

**COUNCIL ACTION FORM**

**SUBJECT: AMENDMENT TO MAJOR SITE DEVELOPMENT PLAN FOR THE BRICKTOWNE PLANNED RESIDENCE DISTRICT (F-PRD) ZONE AT 400 & 500 W CRYSTAL LOOP**

**BACKGROUND:**

The property owner, Bricktowne LLC, requests a major amendment to the approved Bricktowne Residence District (See *Attachment A – Location Map*.) The amendment affects 9.74 acres within the overall 40 acre Planned Residence District (F-PRD). **The request is to approve a change in the layout on Lots 9 and 10 located at the west side of the site nearest the airport. The proposed amendment lowers the total number of units on the site, reconfigures the approved multi-family buildings, and changes the bedroom count for the dwellings.** The overall layout is similar to the original 2017 approved plan.

The Bricktowne F-PRD was approved by City Council on July 5, 2017 (Attachment B-Layout). There have been subsequent Minor Amendments to the PRD that were approved by staff. As the developer built out the project, they desired to make amendments to the building configurations. Amendments in 2019 and 2021, removed residential structures on Lots 9 and 10 in order to allow changes to other areas of the development to be processed as Minor Amendments. As a result of the prior minor amendment changes, a major amendment is now needed to return to having residential units to Lots 9 and 10 to allow for the overall increase intensity of development as compared to original approval.

The 2017 Planned Residential Development Site Plan was approved with a total of 572 dwelling units, consisting of 416 one-bedroom units, 136 two-bedroom units, and 20 three-bedroom units. Lots 1 through 8 provided parking throughout the development in surface lots and detached garage structures. The majority of the units were configured as part of traditional apartment buildings with 68 townhouse style units as well, meaning units that do not have other dwellings above or below them. Eight of these units were located near the clubhouse on Lot 8. The other 60 units were approved on Lots 9 and 10 among 10 separate buildings, the subject area of the amendment.

On Lots 9 and 10 there were originally 60 units total, to be constructed among ten buildings with six attached units per building. The original buildings were a mix of one and two-story units. These units were a mix of two and three bedrooms providing two parking stalls in the adjacent driveway and attached garage (See Attachment B – 2017 PRD Site Plan) as well as with some private-street parking.

In 2019, a Minor Amendment was approved in order for the developer to make changes to Lot 8. This amendment kept the number of townhouse style units at the approved eight. However, floor plan changes were made that increased the number of bedrooms by 2, changed the footprint from two buildings of 4 units to one building with eight units, and removed a parking garage building.

Section 29.1502 (6) defines what changes constitute a major versus minor amendment to a an approved site plan. An increase in the number of bedrooms is an increase in intensity, which categorizes changes to unit configurations as a Major Amendment rather than a Minor Amendment. The developer previously removed units to in the project to ensure there was not an increase in intensity so that they were minor.

In order to move the initial 2019 amendment through as a minor amendment, the developer opted to remove one building from the site plan on Lot 9 to ensure there was no net increase in the number of total bedrooms throughout the development. At the time of this approval, the total number of units decreased to 558, consisting of 416 one-bedroom units, 118 two-bedroom units, 24 three-bedroom units across the development, and lowered the number of townhouse style units from 68 to 62.

The Minor Amendment in 2021 was similar in that a townhouse style building was removed from Lot 10 to allow for changes to the bedroom counts in apartment buildings located on Lots 5 and 8. Initially, these two building were approved with 60 units in each, consisting of 49 one-bedroom units and 11 two-bedroom units in each building. The minor amendment approved changes to each of these buildings that decrease the total number of units in each building from 60 to 48, but increased the number of bedrooms, consisting of 37 two-bedrooms, and 11 one-bedroom units in each building. At the time of this approval, the total number of units was 568 consisting of 340 one-bedroom units, 148 two-bedroom units, 24 three-bedroom units across the development, and lowered the number of townhouse style units from 62 to 56.

### **CURRENT REQUEST:**

The current request is to build similar residential structures as originally approved in 2017, but with fewer total units on Lots 9 and 10. The proposed site plan shows ten sets of buildings with four one-story attached units for a total of forty units amongst Lots 9 and 10. Each unit is proposed to be three bedrooms, resulting in 120 bedrooms. **With approval of the proposed major amendment, it would bring the total unit count to 532 dwelling units with 788 bedrooms, consisting of 340 one-bedroom units, 128 two-bedroom units, and 64 three-bedroom units. Although the change is slight reduction in total units, it is an increase in intensity of 40 bedrooms.**

Each of the proposed 40 townhouse style units will have its own two-car garage. They will all be built to front on the looped drive, West Crystal Loop. Seventeen additional on street parking stalls will be provided at each the north and south end of the looped road.

Access will remain as originally planned in two locations. Crystal Drive is private loop street that provides access to this corner of the site. It is the access point used for daily residential traffic. The plan also includes second emergency access point to the north.

The Fire Code requires two separate points of access when development exceeds 30 units of the townhomes built under the Residential Building code requirements, i.e units without fire sprinklers. This access road will connect from the northeast corner of Lot 10 across the adjacent parcel to Kitty Hawk Drive. The developer controls the property to the north and can ensure this access point is constructed and maintained.

Although the original approval did not require construction of the off-site extension of the emergency access road, with this amendment the access road will be required to be constructed prior building permits being issued. The road will be blocked by gates at each end that are accessible only by Fire and Police.

The adjacent site is zoned Highway Commercial and a access road is allowed to be constructed on the property. An easement to the benefit of the PRD site is needed to ensure it is kept accessible as required. Public Works and the Fire Department have reviewed the proposed road to ensure it complies with and meets necessary requirements to be effective. Staff will administer final approval of the emergency access road.

### **Planning & Zoning Commission Recommendation:**

At the September 21<sup>st</sup> Planning & Zoning Commission meeting the Commission voted 5-0 to approve the proposed Major Site Development Plan with the conditions that the second access to Kitty Hawk must be completed prior to building permits being issued for over thirty units. The Commission asked question about the access road and there was no public comment. Staff clarified that it would be limited to fire and police use only and not daily traffic and that the connection would remain if/when the adjacent parcel is developed.

### **PUBLIC NOTICE:**

A public notice sign has been placed on the property and hearing notices have been sent to all property owners within 200 feet of the Bricktowne PRD boundary.

### **ALTERNATIVES:**

1. The City Council can approve the Major Site Development Plan Amendment for Lots 9 and 10 to modify the site plan to allow the proposed building layout described in this report and the following conditions:
  - A. Complete construction of the fire access connection across the adjacent north parcel to Kitty Hawk prior to issuance of building permits for more than 30 units (in total) located on Lots 9 and 10. An access easement must also be recorded prior to occupancy exceeding 30 units.

2. The City Council can approve with modified conditions the request for an Amendment to Lots 9 and 10 of the Bricktowne PRD.
3. The City Council can deny the request for an Amendment to Lots 9 and 10 of the Bricktowne PRD if the Council finds that the City's regulations and policies are not met related to justifications for a F-PRD.

**CITY MANAGERS RECOMMENDED ACTION:**

The Planned Residence District was initially established as a Residential High Density base zone to allow for multi-family development. The original approval included deviations to allow for private streets, alternative landscaping plan, and provision of amenities in support of the tenants. Additionally, the site was limited through a contract rezoning agreement to a maximum of 750 dwelling units with a mix of bedroom types.

**The proposed amendment does not impact the maximum allowed intensity of development planned for the site and no new deviations are proposed for the amendment. The change in building types results in slightly more standard apartment dwellings and fewer individual townhome type units, but this does not significantly affect the housing mix, character, or layout of the PRD to alter the original findings for approval of the PRD.**

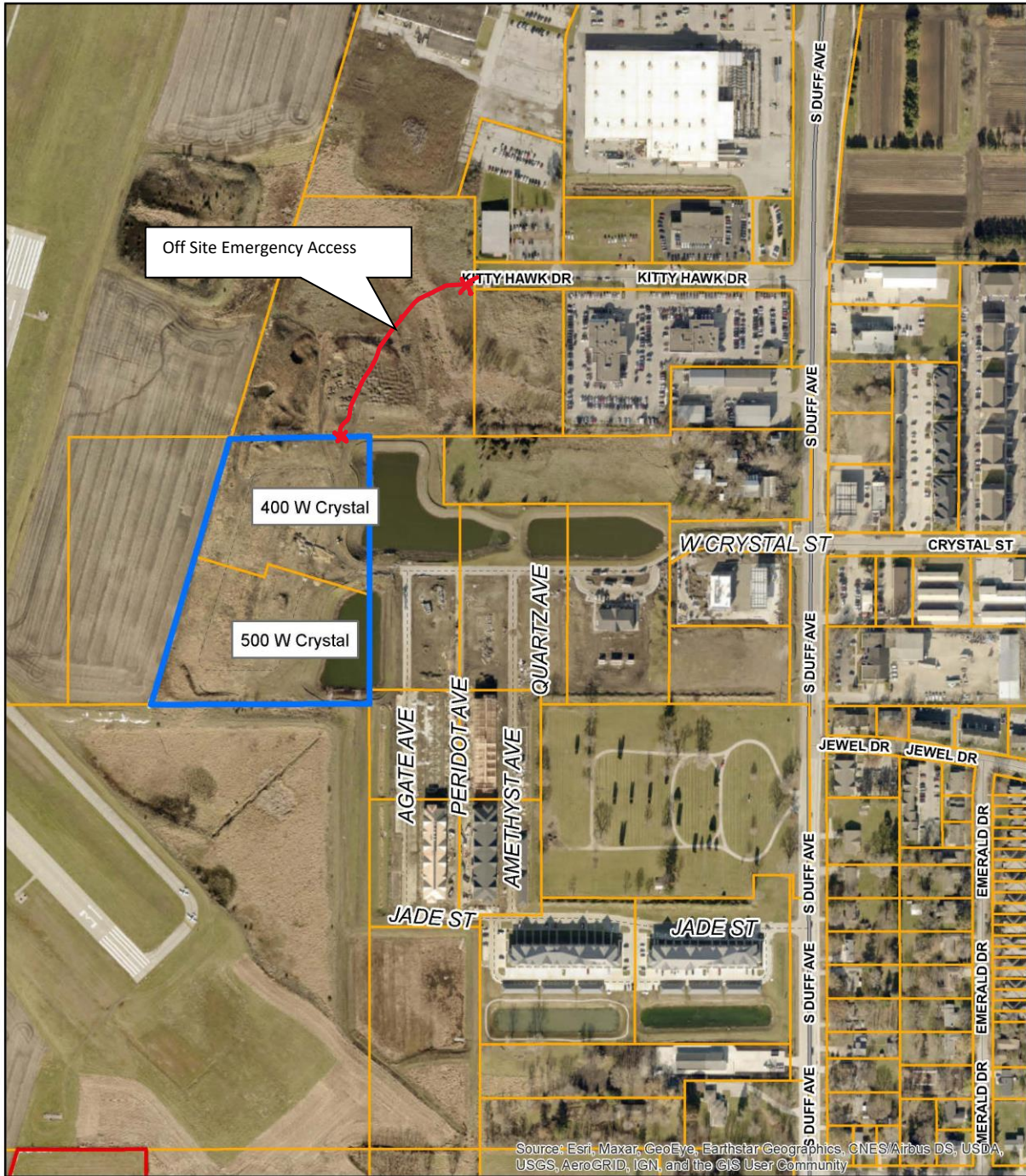
**The required fire access road must be constructed prior to building permits being issued for additional units.** This will provide an additional fire access to the site that will not be used for daily residential traffic. This access road will cross over an adjacent parcel that is owned by the same entity that owns the Bricktowne Development.

**With the conditions of approval, staff finds that the project meets the design principles of the F-PRD and complies with the standards of the MSDP. Therefore, it is the recommendation of the City Manager that City Council approve Alternative #1.**



# Attachment A

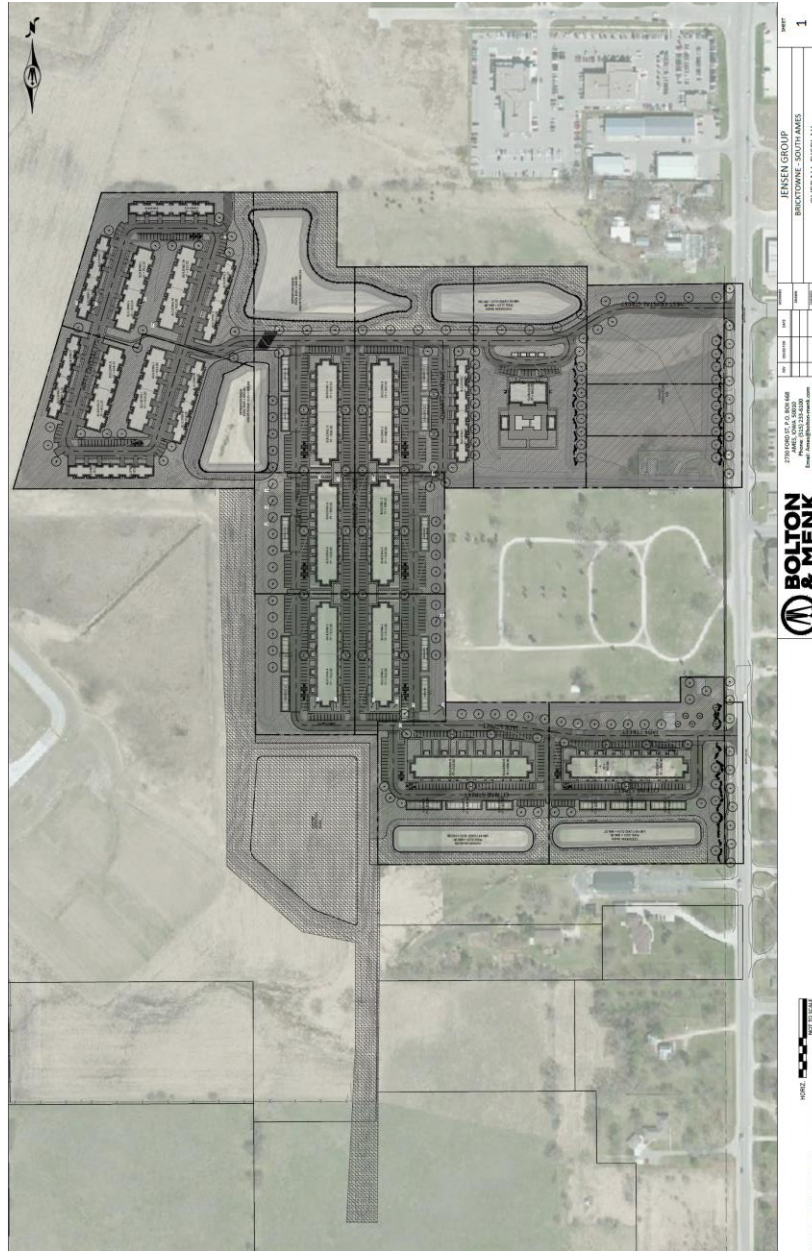
## Location Map



### Major Amendment to Planned Residential Development 400 & 500 W Crystal Loop

# Attachment B

## 2017 PRD Plan



# Attachment C

## *Findings Regarding Planned Residence District (F-PRD)*

### Development Principles

*Property that is zoned F-PRD shall be developed in accordance with the Zone Development Principles listed in Section 29.1203(2). Each principle is addressed below.*

- 1. Provide for innovative and imaginative approaches to residential development that would not occur as a result of the underlying zoning regulations.***

The proposed amendment keeps with the initial intent of the PRD that aimed to develop an apartment community tied together with common architectural styles and landscape features. As a result of the various amendments, a variety of unit types and multi-family designs throughout the development. The landscaping complies with the initially approved landscaping plan from 2017. The helps to facilitate the integrated overall design of an apartment community that would be unlikely to occur with public streets.

- 2. Result in a more efficient, aesthetic, desirable and economic use of land and other resources while maintaining density of use, as provided for in the Land Use Policy Plan (Ames Plan 2040) and the underlying zoning.***

The development layout is efficient. The subdivision of individual lots for each apartment building helps to facilitate financing of the project. However, the plan is tied to together with common design themes for architecture and landscaping to ensure it is desirable and integrated community. The requested relief for lotting does not affect the setbacks and spacing of the structures from perimeter property lines or each other compared to base zone standards.

- 3. Promote innovative housing development that emphasizes efficient and affordable home ownership and occupancy.***

All units provided for in this development will be attached units. Per the existing development agreement, the variety of housing is focused on smaller rental units desirable to a wide range of the market. Home ownership is not the intended goal for this project site.

- 4. Provide for flexibility in the design, height, and placement of buildings that are compatible with and integrate with existing, developed neighborhoods and the natural environment.***

The project includes attractive buildings with roof line variations, siding, and brick. All of the units on Lots 9 and 10 will include enclosed detached garages with some additional surface parking spaces. The buildings are set well into the site and exceed minimum setback requirements.

- 5. Promote aesthetic building architecture, significant availability of open space, well designed and landscaped off-street parking facilities that meet or exceed the underlying zone development standards, more recreation facilities than would***



**result from conventional development, and pedestrian and vehicular linkages within and adjacent to the property.**

The development overall has a significant amount of the area devoted to stormwater management measures. Approximately 25% of the PRD area is within stormwater easements. The site overall has 38% open space with these stormwater features included. The usable open space and amenities are focused on the clubhouse area. The clubhouse and amenity spaces are required elements of the PRD approval.

The site includes internal linkage for both pedestrians and vehicles. There is an additional outlet at the northwest corner of the site for future connectivity.

The landscaping plan is distinct for the site focusing on an “open prairie style theme.” The site utilizes large berms, grasses, and overstory tree to create the desired look and to enhance its compatibility with the surroundings. The project landscaping also accounts for commercial buffering to the HOC zoned land to the east.

- 6. Provide for the preservation of identified natural, geologic, historic and cultural resources, drainage ways, floodplains, water bodies, and other unique site features through the careful placement of buildings and site improvements.**

To meet regional stormwater needs, the site development plan significantly modifies an open channel along the north edge of the site and creates a large detention facility. This design and size of the feature helps to improve management of stormwater runoff for the general area and for the site itself. The design does not release additional water downstream at rate greater than current conditions. An existing drainage tile is not affected by the design.

- 7. Provide for a development design that can be more efficiently served by existing and proposed infrastructure, including: street, water, sewer, and storm water infrastructure, than would be otherwise required as a result of conventional development.**

All utilities will be and have been extended in order to serve the site. The design includes public water and sewer lines, but includes private streets to more efficiently serve the site. No additional through streets are needed that would require a public street into or through the development. Sidewalks are provided for along the site frontage and with the site.

**Planned Residential Development (PRD) Supplemental Development Standards.**

Property that is zoned F-PRD shall be developed in accordance with the Zone Development Standards listed in Table 29.1203(5). Each of those standards is addressed below. Refer to Table 29.1203(5) for the detailed standards.

- 1. Area Requirement. A minimum of two (2) acres shall be required for all areas developed as F-PRD.**

The subject site includes for the PRD zoning is 40.62 acres.

- 2. Density. Densities shall comply with the densities provided for in the Land Use Policy Plan(Ames Plan 2040) and the underlying base zone regulations. In the case of more than one base zone designation, each area of the PRD project shall comply**

***with the density limitation that is established for the base zone of that area. Density transfer from one area of a PRD project to another area of the same project with a lower base zone density is not permitted.***

The proposed development is based upon the RH base zone. The allowed building types include attached housing as apartments. The approved 2017 density is 14 units per gross acre. The net developable area averages a density of approximately 19 dwelling units per acre.

With the proposed changes the density will still exceed the minimum requirement of RH of 11 units per acre.

**3. *Height Limitations. Structures proposed to be developed in areas zoned PRD shall be compatible with the predominant height of the structures in adjacent neighborhoods.***

In this PRD, none of the residential structures or clubhouse will exceed three stories. The site development does not conflict height limits related to the operation of the airport. An aviation easement will be recorded to ensure the property owner maintains the site consistent with regulations for airport obstructions, including vegetation height. Due to the size and location of the site, the 1-story height is compatible with its surroundings. Most of the adjacent sites are underdeveloped or undeveloped sites with a commercial designation that allows for high intensity uses.

**4. *Minimum Yard and Setback Requirements.***

The development plans meets all perimeter setbacks for the existing site boundaries. Internal setbacks for the proposed residential lots are also maintained in conformance with multiple building siting requirements.

**5. *Parking Requirements.***

The parking for the development will be provided along the private streets throughout the development and with private garages. The code requirement for parking varies from 1.5 stalls per unit to 3 stalls per bedroom. The proposed plan exceeds minimum parking requirements for the types of proposed housing units. Parking is also included adjacent to the clubhouse and recreational area.

**6. *Open Space Design Requirements.***

The open space proposed includes common open space areas. Open Space is currently defined as “useable open space designed and intended for the use of all residents.” Common Open Space includes areas planned for active or passive recreation as well as areas of recreational activities such as swimming pools, tennis courts, shuffleboard courts, etc., but not including areas within required setbacks. The proposed project is interconnected with walkways. The stormwater design is intended to create wet ponds as a feature for the development. Amenity space with a clubhouse is included at the north end of the development.

**7. *Open Space Area Requirement.***

A minimum landscaped open space requirement of 35% is required for the PRD with a RH base zone. The overall project is noted to provide 38% open space. The common open space proposed will be maintained by the developer. This meets the requirements of the F-PRD zone.

**8. *Open Space Improvements and Amenities.***

The Plan includes areas for both active recreation and passive open space.

**9. *Maintenance of Open Space and Site Amenities.***

All open spaces will be maintained by the developer. No changes to amenities.

# Attachment D

## Findings Regarding Major Site Development Plan Design Standards

*Additional criteria and standards for review of all Major Site Development Plans are found in Ames Municipal Code Section 29.1502(4)(d) and include the following requirements.*

- 1. The design of the proposed development shall make adequate provisions for surface and subsurface drainage to limit the rate of increased runoff of surface water to adjacent and downstream property.***

The development meets the City's Municipal Code requirements to capture specified quantities of water and treat it for water quality before releasing it from the site. The treatment facilities are located on five outlots, spread throughout the development. The outlots contain three stormwater retention / detention ponds.

- 2. The design of the proposed development shall make adequate provision for connection to water, sanitary sewer, electrical, and other utility lines within the capacity limits of those utility lines.***

The provision and extension of utilities was reviewed and found adequate to support the anticipated load of the proposed development.

- 3. The design of the proposed development shall make adequate provision for fire protection through building placement, acceptable location of flammable materials, and other measures to ensure fire safety.***

The fire inspector has reviewed access and fire truck circulation and found that the needs of the fire department will be met once the fire access road to Kitty Hawk Drive is constructed. Upon completion, the site will have four street connections and a fifth one restricted to fire access only.

- 4. The design of the proposed development shall not increase the danger of erosion, flooding, landslide, or other endangerment to adjoining and surrounding property.***

The proposed development is not anticipated to create a danger. Stormwater facilities are located on five outlots containing three stormwater ponds.

- 5. Natural topographic and landscape features of the site shall be incorporated into the development design.***

There are no notable topographic features of the site. Most of the site is farm field and is mostly devoid of existing landscaping and trees.

- 6. The design of the interior vehicle and pedestrian circulation shall provide for convenient flow of vehicles and movement of pedestrians and shall prevent hazards to adjacent streets or property.**

The proposed development is compliant with all connections previously approved.

- 7. The design of outdoor parking areas, storage yards, trash and dumpster areas, and other exterior features shall be adequately landscaped or screened to minimize potential nuisance and impairment to the use of adjoining property.**

Landscaping and screening are provided as required by code.

- 8. The proposed development shall limit entrances and exits upon adjacent streets in order to prevent congestion on adjacent and surrounding streets and in order to provide for safe and orderly vehicle movement.**

The development complies with development standards for connecting existing street stubs as approved with the original site plan in 2017.

- 9. Exterior lighting shall relate to the scale and location of the development in order to maintain adequate security, while preventing a nuisance or hardship to adjacent property or streets.**

Lighting details will be provided at the time of building permit. The lighting plan will be required to meet the City's standards for "dark sky" protection.

- 10. The proposed development shall ensure that dust and other forms of air pollution, noise disturbances, odor, glare, and other nuisances will be limited to acceptable levels as prescribed in other applicable State and City regulations.**

The proposed development is not expected to generate any nuisances as a residential development.

- 11. Site coverage, building scale, setbacks, and open spaces shall be in proportion with the development property and with existing and planned development and structures, in adjacent and surrounding property.**



The site layout and proposed buildings are of a scale compatible to the surrounding uses. The surrounding area is part of the larger Bricktowne development and within the approved PRD.

# **Attachment E**

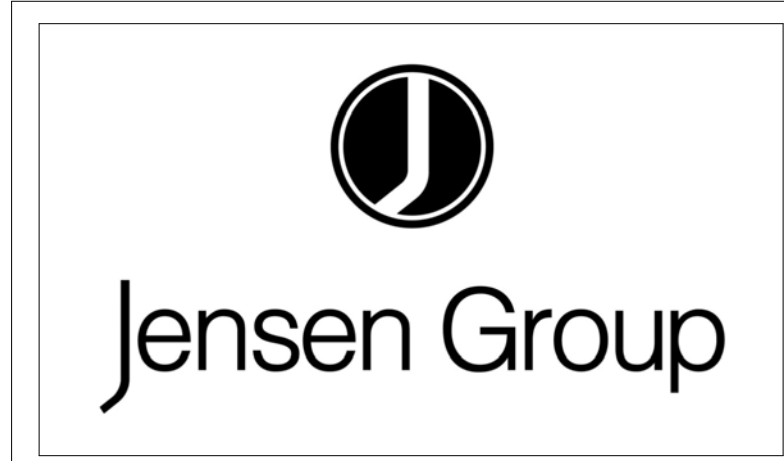
*PRD - Site Development Plan and Townhome Elevations*









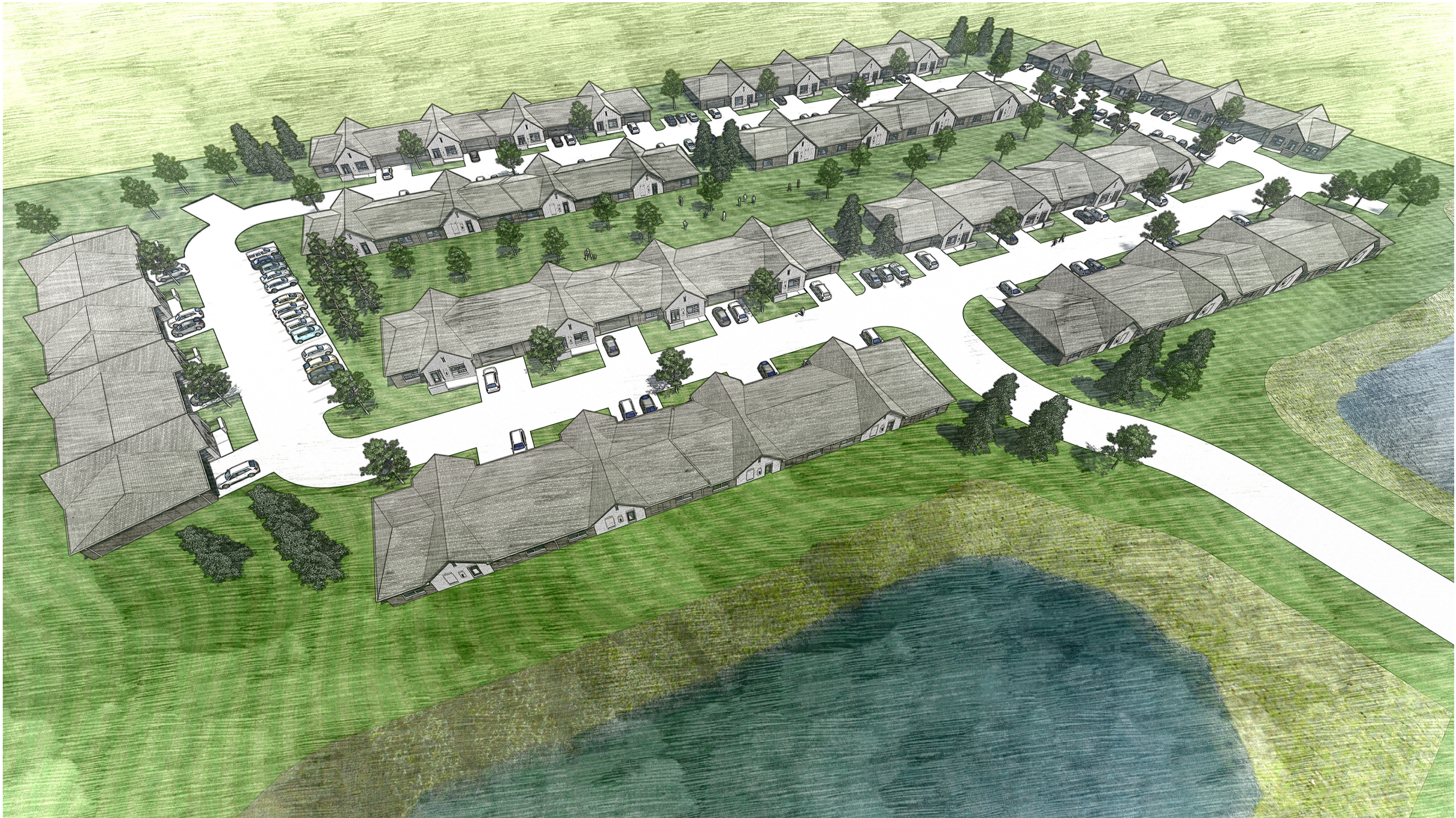


BRICKTOWNE - SOUTH AMES

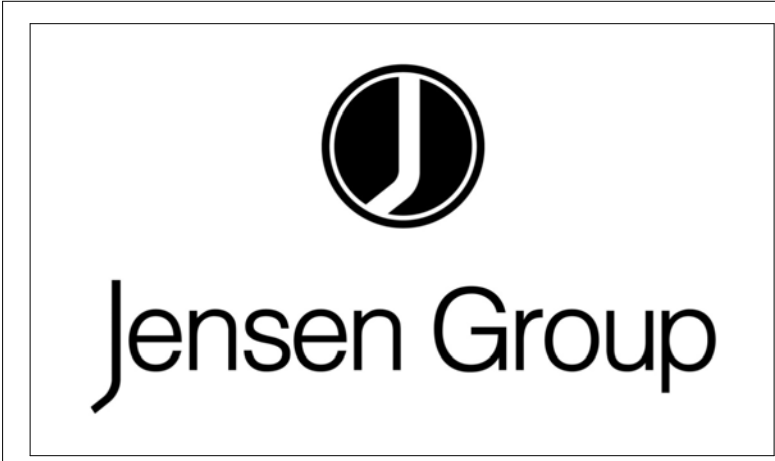
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BRICKTOWNE - SOUTH AMES

Project Number:	PERSPECTIVES
MINOR SITE PLAN AMENDMENT 07/06/22	A5











**GENERAL NOTES**

1. THE 2022 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL COMPLY WITH ADA ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES.
3. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
4. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER, ENGINEER, AND/OR ARCHITECT, AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
5. ONE WEEK PRIOR TO CONSTRUCTION WITHIN CITY R.O.W. OR ANY CONNECTION TO PUBLIC SEWERS CONTRACTOR SHALL NOTIFY THE CITY'S CONSTRUCTION DIVISION.
6. ALL CONSTRUCTION WITHIN PUBLIC R.O.W./ EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS, SHALL COMPLY WITH THE CITY'S STANDARDS.
7. ALL DIMENSIONS ARE TO BACK OF CURB, BUILDING FACE OR PROPERTY LINE UNLESS OTHERWISE NOTED.
8. CONTRACTOR TO VERIFY BUILDING DIMENSIONS WITH ARCHITECTURAL PLANS.
9. PLACE 3/4 INCH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT/SIDEWALKS AND BUILDING. PLACE 1/2 INCH EXPANSION JOINT BETWEEN SIDEWALKS AND P.C.C. PAVEMENT.
10. REMOVE ALL DEBRIS SPILLED INTO R.O.W. AT THE END OF EACH WORK DAY.
11. ALL PROPERTY PINS SHALL BE PROTECTED FROM GRADING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT OF WAY.
13. THE CONTRACTOR SHALL NOT DISTURB DESIRABLE GRASS AREAS AND DESIRABLE TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS. STORAGE, PARKING AND SERVICE AREAS WILL BE SUBJECT TO THE APPROVAL OF THE OWNER.
14. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR SIDEWALK NOT TO BE REMOVED THAT IS DAMAGED DUE TO OPERATING EQUIPMENT ON THE PAVEMENT OR SIDEWALK.
15. THE CONTRACTOR MAY BE REQUIRED TO PLACE TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE REPLACEMENT FEATURES ARE NOT INSTALLED THE SAME DAY, AS DIRECTED BY THE ENGINEER OR THE CITY.
16. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF WORK BETWEEN SUPPLIERS' AND SUBCONTRACTORS INVOLVED IN THE PROJECT, INCLUDING STAGING OF CONSTRUCTION DETAILS.
17. CONCRETE REMOVAL FOR DRIVEWAY APPROACHES SHALL BE REMOVED TO THE NEAREST TRANSVERSE JOINT. CONTRACTOR SHALL VERIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION INSPECTOR PRIOR TO ANY CONCRETE REMOVAL.
18. ALL SCOUR STOP SHALL BE INSTALLED BY A CERTIFIED CONTRACTOR ACCORDING TO SCOUR STOP PRODUCT SPECIFICATIONS.
19. ALL PERMITS AND ADDITIONAL FEES REQUIRED TO COMPLETE THE WORK SHALL BE INCLUDED IN THE CONTRACTOR BID.

**PAVEMENT THICKNESS**

- |                   |           |
|-------------------|-----------|
| 1. SIDEWALKS      | 4" P.C.C. |
| 2. PARKING STALLS | 6" P.C.C. |
| 3. DRIVE AISLES   | 6" P.C.C. |

**TRAFFIC CONTROL NOTES**

1. ALL APPLICABLE CITY PERMITS, INCLUDING BUT NOT LIMITED TO CLOSURE PERMITS, SHALL BE OBTAINED PRIOR TO ANY CONSTRUCTION WITHIN CITY R.O.W. OR LANE CLOSURES.
2. ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
3. PERMANENT SIGNING THAT CONVEYS A MESSAGE CONTRARY TO THE MESSAGE OF TEMPORARY SIGNING AND NOT APPLICABLE TO THE WORKING CONDITIONS SHALL BE COVERED BY THE CONTRACTOR WHEN DIRECTED BY THE CITY.
4. THE CONTRACTOR SHALL COORDINATE HIS TRAFFIC CONTROL WITH OTHER CONSTRUCTION PROJECTS IN THE AREA.
5. SIDEWALK CLOSED SIGNS REQUIRED FOR ALL SIDEWALK CLOSURES.
6. THE CONTRACTOR IS CAUTIONED NEITHER TO OBSTRUCT NOR REMOVE ANY EXISTING PAVEMENT, NOR TO DISTURB THE EXISTING TRAFFIC PATTERNS MORE THAN IS NECESSARY FOR THE PROPER EXECUTION OF THE WORK.
7. ALL SIGNING AND LANE STRIPING WILL NEED TO COMPLY WITH MUTCD. MAINTENANCE AND REPLACEMENT OF THE SIGNING AND STRIPING WILL BE THE RESPONSIBILITY OF THE APPLICANT.

**DEMOLITION NOTES**

1. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER OR ENGINEER AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
2. PRIOR TO DEMOLITION, CONTRACTOR SHALL NOTIFY IN WRITING (48 HRS NOTICE) THE FOLLOWING:
  - A. CITY
  - B. APPROPRIATE UTILITY COMPANIES
  - C. OWNER
  - D. CIVIL DESIGN ADVANTAGE
  - E. ARCHITECT
3. DO NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE NOTED. COORDINATE REMOVAL OR ABANDONMENT OF ALL UTILITIES WITH THE APPROPRIATE UTILITY SUPPLIER AND REGULATORY AGENCIES.
4. PROTECT EXISTING UTILITIES THAT ARE TO REMAIN. THE LOCATIONS OF ALL UTILITIES INDICATED ON THE PLANS ARE TAKEN FROM EXISTING RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLAN MAY BE PRESENT.
5. BACKFILL ALL EXCAVATIONS WITH COHESIVE MATERIAL COMPACTED TO 95% MAXIMUM STANDARD PROCTOR DRY DENSITY AND MOISTURE RANGE OF OPTIMUM MOISTURE TO 4% ABOVE OPTIMUM MOISTURE. TESTING OF BACKFILL TO BE BY A GEOTECHNICAL ENGINEER EMPLOYED BY THE OWNER. IN THE EVENT OF A TEST FAILURE, ANY RETESTING SHALL BE PAID FOR BY THE CONTRACTOR.
6. FIELD VERIFY EXISTING GRADES AND LOCATION OF EXISTING UTILITIES, CONDUIT, LINES, POLES, TREES, PAVING, BUILDING AND OTHER SITE FEATURES PRIOR TO DEMOLITION AND IMMEDIATELY INFORM THE ENGINEER AND/OR OWNER OF ANY DISCREPANCIES.
7. DEMOLITION NOTES AS SHOWN ON THE PLAN ARE NOT ALL INCLUSIVE. CONTRACTOR TO ABANDON IN PLACE OR REMOVE AND DISPOSE OF ALL EXISTING SITE IMPROVEMENTS ABOVE AND BELOW GROUND TO COMPLY WITH THE GENERAL INTENT OF THIS DOCUMENT.
8. ALL CONSTRUCTION/DEMOLITION DEBRIS SHALL BE DISPOSED OFFSITE IN FULL COMPLIANCE WITH CURRENT ENVIRONMENTAL REGULATIONS.
9. CONSTRUCTION LIMITS SHALL BE CONFINED TO THE SITE BOUNDARY AS NOTED. ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
10. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA STANDARDS. NOTHING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.

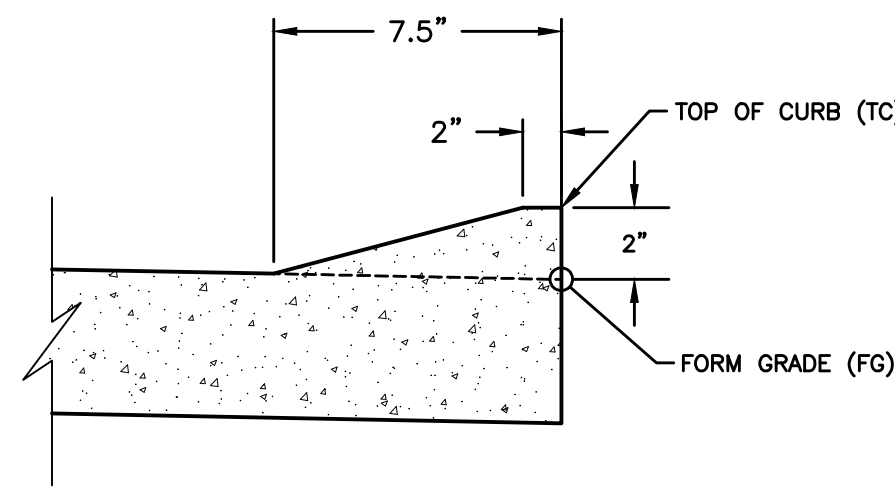
**GRADING NOTES**

1. PRIOR TO ANY GRADING, A COPY OF THE NPDES PERMIT SHALL BE PROVIDED TO THE CITY'S BUILDING DIVISION.
2. CONTRACTOR SHALL STRIP ALL DELETERIOUS MATERIAL. THE TOP 8" OF TOPSOIL IS TO BE STOCKPILED AND RESPREAD AFTER GRADING IS COMPLETE. CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING A SUITABLE TOPSOIL STOCKPILE SITE.
3. EXCAVATION SHALL BE IN ACCORDANCE WITH THE 2022 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE.
4. MATCH EXISTING GRADES AT PROPERTY LINES AND/OR CONSTRUCTION LIMITS.
5. ALL SPOT ELEVATIONS ARE FORM GRADE (FG) OR TOP OF FINISHED SURFACES UNLESS OTHERWISE NOTED.
6. SITE SHALL BE GRADED TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS.
7. SLOPES IN PAVEMENT SHALL BE UNIFORM TO AVOID PONDING.
8. THE CONTRACTOR SHALL CONFINE HIS GRADING OPERATIONS TO WITHIN THE CONSTRUCTION LIMITS AND EASEMENTS SHOWN ON THE PLANS. ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL APPLY NECESSARY MOISTURE CONTROL TO THE CONSTRUCTION AREA AND HAUL ROADS TO PREVENT THE SPREAD OF DUST.
10. REFER TO SEPARATE STORM WATER POLLUTION PREVENTION PLAN FOR DETAILS ON EROSION CONTROL.
11. FINAL FINISH GRADING TO BE APPROVED BY THE ARCHITECT AND CIVIL ENGINEER. MATCH EXISTING GRADES AT THE INTERFACE OF NEW AND EXISTING GRADES OR PAVING.
12. SIDEWALKS: MAINTAIN 1% MINIMUM AND 5% MAXIMUM LONGITUDINAL SLOPES ON ALL PAVED WALKWAYS. ALL WALKS TO HAVE 2.0% MAXIMUM TRANSVERSE SLOPE IN THE DIRECTION OF NATURAL DRAINAGE. SAW CUT JOINTS AS SOON AS CONCRETE HAS SET. SAW CUTS TO BE 1/8" TO 1/4" WIDE; DEPTH: LONGITUDINAL T/3, TRANSVERSE T/4.
13. AS-BUILTS OF ALL ADA RAMPS SHALL BE COMPLETED AND APPROVED BY PUBLIC WORKS ENGINEERING DEPARTMENT FOLLOWING COMPLETION.

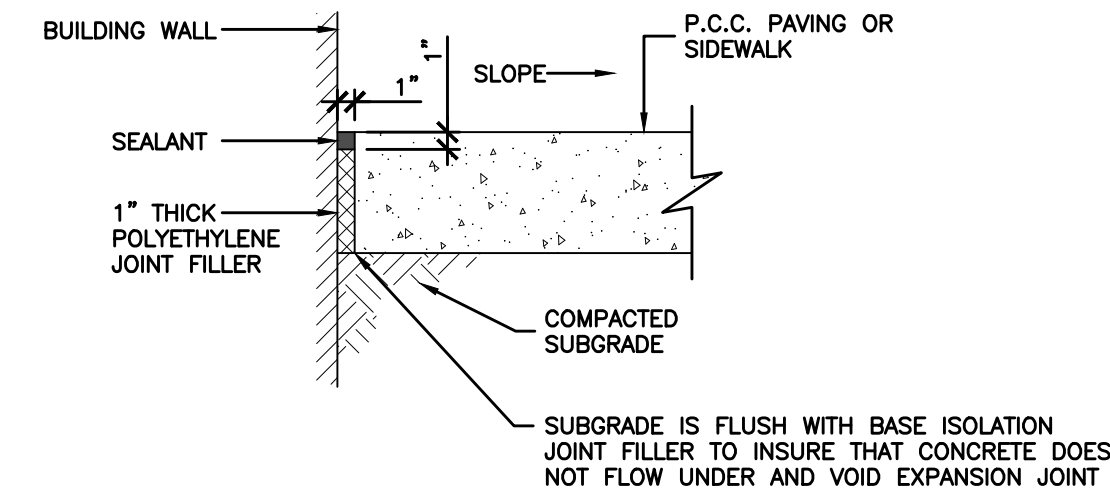
F.F.E = FINISHED FLOOR ELEVATION  
G.F.E = DRIVEWAY ELEVATION AT OUTSIDE FACE OF GARAGE DOOR.

**UTILITY NOTES**

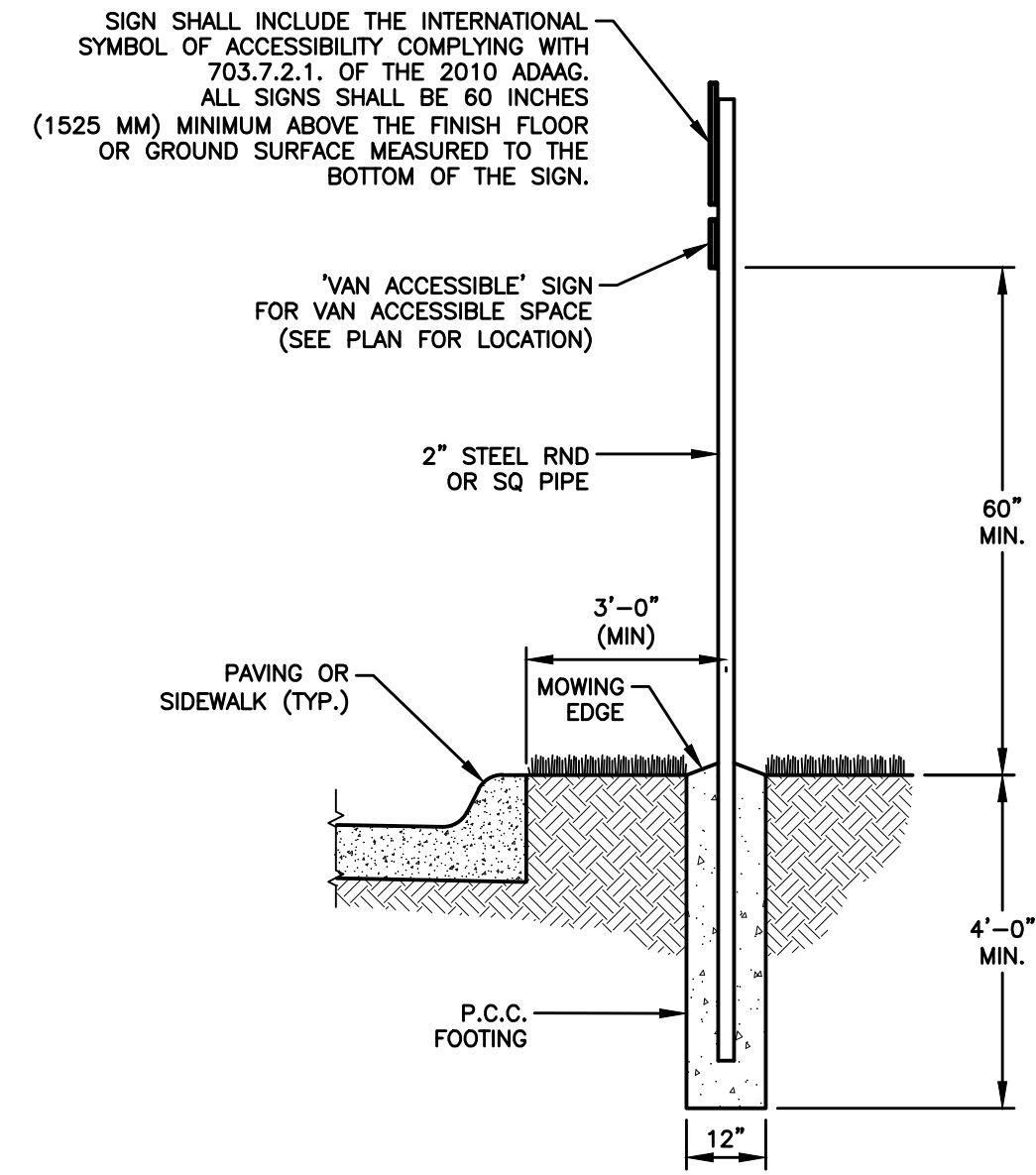
1. REFER TO MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR UTILITY SERVICE SIZES AND EXACT LOCATIONS. REFER TO ELECTRICAL PLANS FOR ELECTRIC AND TELEPHONE SERVICE CONSTRUCTION DETAILS. REFER TO MECHANICAL PLANS FOR GAS SERVICE CONSTRUCTION DETAILS.
2. FIELD VERIFY ELEVATIONS AND LOCATIONS OF ALL CONNECTIONS TO EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION.
3. PROVIDE TEMPORARY SUPPORT FOR EXISTING UTILITY LINES THAT ARE ENCOUNTERED DURING CONSTRUCTION UNTIL BACKFILLING IS COMPLETE.
4. BACKFILL ALL UTILITY TRENCHES ACCORDING TO THE 2022 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS. MAINTAIN A MINIMUM OF 5.5' COVER OVER ALL WATERMANS.
5. ALL UTILITIES SHALL BE STUBBED TO 5 FEET FROM BUILDINGS. REFER TO MEP PLANS FOR DESIGN FROM 5' OUTSIDE OF BUILDING FACE.
6. ADJUST ALL MANHOLES AND INTAKES TO FINISHED GRADES.
7. ALL SANITARY SEWER AND WATER SERVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY'S PLUMBING CODE.
8. 18" MINIMUM VERTICAL CLEARANCE BETWEEN STORM SEWER AND SANITARY SEWER PIPES. 18" MINIMUM VERTICAL CLEARANCE BETWEEN SANITARY SEWER AND WATER MAIN.
9. MAINTAIN A MINIMUM OF 10' HORIZONTAL SEPARATION BETWEEN SANITARY SEWER LINES AND WATER MAINS.
10. WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THESE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES, UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE, THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS. THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATIONS AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK. THE CONTRACTOR IS REQUIRED TO UTILIZE THE UTILITY ONE-CALL SERVICE AT 800-292-8989 AT LEAST 48 HOURS PRIOR TO EXCAVATING ANYWHERE ON THE PROJECT.
11. ALL WATERMAIN WORK, PUBLIC OR PRIVATE SHALL BE DONE IN ACCORDANCE WITH THE 2022 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS.
12. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK OF ALL SUBCONTRACTOR(S) INVOLVED IN THE PROJECT.
13. PRIVATE UTILITIES TO BE INSTALLED PER THE CITY'S STANDARD CONSTRUCTION SPECIFICATIONS FOR PUBLIC IMPROVEMENTS AND THE 2012 UNIFORM PLUMBING CODE. CONTACT BUILDING INSPECTION A MINIMUM OF 24 HOURS IN ADVANCE FOR UTILITY INSTALLATION INSPECTIONS.
14. OWNER IS RESPONSIBLE FOR MAINTENANCE OF PRIVATE RETENTION FACILITIES AND PRIVATE UTILITIES.
15. CONTRACTOR SHALL PREVENT ENTRY OF MUD, DIRT, DEBRIS AND OTHER MATERIAL INTO NEW AND EXISTING SEWER SYSTEMS. SHOULD ANY CONTAMINATION OCCUR DURING CONSTRUCTION, THE CONTRACTOR SHALL CLEAN AT NO COST TO THE OWNER. INSTALL SILT FENCE AT ALL PERMANENT STORM SEWER INLETS.
16. UTILITY AS-BUILTS WILL BE REQUIRED FOLLOWING CONSTRUCTION.



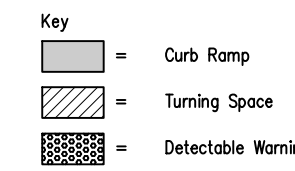
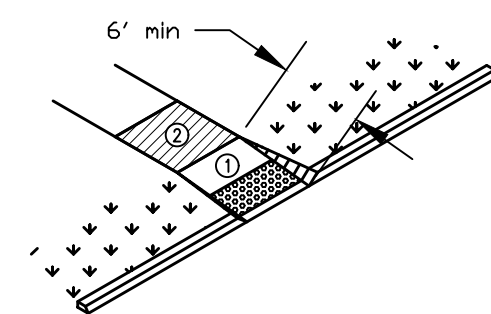
**1 2" ROLLED CURB**  
NOT TO SCALE



**2 ISOLATION JOINT AT BUILDING INTERFACE**  
NOT TO SCALE



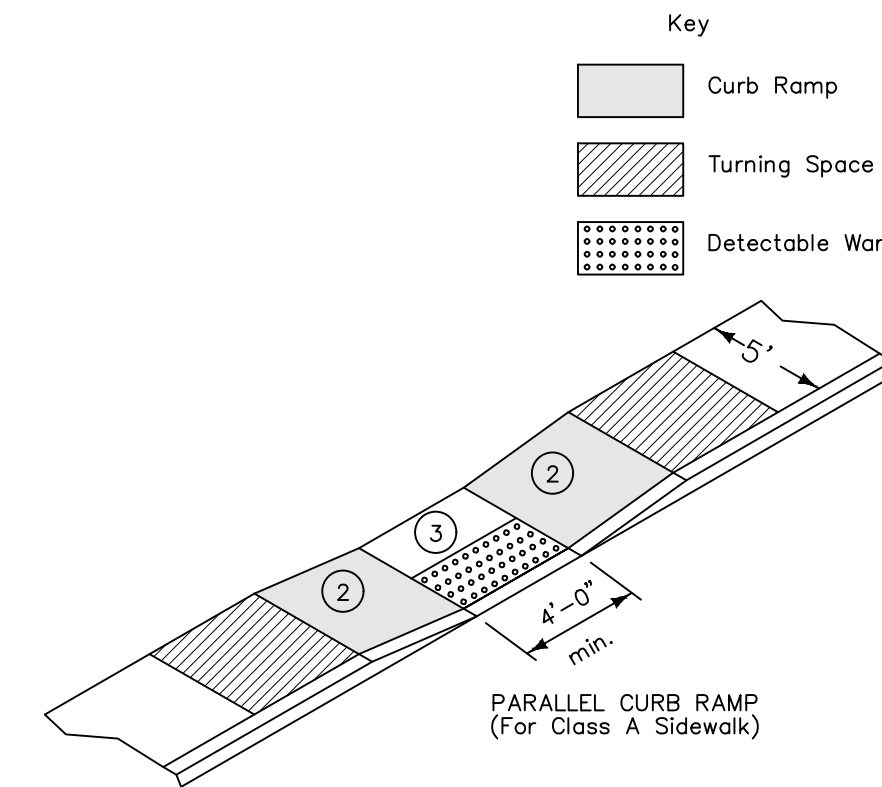
**3 ACCESSIBLE PARKING SIGN ON POST**  
NOT TO SCALE



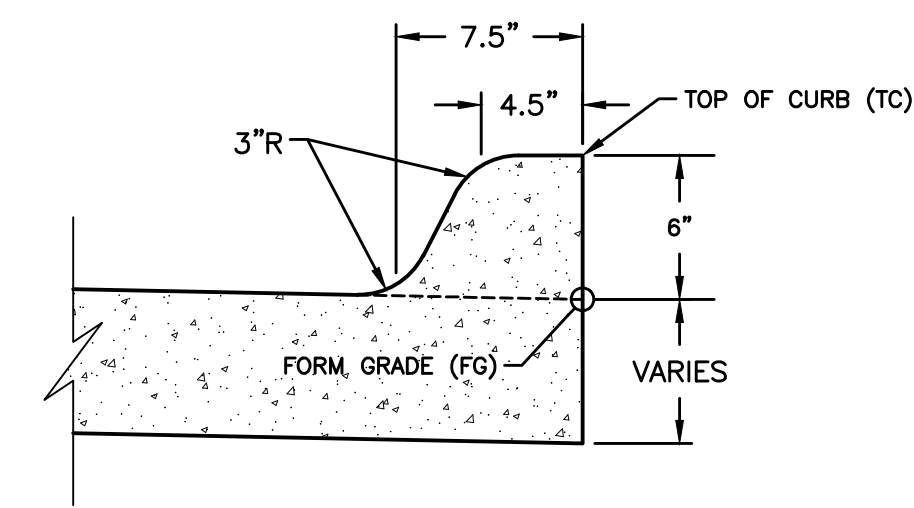
**PERPENDICULAR RAMP**  
(For Class A Sidewalk)

**4 CURB RAMP DETAILS**  
NOT TO SCALE

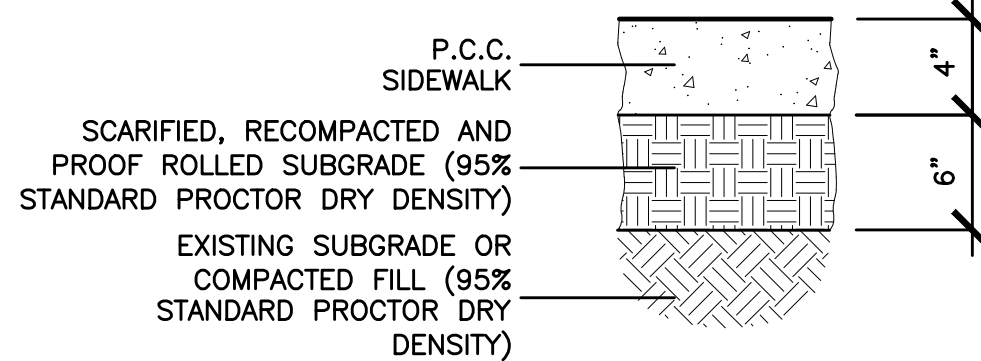
REFER TO PLANS FOR SIDEWALK DIMENSIONS



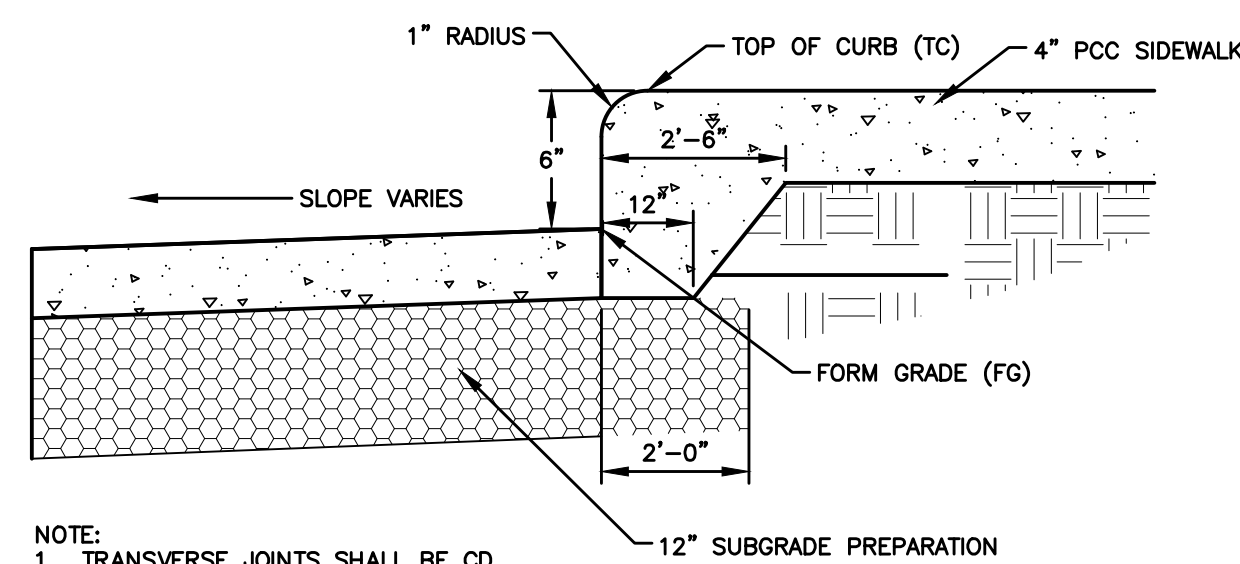
- Key**
- █ Curb Ramp
  - ▨ Turning Space
  - ▤ Detectable Warning
1. Perpendicular Curb Ramp: Target running slope of 6.25% with maximum running slope of 8.3%. Match pedestrian street crossing cross slope at back of curb. At mid-block crossings, cross slope may exceed 2.0% to match roadway grade.
  2. Parallel Curb Ramp: Target cross slope of 1.5% with a maximum cross slope of 2.0%. The length of the parallel ramp is not required to exceed 15 feet, regardless of resulting slope. Do not exceed 8.3% slope for parallel ramps shorter than 15 feet.
  3. Turning Space: Target slope of 1.5% with a maximum slope perpendicular to the travel directions of 2.0%. At mid-block crossings, cross slope of landing may exceed 2.0% to match roadway grade. Minimum 4 feet by 4 feet.
  4. Flare (10:1 max.) required if ramp is contiguous with sidewalk.



**5 INTEGRAL 6" STANDARD CURB**  
NOT TO SCALE

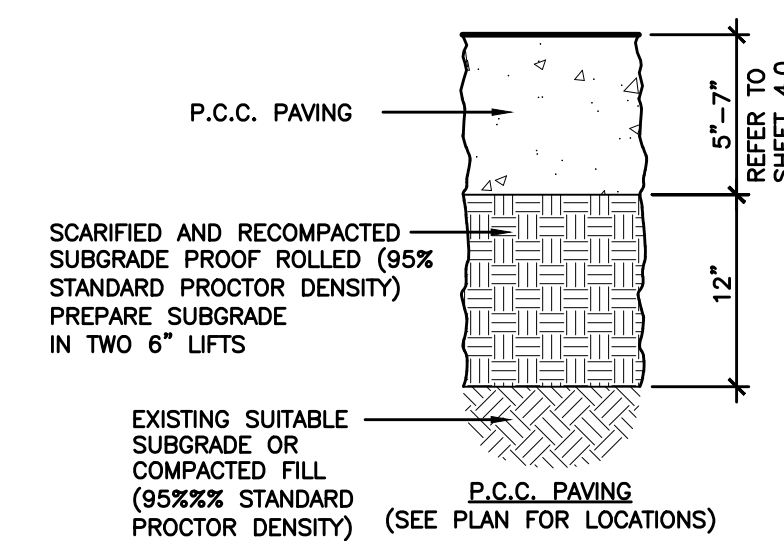


**6 P.C.C. SIDEWALK DETAIL**  
NOT TO SCALE



- NOTE:**
1. TRANSVERSE JOINTS SHALL BE CD JOINTS AT 15' MAX. SPACING.
  2. LONGITUDINAL JOINTS SHALL BE INTERCHANGEABLE, L-2 OR KT-2.

**7 CLASS A CURB & SIDEWALK**  
NOT TO SCALE



- NOTE:**
1. SUBGRADE PREPARATION SHALL EXTEND 24" BEYOND EDGE OF PAVING OR BACK OF CURB, EXCEPT WHEN ABUTTING EXISTING PAVEMENT.
  2. TRANSVERSE JOINTS SHALL BE "CD" JOINTS FOR 8" PCC AND "C" JOINTS FOR 6" PCC AT 15' MAX. SPACING.
  3. LONGITUDINAL JOINTS SHALL BE INTERCHANGEABLE, L-2 OR KT-2 FOR 8" PCC AND L-1 OR KT-1 FOR 7" PCC.

**8 TYPICAL SECTION - P.C.C. PAVEMENT**  
NOT TO SCALE

FILE: H:\2021\119193\DWG\119193-1 SITE LOTS 9 - 10.DWG  
DRAWN: J. H. HERRICK  
CHECKED: J. H. HERRICK  
DATE: 9/12/2022 11:09 AM

DATE	08/19/2022	07/28/2022
REVISIONS		
SITE PLAN SUBMITTAL #1		
SITE PLAN SUBMITTAL #1		
PRELIMINARY SITE PLAN		

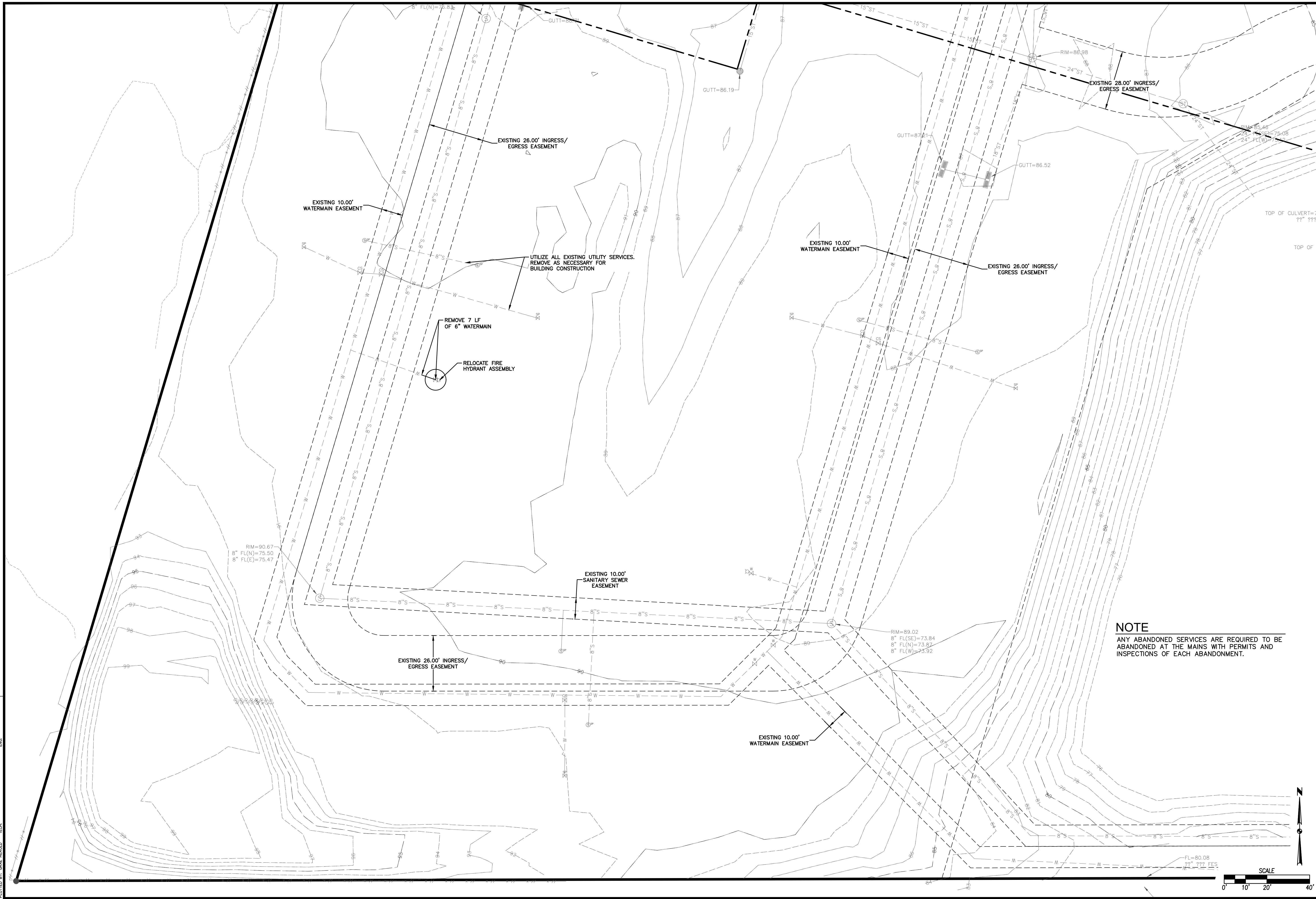
4121 NW URBANDALE DRIVE  
URBANDALE, IOWA 50322  
PHONE: (515) 369-4400 FAX: (515) 369-4410  
ENGINEER: GH

**DA** CIVIL DESIGN ADVANTAGE  
AMES, IOWA

**BRICKTOWNE SOUTH AMES - LOTS 9 & 10**  
**MAJOR SITE PLAN AMENDMENT**  
**DETAILS & NOTES**

**2.0**  
211.931

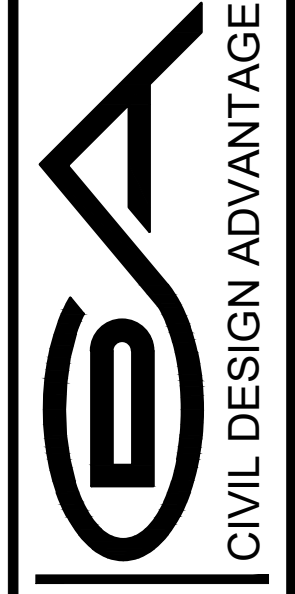
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 PLOTTED BY: ARAZ HEROLD  
 DATE: 9/12/2022 11:09 AM  
 COMMENT: DWG



**NOTE**  
 ANY ABANDONED SERVICES ARE REQUIRED TO BE ABANDONED AT THE MAINS WITH PERMITS AND INSPECTIONS OF EACH ABANDONMENT.

DATE	REVISIONS
08/19/2022	SITE PLAN SUBMITTAL #1
07/28/2022	SITE PLAN SUBMITTAL #1
07/28/2022	PRELIMINARY SITE PLAN

4121 NW URBANDALE DRIVE  
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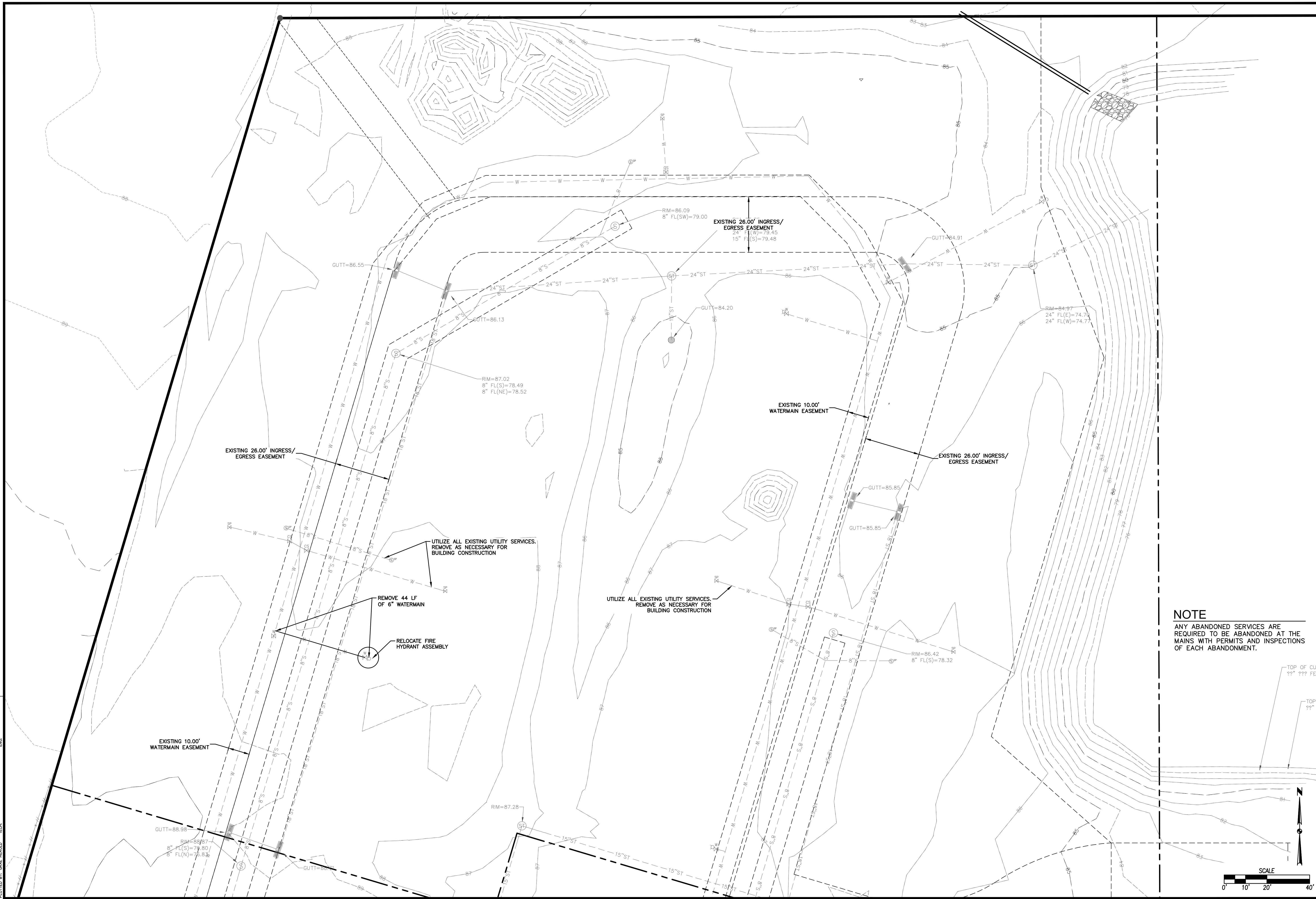


**BRICKTOWNE SOUTH AMES - LOTS 9 & 10**  
**MAJOR SITE PLAN AMENDMENT**  
**TOPOGRAPHIC SURVEY & DEMOLITION PLAN**

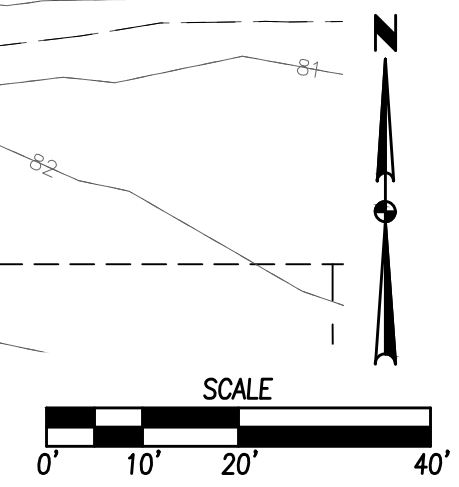
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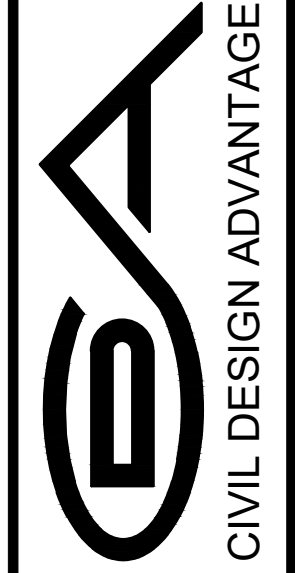


**NOTE**  
 ANY ABANDONED SERVICES ARE  
 REQUIRED TO BE ABANDONED AT THE  
 MAINS WITH PERMITS AND INSPECTIONS  
 OF EACH ABANDONMENT.



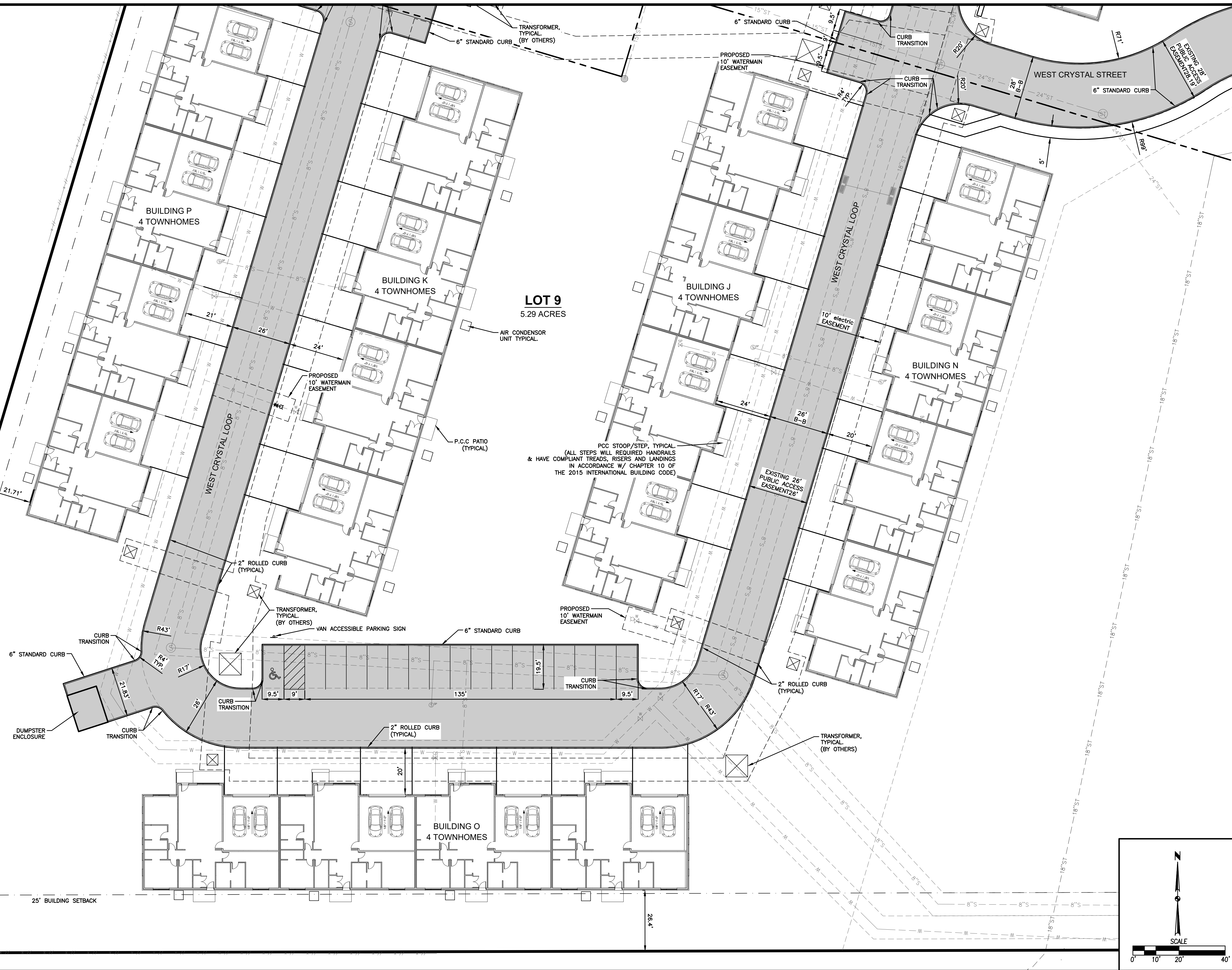
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SITE PLAN SUBMITTAL #1	07/28/2022
PRELIMINARY SITE PLAN	07/28/2022

4121 NW URBANDALE DRIVE  
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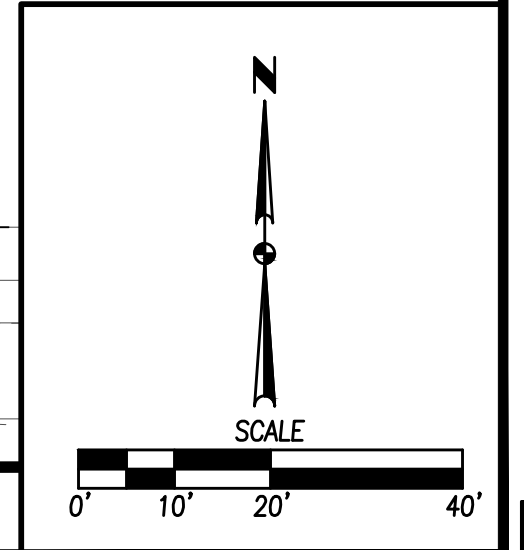


**BRICKTOWNE SOUTH AMES - LOTS 9 & 10**  
**MAJOR SITE PLAN AMENDMENT**  
**TOPOGRAPHIC SURVEY & DEMOLITION PLAN**  
 AMES, IOWA

FILE: H:\2021\2111931\DWG\2111931-SITE LOTS 9 -10.DWG  
 COMMENT: DWG  
 PLOTTED BY: ARAE HEROLD  
 DATE: 9/12/2022 11:09 AM



PCC STOOP/STEP, TYPICAL  
 (ALL STEPS WILL REQUIRED HANDRAILS  
 & HAVE COMPLIANT TREADS, RISERS AND LANDINGS  
 IN ACCORDANCE W/ CHAPTER 10 OF  
 THE 2015 INTERNATIONAL BUILDING CODE)



DATE	REVISIONS
08/19/2022	SITE PLAN SUBMITTAL #1
07/28/2022	SITE PLAN SUBMITTAL #1
07/28/2022	PRELIMINARY SITE PLAN

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 ENGINEER: GH



AMES, IOWA

**BRICKTOWNE SOUTH AMES - LOTS 9 & 10**  
**MAJOR SITE PLAN AMENDMENT**  
**DIMENSION**









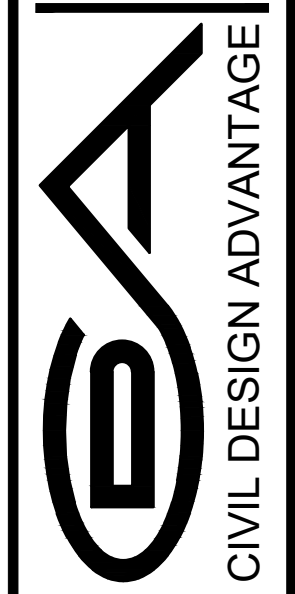




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DATE	REVISIONS
08/19/2022	SITE PLAN SUBMITTAL #1
07/28/2022	PRELIMINARY SITE PLAN

4121 NW URBANDALE DRIVE  
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 ENGINEER: GH



CIVIL DESIGN ADVANTAGE  
 AMES, IOWA

**BRICKTOWNE SOUTH AMES - LOTS 9 & 10**  
**MAJOR SITE PLAN AMENDMENT**  
**GRADING PLAN**



# BRICKTOWNE SOUTH AMES - LOTS 9 & 10

## EROSION AND SEDIMENT CONTROL PLAN

### SWPPP LEGEND

DRAINAGE ARROW		AREA TO BE SEEDED	
GRADING LIMITS		STRAW MAT	
FILTER SOCK		UNDISTURBED AREA	
SILT FENCE		RIP-RAP	
DITCH CHECK		GRAVEL ENTRANCE	
INLET PROTECTION		STAGING AREA	
PORTABLE RESTROOM		TEMPORARY SEDIMENT TRAP	
TEMPORARY STANDPIPE		TEMPORARY SEDIMENT BASIN	
CONCRETE WASHOUT PIT			

### STABILIZATION QUANTITIES

ITEM NO.	ITEM	UNIT	TOTAL
1	SILT FENCE	LF	2,139
2	SEEDING, FERTILIZING, AND MULCHING	AC	7.94
3	INLET PROTECTION DEVICES	EA	11
4	CONCRETE WASHOUT PIT	EA	1

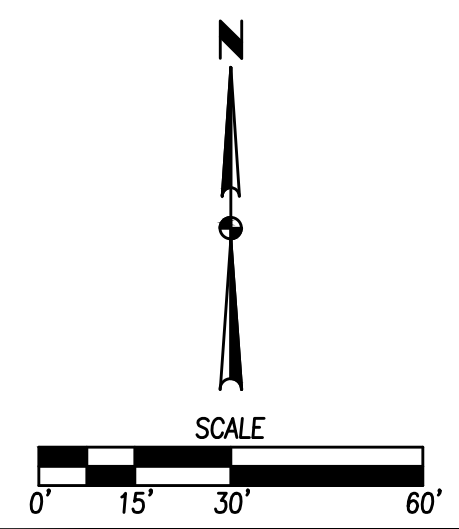
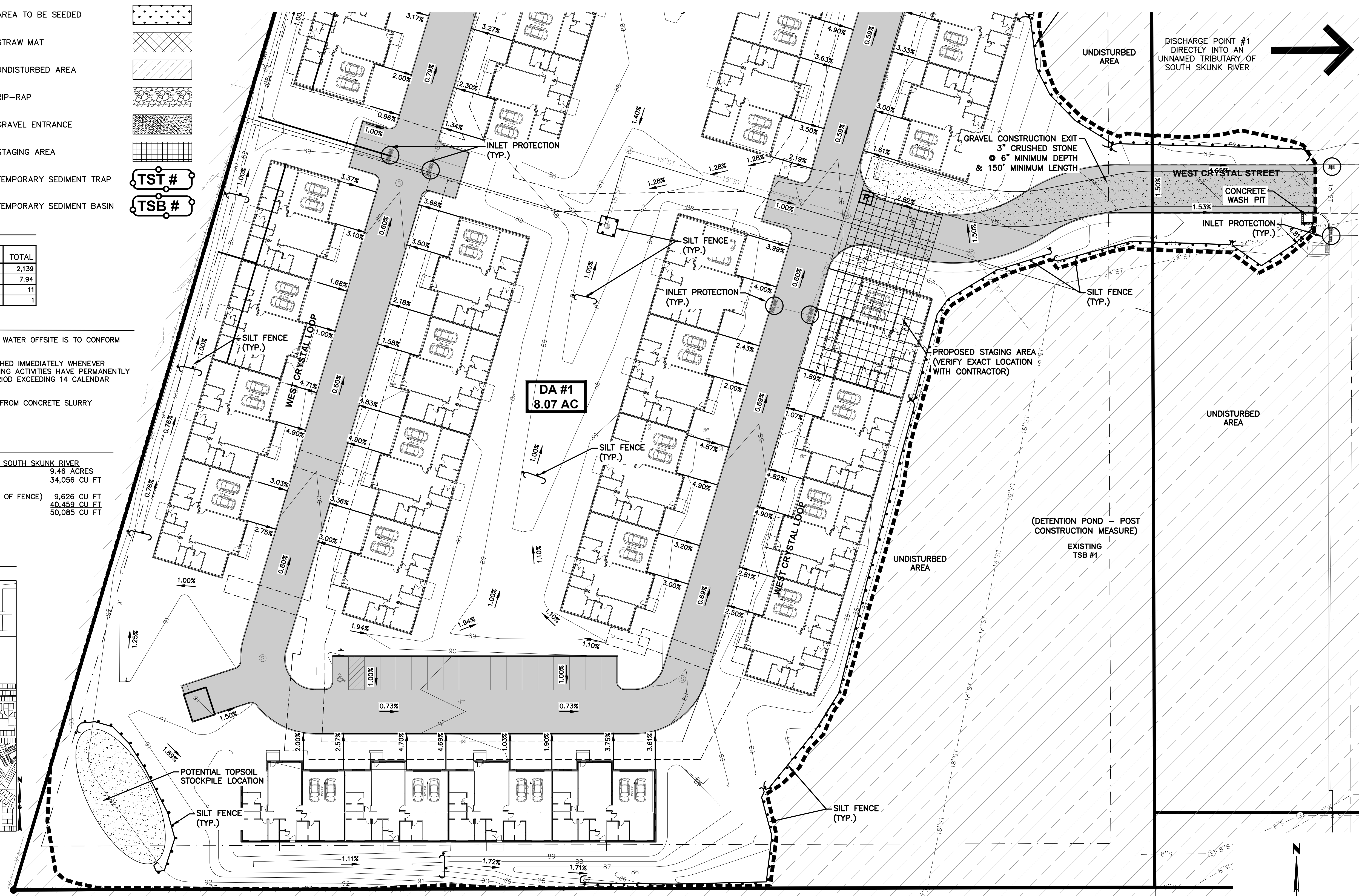
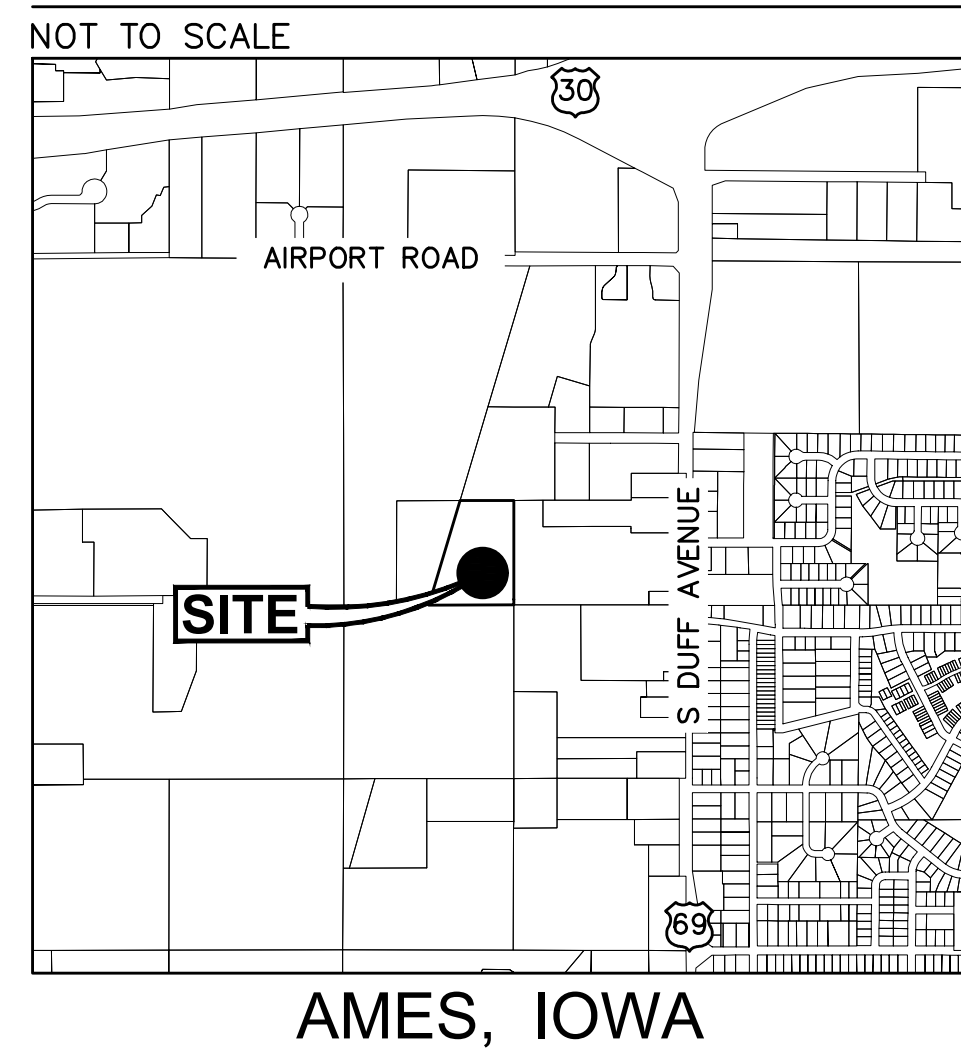
### NOTES:

- IF DEWATERING IS NEEDED FOR ANY REASON, DISCHARGE OF WATER OFFSITE IS TO CONFORM WITH THE GENERAL PERMIT #2 REQUIREMENT.
- DISTURBED AREAS SHALL BE TEMPORARILY SEEDED OR MULCHED IMMEDIATELY WHENEVER CLEARING, GRADING, EXCAVATING, OR OTHER EARTH DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS.
- STORM SEWERS AND DRAINAGE WAYS SHALL BE PROTECTED FROM CONCRETE SLURRY PRODUCED BY SAWCUTTING AND CONCRETE GRINDING.

### DISCHARGE POINT SUMMARY

DISCHARGE POINT #1 DIRECTLY INTO AN UNNAMED TRIBUTARY OF SOUTH SKUNK RIVER	
TOTAL AREA DISTURBED TO DISCHARGE POINT	9.46 ACRES
STORAGE VOLUME REQUIRED (# OF ACRES*3600 CU FT)	34,056 CU FT
VOLUME PROVIDED IN SILT FENCE (2,139 LF @ 4.5 CU FT/LF OF FENCE)	9,626 CU FT
VOLUME PROVIDED IN EXISTING TSB #1	40,459 CU FT
TOTAL VOLUME PROVIDED	50,085 CU FT

### VICINITY MAP



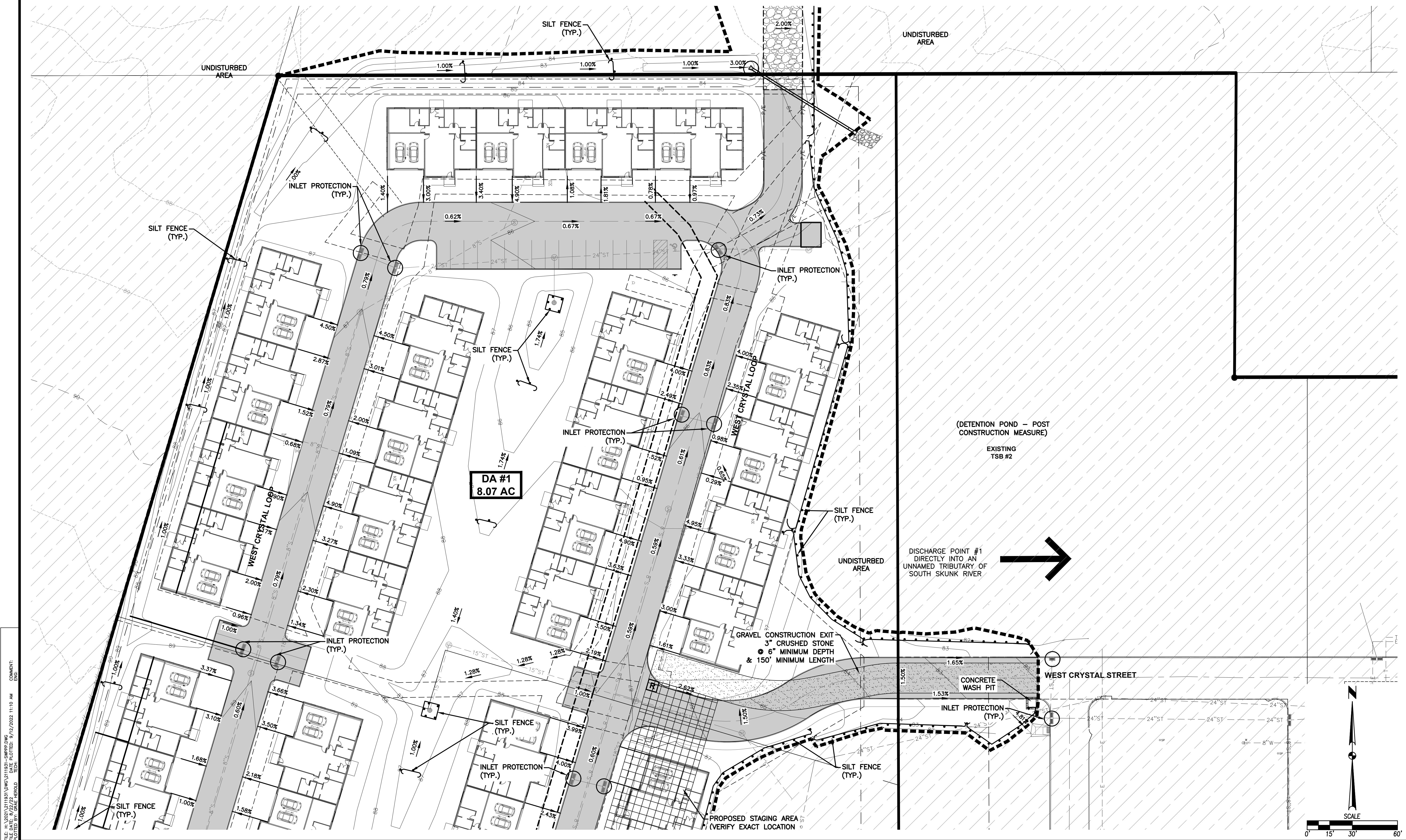
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<p>DATE: 08/19/2022 07/28/2022</p> <p>REVISIONS:</p> <p>SITE PLAN SUBMITTAL #1 SITE PLAN SUBMITTAL #1 PRELIMINARY SITE PLAN</p>	<p>4121 NW URBANDALE DRIVE URBANDALE, IOWA 50322 PHONE: (515) 369-4400 FAX: (515) 369-4410</p> <p>ENGINEER: GH</p>
<p>CIVIL DESIGN ADVANTAGE</p>	
<p><b>BRICKTOWNE SOUTH AMES - LOTS 9 &amp; 10</b>  <b>MAJOR SITE PLAN AMENDMENT</b>  <b>EROSION AND SEDIMENT CONTROL PLAN</b></p>	
<p>AMES, IOWA</p>	
<p><b>6.0</b></p>	
<p>2111.931</p>	



# BRICKTOWNE SOUTH AMES - LOTS 9 & 10

## EROSION AND SEDIMENT CONTROL PLAN



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07/28/2022	SITE PLAN SUBMITTAL #1
07/28/2022	PRELIMINARY SITE PLAN

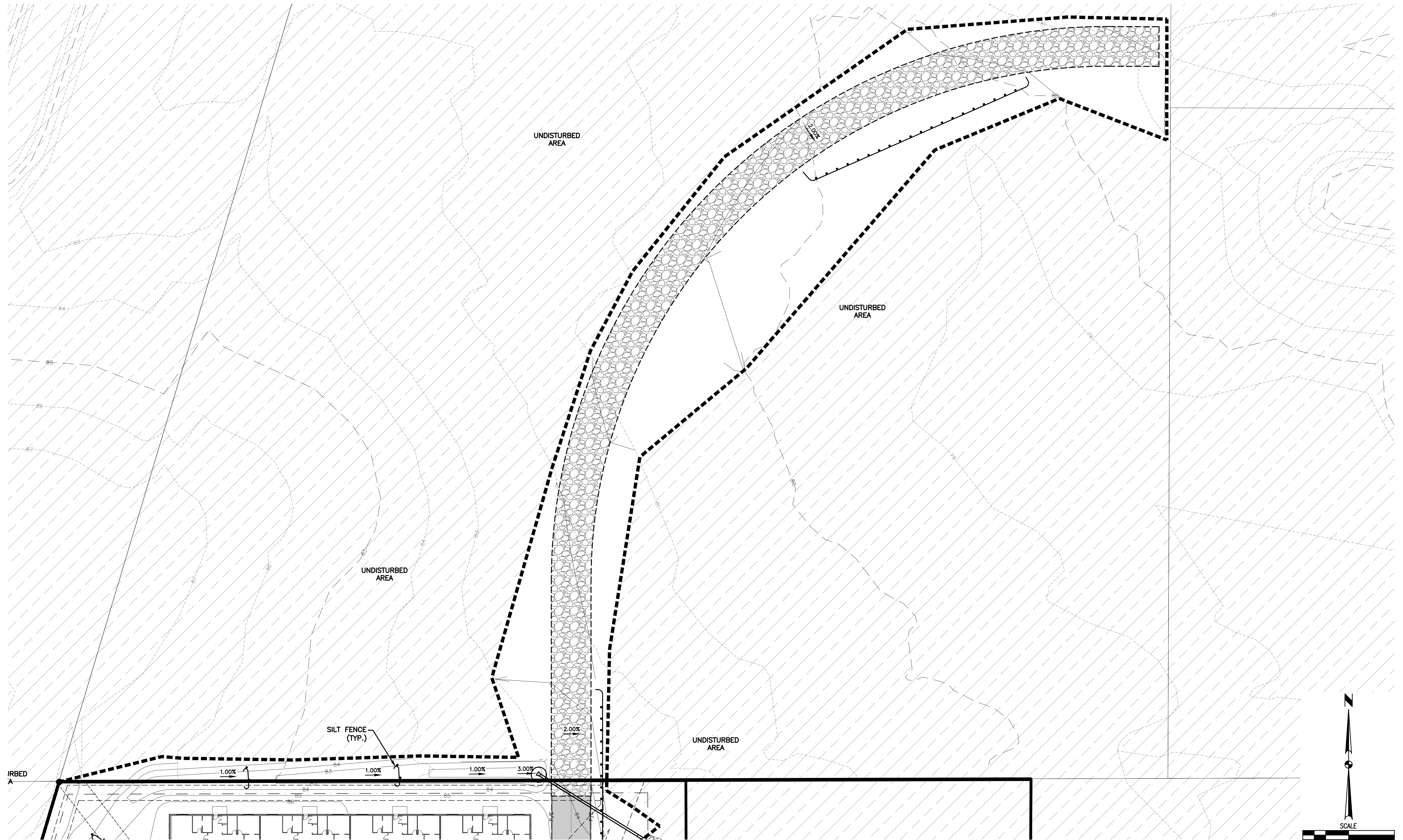
4121 NW URBANDALE DRIVE  
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 PHONE: (515) 369-4400 FAX: (515) 369-4410  
 ENGINEER: GH



BRICKTOWNE SOUTH AMES - LOTS 9 & 10  
 MAJOR SITE PLAN AMENDMENT  
 EROSION AND SEDIMENT CONTROL PLAN  
 AMES, IOWA

# BRICKTOWNE SOUTH AMES - LOTS 9 & 10

## EROSION AND SEDIMENT CONTROL PLAN



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REVISIONS	DATE
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SITE PLAN SUBMITTAL #1	07/28/2022
PRELIMINARY SITE PLAN	07/28/2022

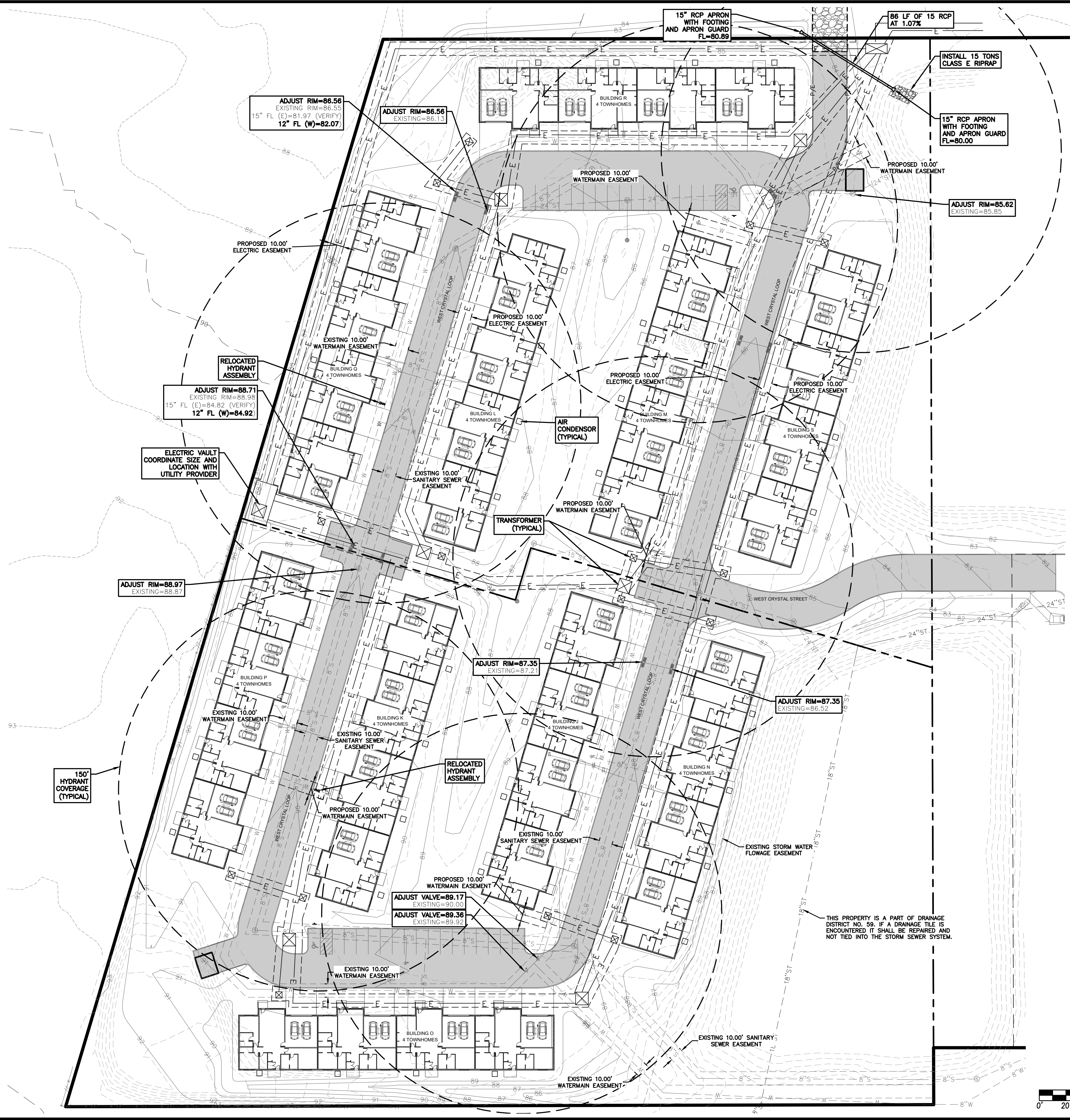
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BRICKTOWNE SOUTH AMES - LOTS 9 & 10  
 MAJOR SITE PLAN AMENDMENT  
 EROSION AND SEDIMENT CONTROL PLAN  
 AMES, IOWA

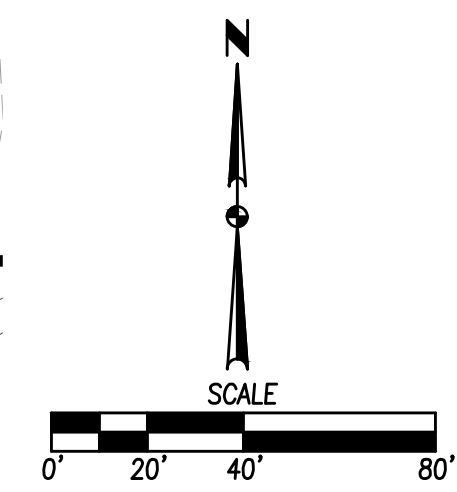


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 COMMENT: DWG  
 PLOTTED BY: ARAE HEROLD  
 DATE: 9/12/2022 11:10 AM



**GRADING NOTES**

ELECTRIC SHOWN IS FOR REFERENCE ONLY. COORDINATE WITH UTILITY PROVIDER FOR FINAL LOCATION.  
 ADJUST ALL UTILITY STRUCTURES TO FINISHED GRADE.



DATE	08/19/2022
REVISIONS	07/28/2022
SITE PLAN - SUBMITTAL #1	
SITE PLAN - SUBMITTAL #1	
PRELIMINARY SITE PLAN	

4121 NW URBANDALE DRIVE  
 URBANDALE, IOWA 50322  
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 ENGINEER: GH

**ES** CIVIL DESIGN ADVANTAGE  
 AMES, IOWA

**BRICKTOWNE SOUTH AMES - LOTS 9 & 10**  
**MAJOR SITE PLAN AMENDMENT**  
**UTILITY PLAN**

**7.0**  
 2111.931







