space, for development in the FS-RL, or FS-RM zones, is accounted for in Outlots A and D, of Village Park Subdivision. The Village Park Subdivision includes 19.83 acres (863,794.80 square feet). The land area included in Outlots A and D, combined, is equal to 151, 038.74 square feet of land area, which is equal to 17.48% of the total land area in Village Park Subdivision (*See Attachment G: Landscape Plan for Outlots A & D, Village Park Subdivision*). Outlots A and D are located in close proximity to the proposed apartment buildings, and the other future apartment buildings to be constructed in Village Park. The outlots have been designed as stormwater detention and treatment areas and include one 10-foot trail extension. Review of the outlot landscape plan is separate from this project. There is no other common usable space included within the project.

### **Landscaping.**

Landscaping requirements are driven by parking lot design and front yard plantings for apartment buildings (Page 18 of the report is the landscape plan). The proposed project's landscaping is most affected by the front yard foundation plantings due to the length of street frontage and the location of the parking. Due to the placement of garages along the north boundary line, screening is not required. The 10-foot area behind the garages is planned to be planted as turf. Screening would be needed if the garages did not abut an existing landscaped yard of the abutting Wessex apartments to meet the intent of the FS standards.

Front yard foundation plantings requirements include a combination of shrubs, understory and overstory trees. The developer has included additional cluster landscaping along the sides of building to add interest and some screening for the residents from the parking lot. Staff has an interest that an open relatively flat area be created between the apartment buildings of Lot 4 and Lot 3 to allow for informal open space. The area to the east of the building is planned as turf and can be graded with minimal slope to meet this interest of having available space for recreational use. There will be approximately 70 feet between buildings and 6000 square feet of informal open space between the buildings on Lots 3 and 4.

The Landscape Plan shows screening for mechanical units along the east and west sides of the building that meet site plan standards, provided the shrubs used for screening are a minimum of six inches higher than the mechanical units, at the time of planting. A condition is included in the report as a reminder of the need to comply with screening at the time of planting compared to growing into a screen over time.

No interior parking lot landscaping is required due to the size of the parking lot with one drive aisle. Perimeter parking lot landscaping is required along the portions of the driveways, parking spaces, and drive aisles to meet perimeter parking lot landscaping. All but one area of the site has the required perimeter parking lot landscaping. The side of the parking space that faces the property at 2121 Cottonwood, to the east, requires five shrubs to meet the "L2"(Low Screen) landscaping standards. The developer does not believe landscaping is needed at the terminus of this drive aisle as it abuts another drive aisle. The City Council can approve modification of landscaping screening

requirements if it finds that an alternative design meets the objectives for parking lot screening. Staff finds that the minimal length of area could be planted, but the overall mix of plants has diversity and interest in the design while meeting the essence of screening objectives of the zoning ordinance (See *Attachment B: Major Site Development Plan, Landscaping- pg 18*).

The trash receptacles are screened with 1" by 6" vertical cedar wood alternating slats to a height of six feet. The site plan does not include gates for the enclosure. Gates are necessary to meet screening requirements in Section 29.408(3) of the Municipal Code, which requires that garbage collection areas be screened from all public rights-of-way and from adjacent properties.

### **Circulation.**

Site access is from one driveway off Cottonwood Road. Cottonwood Road is planned to be extended west to connect to the existing segment of Cottonwood Road, in the Suncrest Subdivision, once the land between Village Park Subdivision and Suncrest Subdivision is annexed into the City and developed as residential. Public sidewalks (5-foot wide) in the right-of-way for Cottonwood Road provide a connection to the shared use path (10-foot wide) that will cross through the site between University Boulevard and the southern boundary of Village Park Subdivision. The shared use path will connect to the Ames community bike trail/shared use path system, and is planned in the future to connect to the regional multi-county bike trail system south of Ames (See *Attachment G: Landscape Plan for Outlots A & D, Village Park Subdivision*). Christofferson Park is situated northwest of the site and will at the time of development of Lot 2 will include a walkway connection to the south boundary of the park.

### **Building Design.**

The overall footprint of the proposed apartment building is approximately 64 feet by 69 feet. Minimum building front, side and rear yard setbacks are all met by the proposed placement of the apartment building on the lot. The detached garage structures are allowed to be as close as 3 feet to the rear property line. The site plan shows the garage structures 10.91 feet from the north property line (See *Attachment B: Major Site Development Plan*).

The 12-unit building is consistent with the maximum building size allowed in the FS-RM zoning district. The building is three stories in height. Four stories, or 50 feet, whichever is lower, is the maximum building height permitted for multiple-family dwellings in the FS-RM zone. Ten of the twelve apartment units have access to either a small patio or a balcony. The 2-bedroom units, on the second and third floors of the northeast corner of the building, do not have balconies. The apartment building design incorporates a 5/12 pitched gable roof design, and asphalt shingles.

Two primary exterior materials are proposed for the building. The majority of the first floor is to be surfaced with cultured stone, while the second and third floors, and the gables are primarily vinyl lap siding. Cultured stone will extend to the roof line on approximately one-third of the south façade, and to the top of the first floor level at the primary entrance to the building. The primary (south) building entrance will include a 5/12 gabled roof above the entrance, as will also be a design feature above each of the

decks on the second and third levels of the building. Decks will be constructed of treated lumber with aluminum guardrails, and supported by pillars of cultured stone. The two (6-stall) garage structures will have similar materials and design features with a band of cultured stoned at the bottom of each façade, vinyl lap siding on the remainder, metal overhead garage doors, and a gabled roof with asphalt shingles and a roof pitch of 6/12. The cultured stone is a dark brown color, and the vinyl lap siding is a lighter brown color with two different variations of tan shading. Each façade has some variation in wall plane to add interest and provide some relief in the architecture (See *Attachment C: Building Elevations & Attachment D: Garage Elevations*).

**Major Site Development Plan Criteria.** Additional criteria and standards for review of all Major Site Development Plans are found in Ames *Municipal Code* Section 29.1502(4)(d) and include the following requirements.

***When acting upon an application for a Major Site Development Plan approval, the Planning and Zoning Commission and the City Council shall rely upon generally accepted site planning criteria and design standards. These criteria and standards are necessary to fulfill the intent of the Zoning Ordinance, the Land Use Policy Plan, and are the minimum necessary to safeguard the public health, safety, aesthetics, and general welfare.***

- 1. The design of the proposed development shall make adequate provisions for surface and subsurface drainage to limit the rate of increased runoff of surface water to adjacent and downstream property.***

The Public Works Department has reviewed the storm water management plan and finds that the proposed development has met the required storm water quantity and quality measures by use of the proposed regional detention facilities on the southeast and northeast areas of the site.

- 2. The design of the proposed development shall make adequate provision for connection to water, sanitary sewer, electrical, and other utility lines within the capacity limits of those utility lines.***

The existing utilities were reviewed and found adequate to support the anticipated load of 12 dwelling units on this lot, comprising 20 bedrooms, consistent with prior determination at the time of rezoning and subdivision approval.

- 3. The design of the proposed development shall make adequate provision for fire protection through building placement, acceptable location of flammable materials, and other measures to ensure fire safety.***

The fire inspector has reviewed access and fire truck circulation, sprinkler, and hydrant requirements and found that the requirements of the Fire Department are met. The subject site is also conditioned on provision of emergency access through Wessex to the north.

- 4. The design of the proposed development shall not increase the danger of***

***erosion, flooding, landslide, or other endangerment to adjoining and surrounding property.***

It is not anticipated that this proposed development will be a danger due to its site location and proximity to other uses.

***5. Natural topographic and landscape features of the site shall be incorporated into the development design.***

The site is currently devoid of any significant vegetation. Minimal grading will occur for the construction of the buildings. The site is relatively flat and grading will occur mostly to direct storm water where required.

***6. The design of the interior vehicle and pedestrian circulation shall provide for convenient flow of vehicles and movement of pedestrians and shall prevent hazards to adjacent streets or property.***

Access to this site from Cottonwood Road is through one access point at the southwest corner of the property. Vehicular and pedestrian access is accommodated between the subject property, and other properties (buildings) within the Village Park subdivision. The on-site sidewalks, sidewalks in the public street right-of-ways, and shared use paths provide pathways throughout the site, and to external connections to allow for circulation throughout the community, and eventually within the Central Iowa region.

***7. The design of outdoor parking areas, storage yards, trash and dumpster areas, and other exterior features shall be adequately landscaped or screened to minimize potential nuisance and impairment to the use of adjoining property.***

The general development standards of the zoning ordinance have been met for the screening of parking areas. Gates are needed on the trash enclosure to comply with the zoning ordinance standards for a fenced enclosure of garbage collection areas. The enclosure standard requires full fenced screening with a gate to ensure the compliance with the standards. In this plan the design has a large opening for rollout of the dumpster with no gate to obscure or secure the dumpster. The apartment building provides a high degree of separation and screening of the parking and garages in the rear yard from adjoining views, and as viewed from Cottonwood Road.

***8. The proposed development shall limit entrances and exits upon adjacent streets in order to prevent congestion on adjacent and surrounding streets and in order to provide for safe and orderly vehicle movement.***

A shared access is planned for the subject property (Lot 5 and Lot 4), to minimize the number of curb cuts onto Cottonwood Road.

***9. Exterior lighting shall relate to the scale and location of the development in order to maintain adequate security, while preventing a nuisance or hardship to adjacent property or streets.***

Additional information is needed on the proposed exterior lighting, to determine compliance, prior to the occupancy of the apartment building. The proposed lighting must meet the lighting standards, found in Sec 29.411 of the *Municipal Code*.

***10. The proposed development shall ensure that dust and other forms of air pollution, noise disturbances, odor, glare, and other nuisances will be limited to acceptable levels as prescribed in other applicable State and City regulations.***

The proposed residential uses are not expected to exceed the level of nuisances, typical of this type of development, beyond acceptable levels, as prescribed in other applicable State and City regulations.

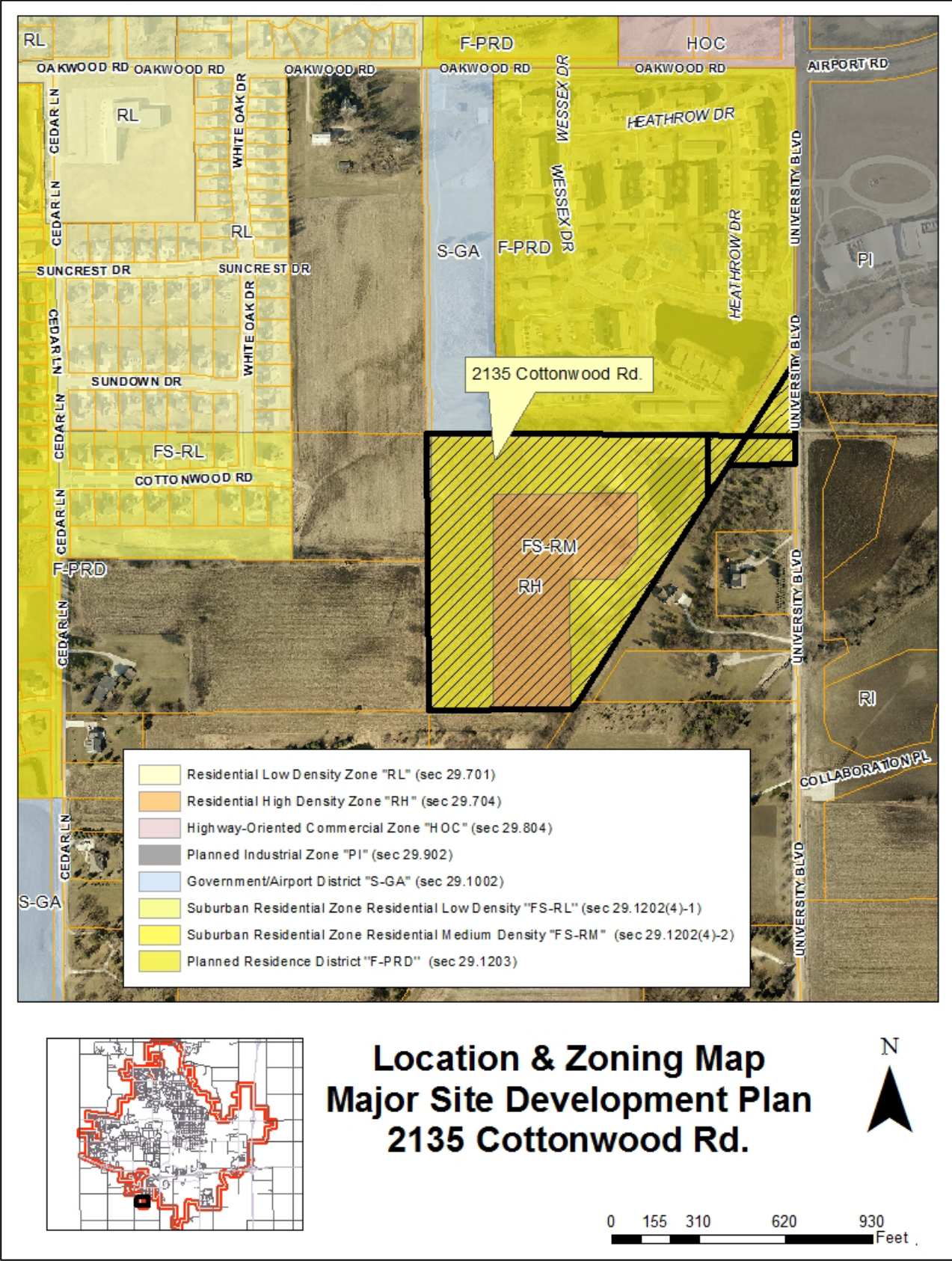
***11. Site coverage, building scale, setbacks, and open spaces shall be in proportion with the development property and with existing and planned development and structures, in adjacent and surrounding property.***

The scale of the proposed apartment building, in relation to the lot size, is appropriate, given minimum building setback requirements, landscaping requirements, and areas of the site planned for parking. The developer has proposed the maximum number of units (12) in a multiple family structure within the FS-RM zoning district. The density of development is well within the approximate densities found on the approved Master Plan for this subdivision, and the apartment appearance is compatible with the character and scale of the general surrounding with the proposed 36-unit apartment dwellings on the other side of Cottonwood Road, and the 24-unit and 36-unit buildings in the Wessex development adjacent to the north. Open areas and landscaped areas meet the quantitative standards of the zoning code and allow for informal activities by the future residents of this subdivision.

The Development Review Committee has reviewed the Major Site Development Plan and found that it complies with all other requirements of the Ames Municipal Code.



Attachment A: Location & Zoning Map

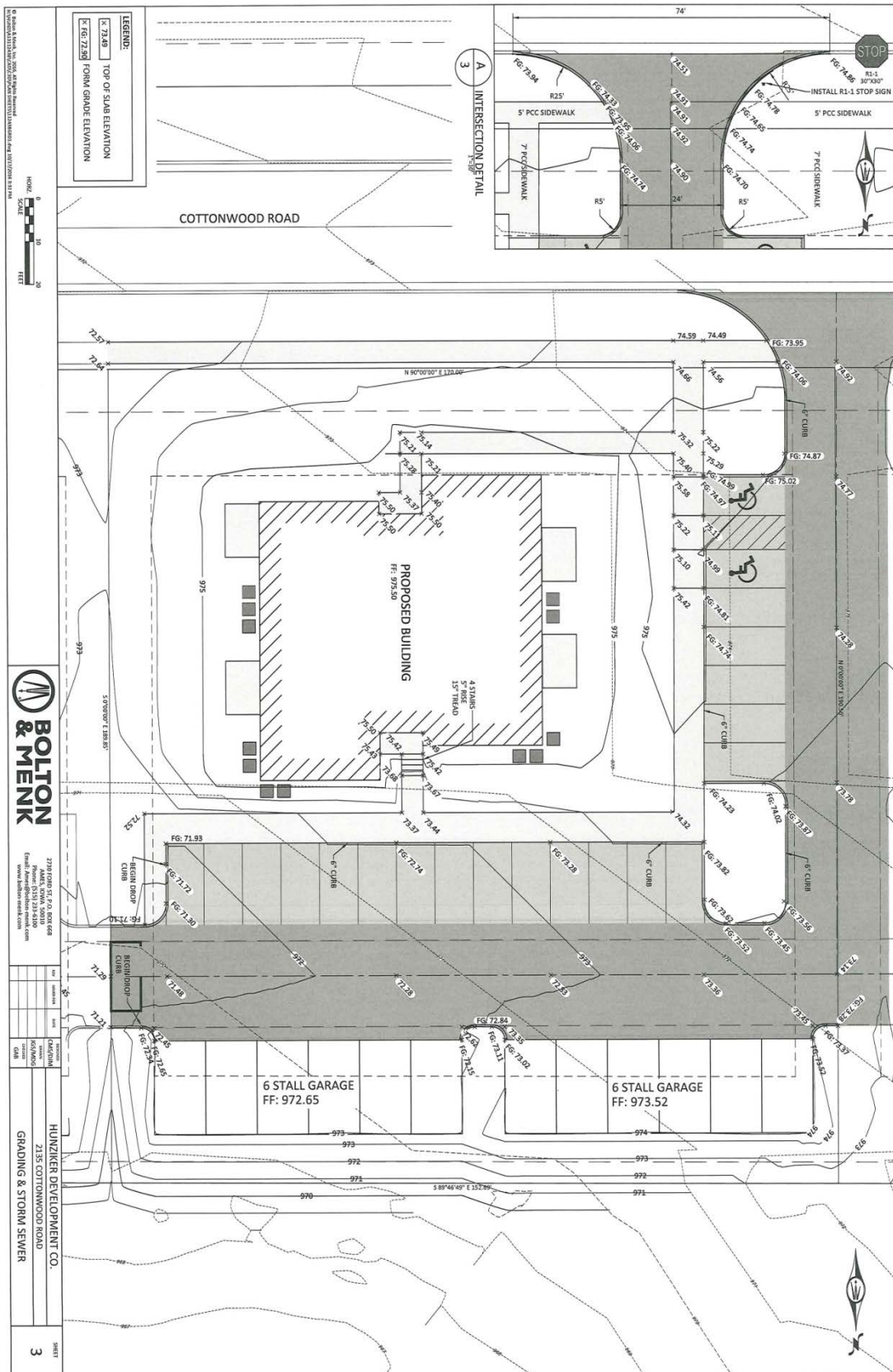








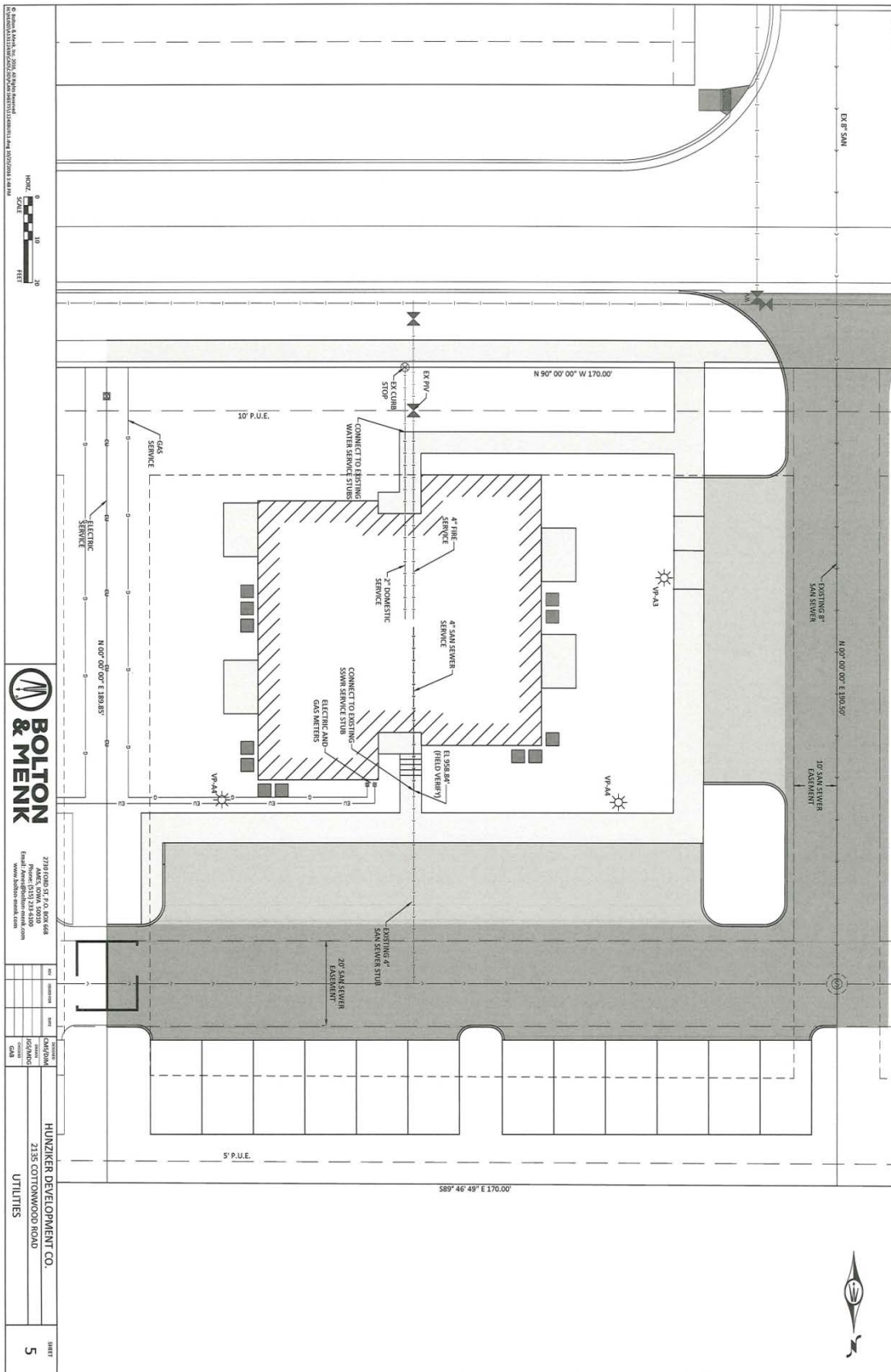
# Attachment B: Major Site Development Plan (Grading & Storm Sewer)



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## Attachment B: Major Site Development Plan (Utilities)







**1 EAST ELEVATION**  
SCALE: 1/8" = 1'-0"

**2 WEST ELEVATION**  
SCALE: 1/8" = 1'-0"

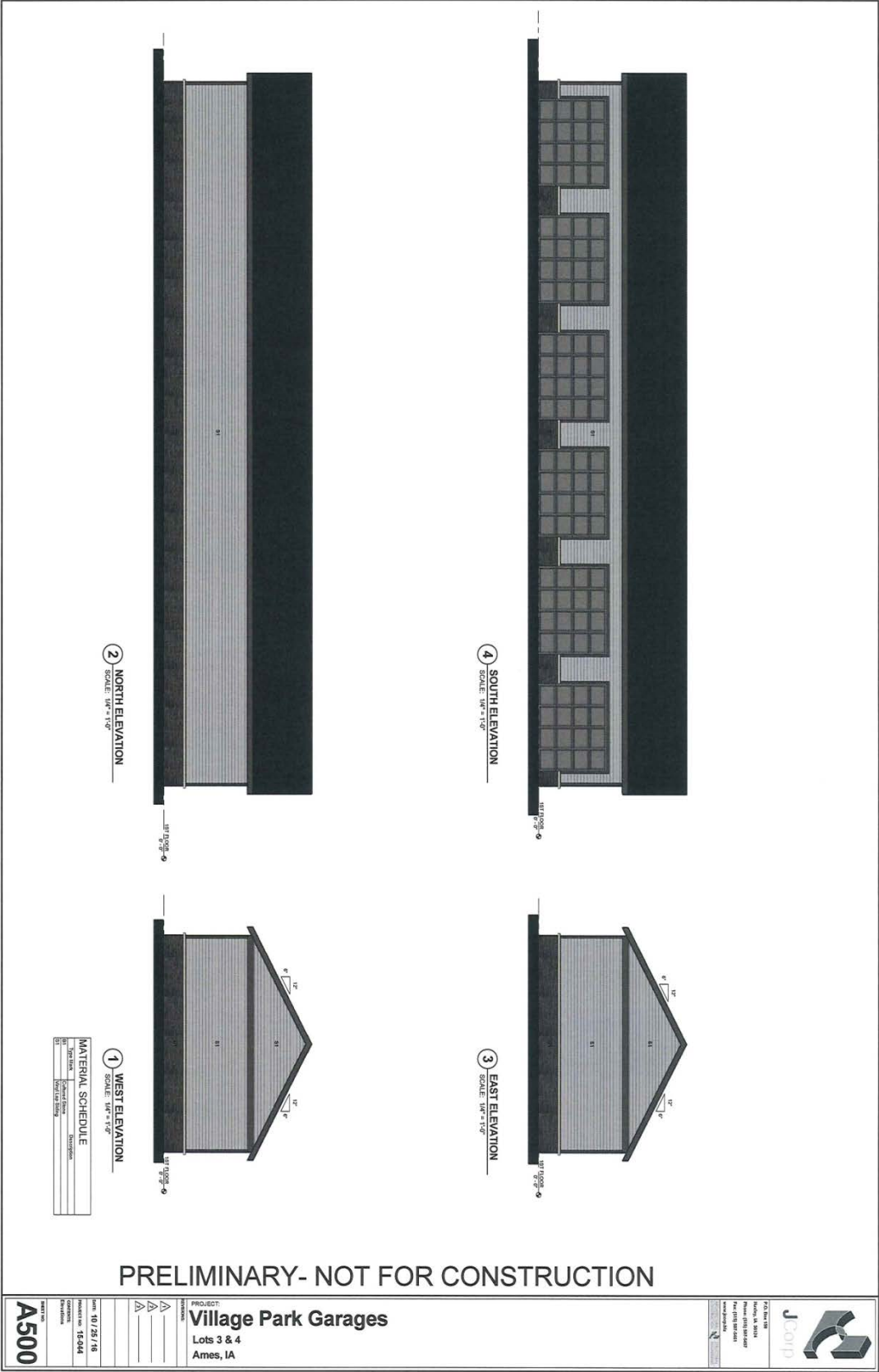
**3 SOUTH ELEVATION**  
SCALE: 1/8" = 1'-0"

**4 NORTH ELEVATION**  
SCALE: 1/8" = 1'-0"

**MATERIAL SCHEDULE**

ITEM	QUANTITY	UNIT	PRICE	TOTAL
1. Siding	1000	SQ. FT.	1.50	1500.00
2. Windows	10	EA.	100.00	1000.00
3. Doors	1	EA.	500.00	500.00
4. Trim	100	LF.	1.00	100.00
5. Roofing	1000	SQ. FT.	2.00	2000.00
6. Foundation	100	LF.	1.00	100.00
7. Landscaping	100	SQ. FT.	1.00	100.00
8. Fencing	100	LF.	1.00	100.00
9. Driveway	100	SQ. FT.	1.00	100.00
10. Walkways	100	SQ. FT.	1.00	100.00
11. Stairs	10	EA.	100.00	1000.00
12. Porch	100	SQ. FT.	1.00	100.00
13. Deck	100	SQ. FT.	1.00	100.00
14. Garage	100	SQ. FT.	1.00	100.00
15. Pool	100	SQ. FT.	1.00	100.00
16. Hot Tub	1	EA.	500.00	500.00
17. Fences	100	LF.	1.00	100.00
18. Gates	10	EA.	100.00	1000.00
19. Driveways	100	SQ. FT.	1.00	100.00
20. Walkways	100	SQ. FT.	1.00	100.00
21. Stairs	10	EA.	100.00	1000.00
22. Porch	100	SQ. FT.	1.00	100.00
23. Deck	100	SQ. FT.	1.00	100.00
24. Garage	100	SQ. FT.	1.00	100.00
25. Pool	100	SQ. FT.	1.00	100.00
26. Hot Tub	1	EA.	500.00	500.00
27. Fences	100	LF.	1.00	100.00
28. Gates	10	EA.	100.00	1000.00
29. Driveways	100	SQ. FT.	1.00	100.00
30. Walkways	100	SQ. FT.	1.00	100.00
31. Stairs	10	EA.	100.00	1000.00
32. Porch	100	SQ. FT.	1.00	100.00
33. Deck	100	SQ. FT.	1.00	100.00
34. Garage	100	SQ. FT.	1.00	100.00
35. Pool	100	SQ. FT.	1.00	100.00
36. Hot Tub	1	EA.	500.00	500.00
37. Fences	100	LF.	1.00	100.00
38. Gates	10	EA.	100.00	1000.00
39. Driveways	100	SQ. FT.	1.00	100.00
40. Walkways	100	SQ. FT.	1.00	100.00
41. Stairs	10	EA.	100.00	1000.00
42. Porch	100	SQ. FT.	1.00	100.00
43. Deck	100	SQ. FT.	1.00	100.00
44. Garage	100	SQ. FT.	1.00	100.00
45. Pool	100	SQ. FT.	1.00	100.00
46. Hot Tub	1	EA.	500.00	500.00
47. Fences	100	LF.	1.00	100.00
48. Gates	10	EA.	100.00	1000.00
49. Driveways	100	SQ. FT.	1.00	100.00
50. Walkways	100	SQ. FT.	1.00	100.00
51. Stairs	10	EA.	100.00	1000.00
52. Porch	100	SQ. FT.	1.00	100.00
53. Deck	100	SQ. FT.	1.00	100.00
54. Garage	100	SQ. FT.	1.00	100.00
55. Pool	100	SQ. FT.	1.00	100.00
56. Hot Tub	1	EA.	500.00	500.00
57. Fences	100	LF.	1.00	100.00
58. Gates	10	EA.	100.00	1000.00
59. Driveways	100	SQ. FT.	1.00	100.00
60. Walkways	100	SQ. FT.	1.00	100.00
61. Stairs	10	EA.	100.00	1000.00
62. Porch	100	SQ. FT.	1.00	100.00
63. Deck	100	SQ. FT.	1.00	100.00
64. Garage	100	SQ. FT.	1.00	100.00
65. Pool	100	SQ. FT.	1.00	100.00
66. Hot Tub	1			

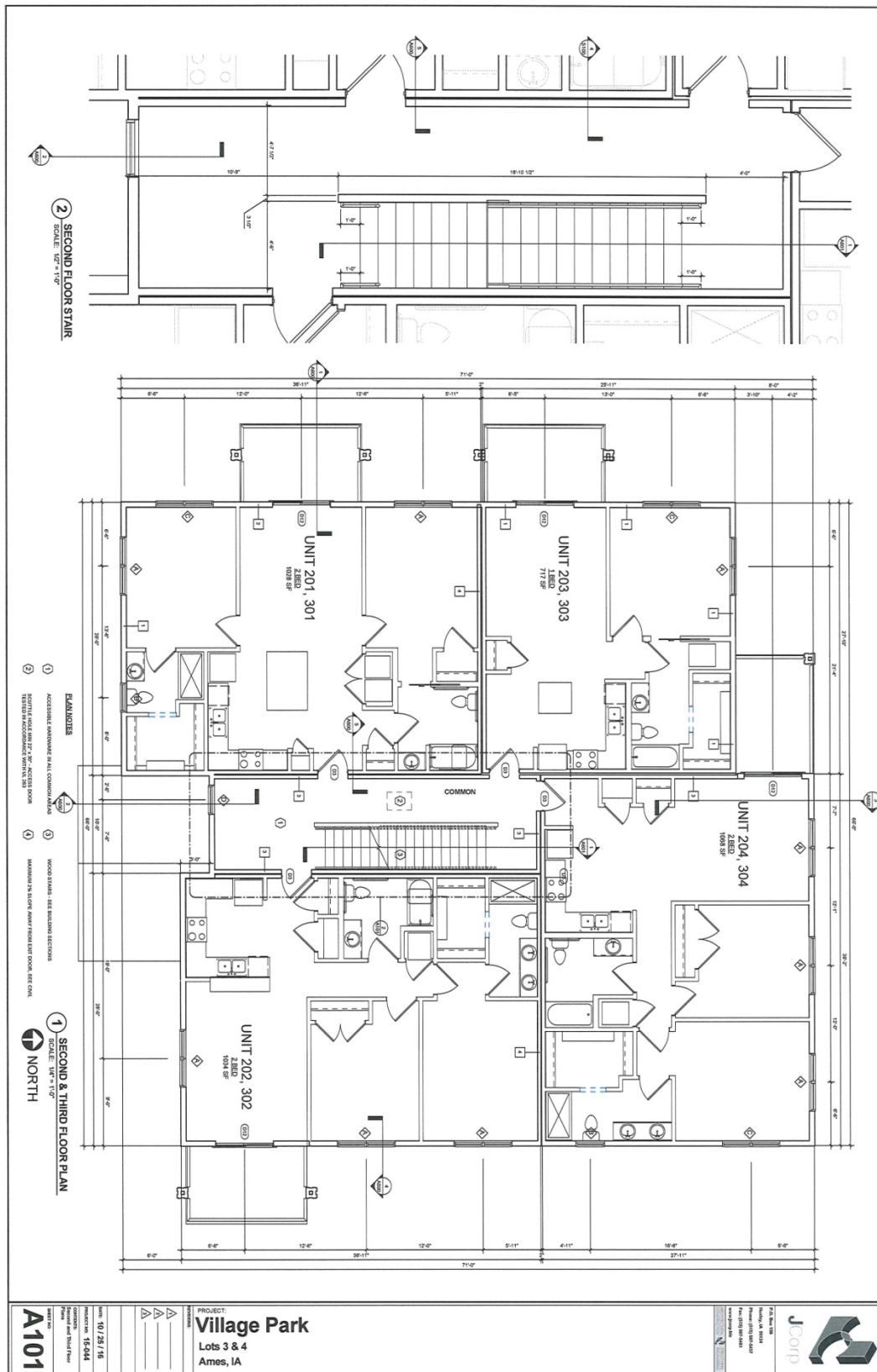
Attachment D: Garage Elevations





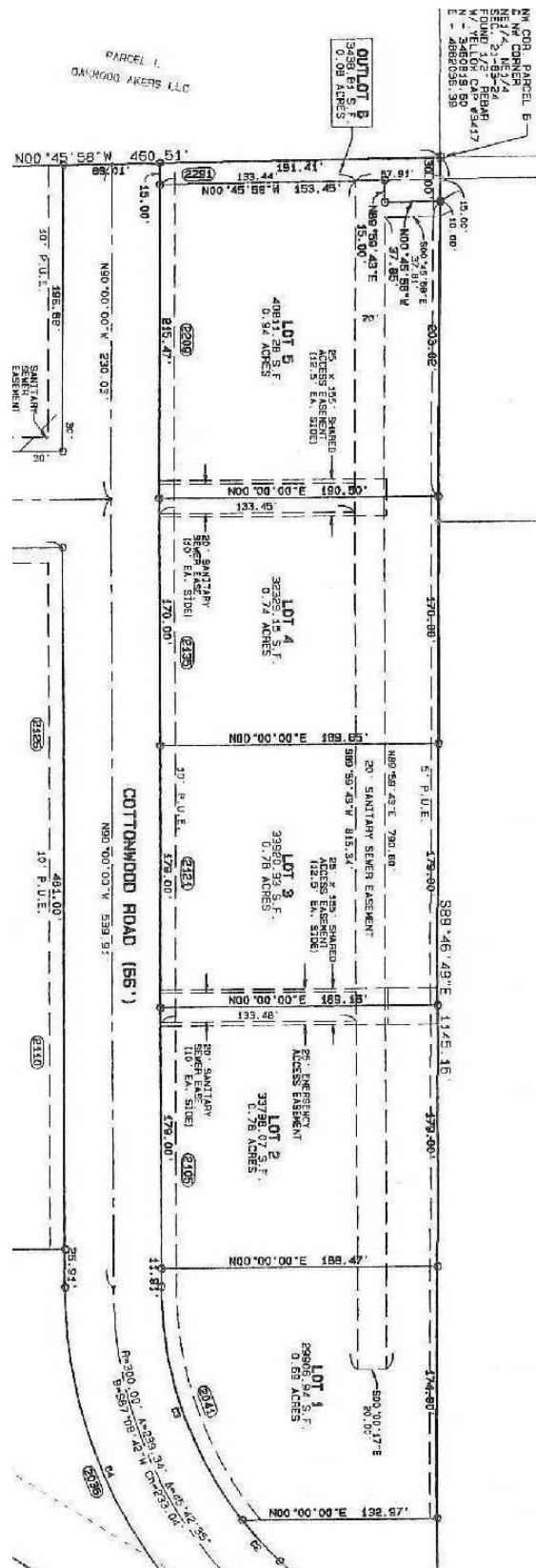


## Attachment E: Floor Plans (Second & Third Floor)



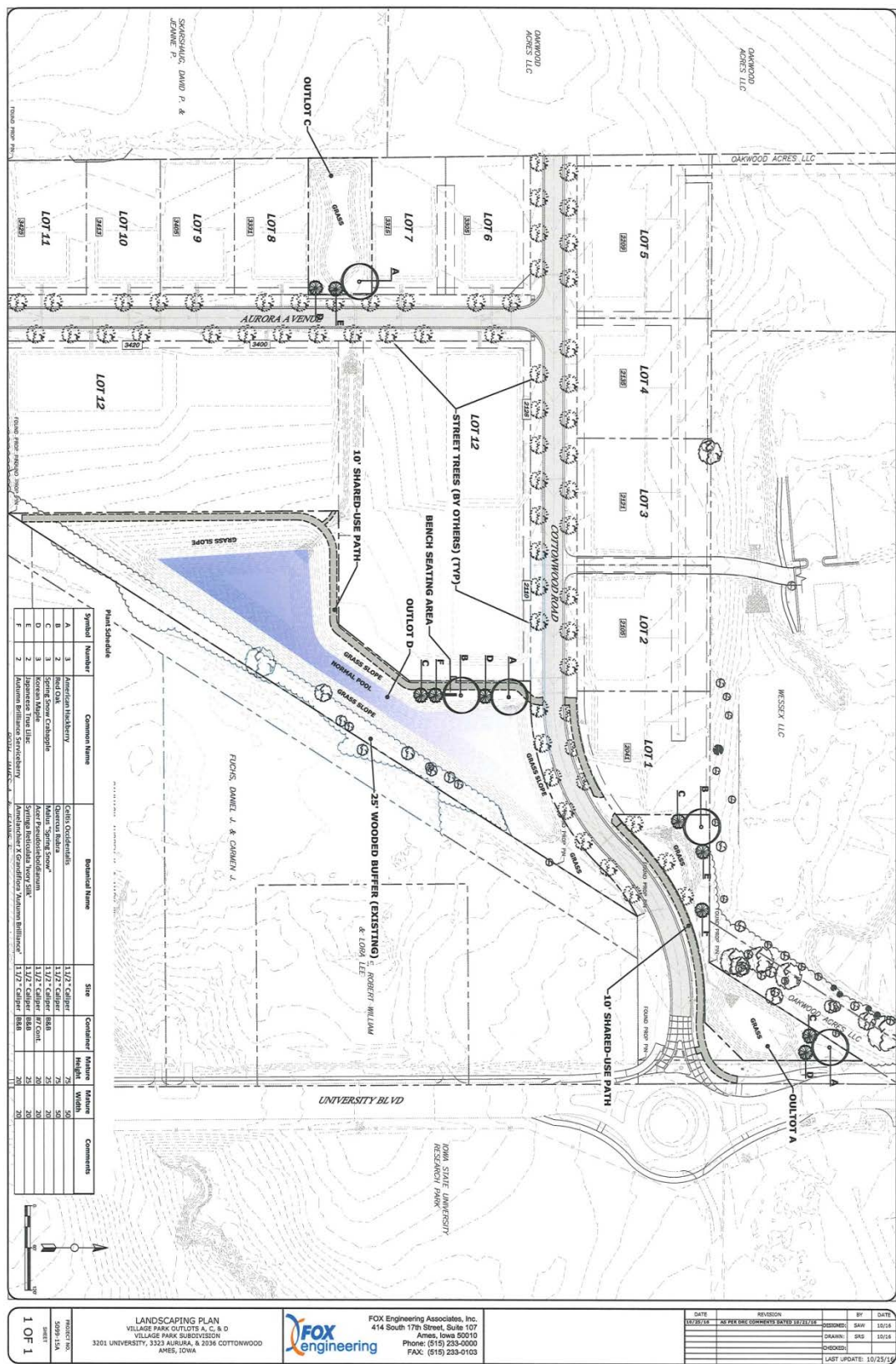
[illegible]

# Attachment F: Village Park Subdivision (Lot 1, 2, 3, 4 & 5)





Attachment G: Landscape Plan for Outlots A & D, Village Park Subdivision



Attachment H: Master Plan for Village Park Subdivision

