

ITEM #: 27c
DATE: 10-15-16

COUNCIL ACTION FORM

REQUEST: MAJOR SITE DEVELOPMENT PLAN FOR 2121 COTTONWOOD ROAD

BACKGROUND:

Hunziker Development Company, LLC is requesting approval of a Major Site Development Plan for a 12-unit apartment building on the property located at 2121 Cottonwood Road, (Lot 3 of Village Park Subdivision). Lot 3 includes 33,920.93 square feet (0.78 acres), and is zoned as “FS-RM” (Suburban Residential Medium Density), (See *Attachment A: Location & Zoning Map & Attachment B: Major Site Development Plan.*) The site is located west of University Boulevard, across from the Iowa State University Research Park, and south of the Wessex apartment complex. Land to the west and south of the Village Park Subdivision is outside the Ames corporate limits, in rural Story County. The overall layout of Village Park Subdivision, including open space landscaping in the Outlots, is shown on *Attachment G*.

A three-story, 12-unit, apartment building is proposed, including four (1-bedroom units), and eight (2-bedroom) units for a total of twenty bedrooms, with an average of 1.67 bedrooms per unit (See *Attachment E: Floor Plans*). Development of the property is required to be consistent with the rezoning Master Plan agreement for the property. The Master Plan for this property, originally identified as 3535 S. 530th Avenue (Village Park Subdivision), includes FS-RM and RH (Residential High Density). The Final Plat for Village Park Subdivision includes eleven lots for development in the FS-RM zone, and one lot for development in the “RH” (Residential High Density) zone (See *Attachment F: Village Park Subdivision Final Plat*). Approximately 100 to 120 units, and an approximate density of thirteen to seventeen units per acre is shown on the approved Master Plan (See *Attachment H: Master Plan for Village Park Subdivision*) for the FS-RM areas of the Master Plan.

The FS-RM lots are planned for a single 12-unit, three-story, apartment building on Lots 1, 2, 3 and 4, and a 12-unit, two-story apartment building on Lot 5. One 8-unit, two-story building is planned for each lot along the west perimeter of the subdivision, on Lots 6 through 11. Apartment buildings in the RH portion of the subdivision, will be located south of Cottonwood Road and east of Aurora Avenue, and are planned to include four, 36-unit buildings, three stories in height. The RH apartment buildings require City staff approval of Minor Site Development Plans for development of the entire 5.52-acre site (Lot 12).

The proposed development of 12-unit apartment buildings, along the north side of Cottonwood Road, follow a pattern and orientation that features the buildings facing the street, with surface parking along one side of the building, and behind the building in the rear yard, and garages along the north property line of each lot. The front door of the

building faces Cottonwood. The proposed development on this particular lot, includes two garage buildings along the north property line, with six parking stalls in each garage building. Surface parking spaces are located along the east property line, and behind the apartment building. The parking spaces behind the building share an access aisle with the garage parking stalls. Access to the site for parking is a shared access easement centered on the property line between Lot 3 (2121 Cottonwood Road), and Lot 2 (2105 Cottonwood Road). The shared access between Lots 2 and 3, also serves as a temporary emergency vehicle access that extends through the Wessex Apartment development adjacent to the north, and out to Oakwood Road, until such time that Cottonwood Road is extended to the west to connect with the existing Cottonwood Road in the Suncrest Subdivision (See *Attachment B: Major Site Development Plan, Parking Layout*).

The proposed design is for 3-story buildings with gabled roofs. The individual units are accessed from an internal corridor with an exit to the front and rear of the building. Exterior building materials include primarily vinyl lap siding on the second and third stories, and in the gables facing east and west. Cultured stone veneer is limited to the first floor level, except for the south façade where the cultured stone material extends to the roofline in one area, and a point between the second and third stories in another area of the south facade. The primary entry to the building, facing the street is also covered with the cultured stone veneer. Each façade includes a cultured stone veneer element to provide design continuity and interest on all facades. Cultured stone pillars support the decks and roofs above the decks, and above the primary entry to the building. Each deck will be constructed with treated lumber decking and aluminum guardrails. Sliding glass doors will provide access to the decks and patios outside the apartment units (See *Attachment C: Building Elevations*).

The FS-RM zone includes requirements for common open space that consists of a minimum of 10% of the overall FS-RM area in a development. The common open space that serves the tenants of the proposed apartment building, and all lots in the FS-RM portion of Village Park Subdivision, is located within outlots (See *Attachment D: Final Plat for Village Park Subdivision & Attachment G: Open Space Landscape Plan*). Outlot D includes a 10-foot wide shared use path along the edge of the outlot. The shared use path for Outlot A is located in the right-of-way for Cottonwood Road. Ponds for storm water treatment and detention are located in both outlots. A 25-foot wide existing wooded buffer follows the majority of the southeast boundary of Outlot D along the eastern edge of the detention pond. The open space has essentially been designed to accommodate stormwater detention (See *Landscape Plan for Outlots A & D*). Landscape Plans for Outlots A and D will be considered by the City Council as a separate agenda item.

Landscaping is provided to meet front yard apartment foundation and parking lots screening requirements. The developer has proposed parking lot landscaping as a combination of L1 and L2 parking lot screening elements. No landscaping has been proposed or required along the north property line behind the proposed garages due to existing landscaping along the Wessex development. The apartments are

approximately 200 feet to the north.

Off-street parking has been provided to meet the minimum parking requirements for apartment dwellings in the FS-RM zone. Access is shared with the adjoining property as required within the subdivision plat. No lighting has been proposed with the site development plan. Further details on the overall plans are found in the Addendum of this report.

At its meeting on November 2, 2016, the Planning and Zoning Commission reviewed the proposed Major Site Development Plan, and discussed site layout, open space access, landscaping configurations, and design of the project. There were no comments from the applicant, or the public. By a vote of 5-0, the Commission recommended approval of the Site Plan with the following conditions, as recommended by staff:

- A. That the landscaping, and site improvements, as shown on the Landscape Plan for Outlots A & D, be installed, prior to occupancy of the first FS-RM apartment building in the Village Park Subdivision;
- B. Prior to approval of the Major Site Development Plan by the City Council, a Minor Amendment to the Planned Residence District Plan for the Wessex Apartment Development, to include the temporary emergency vehicle access, must be approved by City staff;
- C. Prior to occupancy of the first FS-RM apartment building in the Village Park Subdivision, construction of a temporary emergency vehicle access to the Village Park Subdivision must be constructed to the specification of the Fire Department with a minimum capability of accommodating vehicles with a gross vehicle weight of up to 80,000 pounds;
- D. Prior to consideration by the City Council, gates must be added to the Major Site Development Plan, for screening of the garbage collection areas, in order to meet the requirements of Section 29.408(3) of the *Municipal Code*.
- E. Prior to occupancy of the apartment building at 2121 Cottonwood Road, shrubs used as screening for mechanical units must be installed to meet zoning standards at a minimum height that is six inches higher (at the time of planting) than the mechanical equipment the shrubs are screening.
- F. Compliance of all exterior lighting, with the Outdoor Lighting Code, Section 29.411 of the *Municipal Code*, and approval by the Planning & Housing Department, is required prior to occupancy.

Since the Planning and Zoning Commission meeting, staff has approved the Minor Amendment to the Planned Residence District Plan for the Wessex apartment development (Condition “B”), and the applicant has made revisions to the plan to address outdoor lighting code compliance. Staff and the developer have finalized trash enclosure options. No lighting is proposed with the plan at this time. The recommended conditions have been updated to reflect recent work with staff. The Developer is in agreement with the overall set of conditions and will finalize their plan sets upon approval by City Council.

ALTERNATIVES:

1. The City Council can approve the Major Site Development Plan for the proposed 12-unit apartment building, located at 2121 Cottonwood Road, as proposed, with the following conditions:
 - A. That the landscaping, and site improvements, as shown on the Landscape Plan for Outlots A & D, be installed, prior to occupancy of the first FS-RM apartment building in the Village Park Subdivision;
 - B. Prior to occupancy of the first FS-RM apartment building in the Village Park Subdivision, construction of a temporary emergency vehicle access to the Village Park Subdivision must be constructed to the specification of the Fire Department with a minimum capability of accommodating vehicles with a gross vehicle weight of up to 80,000 pounds;
 - C. The landscape plan shall include revisions necessary to specify plantings appropriate in size equal to the height of the mechanical equipment to ensure the compliance with the zoning ordinance screening requirements.
2. The City Council can approve the Major Site Development Plan for the proposed 12-unit apartment building, located at 2121 Cottonwood Road, with modifications.
3. The City Council can deny the Major Site Development Plan for the proposed 12-unit apartment building, if it finds the project does not meet the Major site Development Plan criteria.
4. Action on this request can be postponed and referred back to City staff and/or the applicant for additional information.

CITY MANAGER'S RECOMMENDED ACTION:

The Major Site Development Plan review is to determine conformance with development standards, and appropriate arrangement and design of the use of the site. FS-RM zoning is intended to implement the LUPP vision of landscaped suburban style development that provides for desirable apartment housing choices. The proposed development project is consistent with the Master Plan for use limitations and meets a community interest for providing apartment types of various floor plans from one to two bedrooms that will have wide appeal. The specific criteria for the site development plan approval are discussed in greater detail in the addendum.

The proposed building design is consistent with the common expectations of apartment homes marketed above an entry level product in that it includes some covered parking and private spaces for individual and the units are sized for smaller living situations. As a small site there is not unique amenities or characteristics to the project that have been included with recent FS-RM approvals on Mortenson or commitments for amenities on larger RH sites on South Duff.

The architectural appearance is similar to other designs that have been developed in Ames and Ankeny. The building design has a traditional apartment aesthetic in terms of exterior materials and architectural features. The overall massing of the proposed apartment buildings in this location is significantly smaller than the 36-unit apartment buildings proposed across the street in the RH zone portion of Village Park, and also smaller in scale than the 24-unit and 36-unit apartment buildings, close to this site, in the Wessex development to the north. Staff has worked with the applicant to provide landscaping on-site to meet front yard, and perimeter parking lot landscaping requirements. The arrangement of the site has a residential appearance and with the required street trees and apartment foundation plantings the development of all the sites along Cottonwood will create a substantially landscaped corridor.

Therefore, it is the City Manager's recommendation that the City Council adopt Alternative #1, thereby approving the Major Site Development Plan for the proposed 12-unit apartment building located at 2121 Cottonwood Road with conditions stated above.

ADDENDUM

Project Description. Hunziker Development Company, LLC is requesting approval of a Major Site Development Plan for a 12-unit apartment building on the property located at 2121 Cottonwood Road, (Lot 3 of Village Park Subdivision). Lot 3 includes 33,920.93 square feet (0.78 acres), is zoned as “FS-RM” (Suburban Residential Medium Density), and will have a public sidewalk (5 feet wide) for the entire frontage of the lot along Cottonwood Road (See *Attachment A: Location & Zoning Map & Attachment F: Village Park Subdivision Final Plat*).

Density limitations in the FS-RM zone requires 7,000 square feet for the first two units, and 1,800 square feet for each additional unit. This lot could accommodate 17 dwelling units, provided all other site plan requirements are met. The FS-RM zone allows a maximum of 12 units in each building, which is what is proposed for the entire lot. The proposed density of 12 units on 0.78 acres is the equivalent of 15.38 dwelling units per acre, which is within the density range on the approved Master Plan for Village Park Subdivision (See *Attachment H: Village Park Subdivision Master Plan*). The proposed 12-unit building consists of four (1-bedroom units), and eight (2-bedroom units), for a total of twenty bedrooms in the entire building.

Parking.

The Zoning regulations require a minimum of one parking space per bedroom for units of two bedrooms, or more, and one and one half parking spaces for one-bedroom units. The minimum number of parking spaces required for this site is twenty-two spaces. The developer is proposing a total of 34 parking spaces, including two handicap-accessible spaces and twelve garage stalls. Off-street parking on the site is configured as eight standard (9’x19’) surface parking spaces in the side yard, east of the building, and twelve standard surface parking spaces, plus two van-accessible handicap spaces, as required, in the rear yard behind the building. Across the drive aisle from the surface parking spaces in the rear yard, are two (6-stall) garages approximately 10.91 feet from the north property line, and running parallel to the north perimeter of the site. The parking spaces, and garage stalls, are served by a single access from Cottonwood Road, centered on the property line that separates Lots 2 and 3 in a 25-foot wide access easement. The shared access between Lots 2 and 3, also serves as a temporary emergency vehicle access that extends through the Wessex Apartment development adjacent to the north, and out to Oakwood Road, until such time that Cottonwood Road is extended to the west to connect with the existing Cottonwood Road in the Suncrest Subdivision (See *Attachment B: Master Plan, Parking Layout*).

Layout.

The building footprint of the apartment building occupies 4,205 square feet of the site. Garages footprints cover 3,168 square feet, and the parking spaces, driveways, drive aisles, and sidewalks cover an additional 12,640 square feet for a total of 20,013 square feet of the site covered by impervious surfaces, which is 59% of the total site area. The remainder of the lot is landscaped area, which amounts to 41% of the total site. In addition, the minimum requirement of 10% of the gross area of the subdivision to be

devoted to common open space, for development in the FS-RL, or FS-RM zones, is accounted for in Outlots A and D, of Village Park Subdivision. Outlots A and D are located in close proximity to the proposed apartment buildings, and the other future apartment buildings to be constructed in Village Park. The Village Park Subdivision includes 19.83 acres (863,794.80 square feet). The land area included in Outlots A and D, combined, is equal to 151, 038.74 square feet of land area, which is equal to 17.48% of the total land area in Village Park Subdivision (*See Attachment G: Landscape Plan for Outlots A & D, Village Park Subdivision*). Outlots A and D are located in close proximity to the proposed apartment buildings, and the other future apartment buildings to be constructed in Village Park. The outlots have been designed as stormwater detention and treatment areas and include one 10-foot trail extension. Review of the outlot landscape plan is separate from this project. There is no other common usable space included within the project.

Landscaping.

Landscaping requirements are driven by parking lot design and front yard plantings for apartment buildings (Page 18 of the report is the landscape plan). The proposed project's landscaping is most affected by the front yard foundation plantings due to the length of street frontage and the location of the parking. Due to the placement of garages along the north boundary line, screening is not required. The 10-foot area behind the garages is planned to be planted as turf. Screening would be needed if the garages did not abut an existing landscaped yard of the abutting Wessex apartments to meet the intent of the FS standards.

Front yard foundation plantings requirements include a combination of shrubs, understory and overstory trees. The developer has included additional cluster landscaping along the sides of building to add interest and some screening for the residents from the parking lot. Staff has an interest that an open relatively flat area be created between the apartment buildings of Lot 4 and Lot 3 to allow for informal open space. The area to the west of the building is planned as turf and can be graded with minimal slope to meet this interest of having available space for recreational use. There will be approximately 70 feet between buildings and 6000 square feet of informal open space between the buildings on Lots 3 and 4.

The Landscape Plan shows screening for mechanical units along the east and west sides of the building that meet site plan standards, provided the shrubs used for screening are a minimum of six inches higher than the mechanical units, at the time of planting. A condition is included in the report as a reminder of the need to comply with screening at the time of planting compared to growing into a screen over time.

No interior parking lot landscaping is required due to the size of the parking lot with one drive aisle. Perimeter parking lot landscaping is required along the portions of the driveways, parking spaces, and drive aisles to meet perimeter parking lot landscaping. All but one area of the site has the required perimeter parking lot landscaping. The side of the parking space that faces the property at 2135 Cottonwood, to the west, requires five shrubs to meet the "L2"(Low Screen) landscaping standards. The developer does

not believe landscaping is needed at the terminus of this drive aisle as it abuts another drive aisle. The City Council can approve modification of landscaping screening requirements if it finds that an alternative design meets the objectives for parking lot screening. Staff finds that the minimal length of area could be planted, but the overall mix of plants has diversity and interest in the design while meeting the essence of screening objectives of the zoning ordinance (*See Attachment B: Major Site Development Plan, Landscaping- pg 18*).

The trash receptacles are screened with 1" by 6" vertical cedar wood alternating slats to a height of six feet. The site plan does not include gates for the enclosure. Gates are necessary to meet screening requirements in Section 29.408(3) of the Municipal Code, which requires that garbage collection areas be screened from all public rights-of-way and from adjacent properties.

Circulation.

Site access is from one driveway off Cottonwood Road. Cottonwood Road is planned to be extended west to connect to the existing segment of Cottonwood Road, in the Suncrest Subdivision, once the land between Village Park Subdivision and Suncrest Subdivision is annexed into the City and developed as residential. Public sidewalks (5-foot wide) in the right-of-way for Cottonwood Road provide a connection to the shared use path (10-foot wide) that will cross through the site between University Boulevard and the southern boundary of Village Park Subdivision. The shared use path will connect to the Ames community bike trail/shared use path system, and is planned in the future to connect to the regional multi-county bike trail system south of Ames (*See Attachment G: Landscape Plan for Outlots A & D, Village Park Subdivision*).

Building Design.

The overall footprint of the proposed apartment building is approximately 64 feet by 69 feet. Minimum building front, side and rear yard setbacks are all met by the proposed placement of the apartment building on the lot. The detached garage structures are allowed to be as close as 3 feet to the rear property line. The site plan shows the garage structures 10.91 feet from the north property line (*See Attachment B: Major Site Development Plan*).

The 12-unit building is consistent with the maximum building size allowed in the FS-RM zoning district. The building is three stories in height. Four stories, or 50 feet, whichever is lower, is the maximum building height permitted for multiple-family dwellings in the FS-RM zone. Ten of the twelve apartment units have access to either a small patio or a balcony. The 2-bedroom units, on the second and third floors of the northeast corner of the building, do not have balconies. The apartment building design incorporates a 5/12 pitched gable roof design, and asphalt shingles.

Two primary exterior materials are proposed for the building. The majority of the first floor is to be surfaced with cultured stone, while the second and third floors, and the gables are primarily vinyl lap siding. Cultured stone will extend to the roof line on approximately one-third of the south façade, and to the top of the first floor level at the

primary entrance to the building. The primary (south) building entrance will include a 5/12 gabled roof above the entrance, as will also be a design feature above each of the decks on the second and third levels of the building. Decks will be constructed of treated lumber with aluminum guardrails, and supported by pillars of cultured stone. The two (6-stall) garage structures will have similar materials and design features with a band of cultured stoned at the bottom of each façade, vinyl lap siding on the remainder, metal overhead garage doors, and a gabled roof with asphalt shingles and a roof pitch of 6/12. The cultured stone is a dark brown color, and the vinyl lap siding is a lighter brown color with two different variations of tan shading. Each façade has some variation in wall plane to add interest and provide some relief in the architecture (See *Attachment C: Building Elevations & Attachment D: Garage Elevations*).

Major Site Development Plan Criteria. Additional criteria and standards for review of all Major Site Development Plans are found in Ames *Municipal Code* Section 29.1502(4)(d) and include the following requirements.

When acting upon an application for a Major Site Development Plan approval, the Planning and Zoning Commission and the City Council shall rely upon generally accepted site planning criteria and design standards. These criteria and standards are necessary to fulfill the intent of the Zoning Ordinance, the Land Use Policy Plan, and are the minimum necessary to safeguard the public health, safety, aesthetics, and general welfare.

- 1. The design of the proposed development shall make adequate provisions for surface and subsurface drainage to limit the rate of increased runoff of surface water to adjacent and downstream property.***

The Public Works Department has reviewed the storm water management plan and finds that the proposed development has met the required storm water quantity and quality measures by use of the proposed regional detention facilities on the southeast and northeast areas of the site.

- 2. The design of the proposed development shall make adequate provision for connection to water, sanitary sewer, electrical, and other utility lines within the capacity limits of those utility lines.***

The existing utilities were reviewed and found adequate to support the anticipated load of 12 dwelling units on this lot, comprising 20 bedrooms, consistent with prior determination at the time of rezoning and subdivision approval.

- 3. The design of the proposed development shall make adequate provision for fire protection through building placement, acceptable location of flammable materials, and other measures to ensure fire safety.***

The fire inspector has reviewed access and fire truck circulation, sprinkler, and hydrant requirements and found that the requirements of the Fire Department are met. The

subject site is also conditioned on provision of emergency access through Wessex to the north.

- 4. The design of the proposed development shall not increase the danger of erosion, flooding, landslide, or other endangerment to adjoining and surrounding property.***

It is not anticipated that this proposed development will be a danger due to its site location and proximity to other uses.

- 5. Natural topographic and landscape features of the site shall be incorporated into the development design.***

The site is currently devoid of any significant vegetation. Minimal grading will occur for the construction of the buildings. The site is relatively flat and grading will occur mostly to direct storm water where required.

- 6. The design of the interior vehicle and pedestrian circulation shall provide for convenient flow of vehicles and movement of pedestrians and shall prevent hazards to adjacent streets or property.***

Access to this site from Cottonwood Road is through one access point at the southeast corner of the property. Vehicular and pedestrian access is accommodated between the subject property, and other properties (buildings) within the Village Park subdivision. The on-site sidewalks, sidewalks in the public street right-of-ways, and shared use paths provide pathways throughout the site, and to external connections to allow for circulation throughout the community, and eventually within the Central Iowa region.

- 7. The design of outdoor parking areas, storage yards, trash and dumpster areas, and other exterior features shall be adequately landscaped or screened to minimize potential nuisance and impairment to the use of adjoining property.***

The general development standards of the zoning ordinance have been met for the screening of parking areas. Gates are needed on the trash enclosure to comply with the zoning ordinance standards for screening garbage collection areas. The enclosure standard requires full fenced screening with a gate to ensure the compliance with the standards. In this plan the design has a large opening for rollout of the dumpster with no gate to obscure or secure the dumpster. The apartment building provides a high degree of separation and screening of the parking and garages in the rear yard from adjoining views, and as viewed from Cottonwood Road.

- 8. The proposed development shall limit entrances and exits upon adjacent streets in order to prevent congestion on adjacent and surrounding streets and in order to provide for safe and orderly vehicle movement.***

A shared access is planned for the subject property (Lot 3), and the adjacent property to the easement (Lot 2), to minimized the number of curb cuts onto Cottonwood Road.

9. Exterior lighting shall relate to the scale and location of the development in order to maintain adequate security, while preventing a nuisance or hardship to adjacent property or streets.

Additional information is needed on the proposed exterior lighting, to determine compliance, prior to the occupancy of the apartment building. The proposed lighting must meet the lighting standards, found in Sec 29.411 of the *Municipal Code*.

10. The proposed development shall ensure that dust and other forms of air pollution, noise disturbances, odor, glare, and other nuisances will be limited to acceptable levels as prescribed in other applicable State and City regulations.

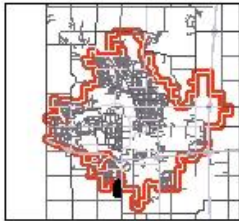
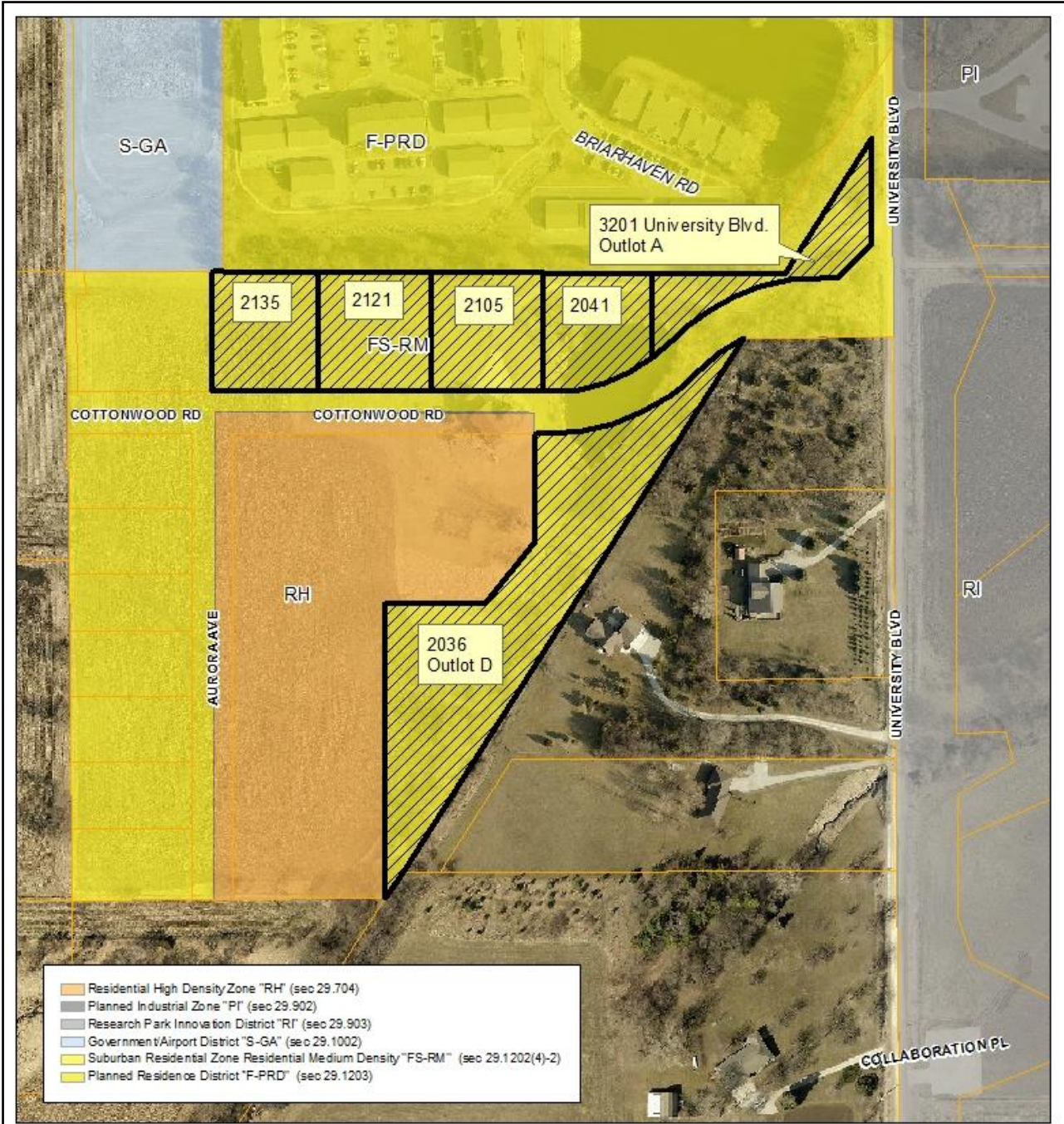
The proposed residential uses are not expected to exceed the level of nuisances, typical of this type of development, beyond acceptable levels, as prescribed in other applicable State and City regulations.

11. Site coverage, building scale, setbacks, and open spaces shall be in proportion with the development property and with existing and planned development and structures, in adjacent and surrounding property.

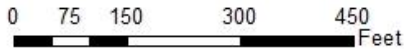
The scale of the proposed apartment building, in relation to the lot size, is appropriate, given minimum building setback requirements, landscaping requirements, and areas of the site planned for parking. The developer has proposed the maximum number of units (12) in a multiple family structure within the FS-RM zoning district. The density of development is well within the approximate densities found on the approved Master Plan for this subdivision, and the apartment appearance is compatible with the character and scale of the general surrounding with the proposed 36-unit apartment dwellings on the other side of Cottonwood Road, and the 24-unit and 36-unit buildings in the Wessex development adjacent to the north. Open areas and landscaped areas meet the quantitative standards of the zoning code and allow for informal activities by the future residents of this subdivision.

The Development Review Committee has reviewed the Major Site Development Plan and found that it complies with all other requirements of the Ames Municipal Code.

Attachment A: Location & Zoning Map



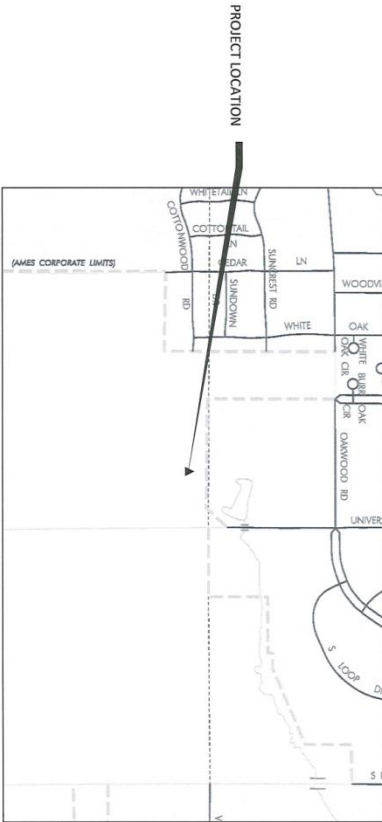
Location & Zoning Map
2036, 2041, 2105, 2121, 2135
Oakwood Rd. & 3201 University Blvd.



Attachment B: Major Site Development Plan (Title Sheet)

GOVERNING SPECIFICATIONS
 HUNZIKER URBAN SPECIFICATIONS FOR PUBLIC IMPROVEMENTS AND THE CITY OF AMES SUPPLEMENTAL SPECIFICATIONS SHALL GOVERN.
 IOWA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" SHALL GOVERN AS REFERENCED.
 ALL APPLICABLE FEDERAL, STATE, AND LOCAL LAWS AND ORDINANCES WILL BE COMPLIED WITH IN THE CONSTRUCTION OF THIS PROJECT.
 MATCO 2009 AS ADOPTED BY IOWA DEPARTMENT OF TRANSPORTATION.

HUNZIKER DEVELOPMENT CO. CONSTRUCTION PLANS FOR **2121 COTTONWOOD ROAD** LOT 3 - VILLAGE PARK SUBDIVISION MAJOR SITE DEVELOPMENT PLANS SEPTEMBER 2016



NOTE: EXISTING UTILITY INFORMATION SHOWN ON THIS PLAN HAS BEEN PROVIDED BY THE UTILITY OWNER. THE UTILITY OWNER SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION AS REQUIRED BY STATE LAW. NOTIFY IOWA ONE CALL 1-800-292-8989

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS UTILITY QUALITY LEVEL WAS OBTAINED FROM A UTILITY RECORD DRAWING FOR THE COLLECTION AND DEPTH OF EXISTING SUBSURFACE UTILITY DATA.

DATE	SHEET NUMBER	APPROVED BY	MODIFICATIONS

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HUNZIKER ASSOCIATES REALTORS®
 2224 GARDEN ST., SUITE 100
 AMES, IOWA 50010
 (515) 271-1111
 www.hunziker.com

BOLTON & MENK
 2224 GARDEN ST., SUITE 100
 AMES, IOWA 50010
 (515) 271-1111
 www.boltonmenk.com

OWNER
 HUNZIKER DEVELOPMENT CO.
 105 S. 10TH STREET
 AMES, IA 50010

SHEET INDEX	
1	TITLE SHEET
2	GENERAL NOTES
3	GRADING & STORM SEWER
4	PARKING LAYOUT
5	UTILITIES
6	LANDSCAPING
7	LANDSCAPE DETAILS

OWNER:
 HUNZIKER DEVELOPMENT COMPANY, LLC
 105 S. 10TH STREET
 AMES, IA 50010

APPLICANT:
 HUNZIKER DEVELOPMENT COMPANY, LLC
 105 S. 10TH STREET
 AMES, IA 50010

PLAN PREPARED BY:
 BOLTON & MENK, INC.
 P.O. BOX 688
 2224 GARDEN STREET
 AMES, IA 50010

LEGAL DESCRIPTION:
 LOT 3 OF VILLAGE PARK SUBDIVISION, 1ST ADDITION TO THE CITY OF AMES, STORY COUNTY, IOWA.

ZONING DESIGNATION:
 SUBURBAN RESIDENTIAL MEDIUM DENSITY (S-AM)

PROPOSED LAND USE:
 ADVERTISING BUILDINGS

MINIMUM BUILDING SETBACKS:
 FRONT: 10 FT
 SIDE: 10 FT
 REAR LOT LINE: 25 FT

PRELIMINARY

LICENSED PROFESSIONAL ENGINEER
 CATEGORY 1
 NUMBER 1530
 EXPIRES 12/31/2017

HUNZIKER CERTIFIES THAT THE INFORMATION CONTAINED ON THESE PLANS WAS PREPARED BY THE ENGINEER OR UNDER HIS CLOSE PERSONAL SUPERVISION AND THAT HE OR SHE IS A LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

DATE: DECEMBER 14, 2017
 PROJECT OR SHEET COVERED BY THIS SEAL: 2121 COTTONWOOD ROAD, LOT 3

PROJECT:	HUNZIKER DEVELOPMENT CO.
LOCATION:	2121 COTTONWOOD ROAD
TITLE:	TITLE SHEET
SHEET:	1

Attachment B: Major Site Development Plan (General Notes)

GENERAL NOTES

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE URBAN STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENTS AND I.D.O.T. STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, SERIES 2016 (WHERE NOTED ON PLANS), AND CITY OF AMES SUPPLEMENTAL SPECIFICATIONS.
- THE PLANS SHOW APPROXIMATE LOCATIONS OF EXISTING UTILITIES LOCATED WITHIN THE LIMITS OF THE PROJECT. THE COMPLETENESS OR ACCURACY OF ANY INFORMATION IS NOT GUARANTEED. THE CONTRACTOR SHALL OBTAIN UTILITY LOCATE INFORMATION FROM THE OWNERS OF THOSE UTILITIES PRIOR TO BEGINNING ANY EXCAVATION AND WILL BE RESPONSIBLE FOR ANY DAMAGE TO SAID UTILITIES RESULTING FROM THEIR OPERATIONS.
- CONTRACTOR OPERATIONS EXCEEDING OBJECTABLE LEVELS OF NOISE SHALL NOT BEGIN PRIOR TO 7:00 AM AND SHALL END PRIOR TO 7:00 PM.
- ALL UTILITY SERVICES (GAS, WATER, AND SANITARY) SHALL BE INSTALLED BY A LICENSED LICENSED BY THE CITY OF AMES.
- ALL UTILITY TRENCHES SHALL BE CAREFULLY BACKFILLED IN 6-IN. LFTS, COMPACTED TO 95% OF MAXIMUM STANDARD PROCTOR DENSITY. THE CONTRACTOR SHALL UNDERSTAND AND FOLLOW THE REQUIREMENTS OF THE MOHS GENERAL PERMIT NO. 2 AND THE STORM WATER DISTURBED AREAS SHALL BE SEDED AND/OR MULCHED (DEPENDING ON DATE) UPON COMPLETION OF GRADING.
- THE CONTRACTOR SHALL NOTIFY THE PUBLIC WORKS DEPARTMENT, THE DEVELOPER AND I.D.O.T. & M.E.N.K. INC. ONE WEEK PRIOR TO THE START OF ANY CONSTRUCTION.
- THE CONTRACTOR SHALL INSTALL UNDERGROUND UTILITIES IN THE FOLLOWING ORDER:
 - STORM SEWER
 - WATER MAIN
 - WATER MAIN
- IF ANY OTHER SEQUENCE IS APPROVED BY THE CITY'S MUNICIPAL ENGINEER, THE CONTRACTOR SHALL NOTIFY THE DEVELOPER AND I.D.O.T. & M.E.N.K. INC. PRIOR TO THE START OF ANY CONSTRUCTION.
- ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS, OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC RIGHT-OF-WAY.
- NO OUTROOM LIGHTING SHALL BE INSTALLED ON THE BUILDING OR SITE UNTIL THE LIGHTING HAS BEEN APPROVED BY THE CITY OF AMES.
- LOT DIMENSIONS SHOWN ARE PLATTED DIMENSIONS FROM VILLAGE PARK SUBDIVISION 1ST ADDITION FINAL PLAT.
- ALL SERVICE CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY OF AMES REQUIREMENTS.
- ALL SPOT ELEVATIONS SHOWN ARE TOP OF SLAB UNLESS OTHERWISE SPECIFIED AS FORM GRADE (FG).

GRADING NOTES

- ALL GRADING OPERATIONS SHALL BE COMPLETED IN ACCORDANCE WITH DIVISION 2 OF THE SPECIFICATIONS.
- THE PROPOSED CONTOURS ARE SHOWN TO FINISH GROUND/TOP OF PAVEMENT.
- CONTRACTOR SHALL GRADE ALL AREAS IN THE PAVEMENT BOX TO FORM GRADE AND ALL OTHER AREAS TO FINISH GROUND.
- CUT = 80 CY
FILL = 30% = 1402 CY
- AREAS WITHIN THE RIGHT-OF-WAY SHALL BE COMPACTED TO NO LESS THAN 95% OF MAXIMUM STANDARD PROCTOR DENSITY AT A MOSTURE PERCENTAGE OF 10%.
- AREAS OUTSIDE THE RIGHT-OF-WAY SHALL BE FINAL GRADED TO WITHIN 0.2' OF PLAN GRADE. AREAS WITHIN THE RIGHT-OF-WAY SHALL BE FINAL GRADED TO WITHIN 0.1' OF PLAN GRADE.
- AREAS OUTSIDE OF THE PAVEMENT BOX SHALL HAVE A MINIMUM TOP SOIL DEPTH OF 6".
- TOPSOIL AND/OR OTHER UNSUITABLE MATERIAL WITHIN THE PAVEMENT BOX SHALL BE REMOVED.
- TOPSOIL MAY BE USED FOR FILL UNDER 4" OUTSIDE OF THE PAVEMENT BOX.
- THE CONTRACTOR SHALL SHAPE GRADED AREAS TO MAINTAIN SURFACE DRAINAGE AT ALL TIMES.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN AND MAINTAIN POSITIVE DRAINAGE CONTROL PLAN LOCATED ON SITE.
- TOPSOIL STRIPPING AND RESPAVING IS INCIDENTAL AND WILL NOT BE PAID FOR SEPARATELY.
- GRADING INCLUDES FINAL GRADING FOR FUTURE STREETS AND LOTS AS SHOWN.
- DRAINAGE (IF NECESSARY) IS CONTRACTOR'S RESPONSIBILITY AND SHALL BE CONSIDERED INCIDENTAL.
- GRADING SHALL BE COMPLETED TO WITHIN 0.2' OF FINISH GRADE OUTSIDE OF THE PAVEMENT BOX SHALL HAVE A MINIMUM 6" TOPSOIL LAYER. NO ADDITIONAL PAVEMENT WILL BE MADE FOR TOPSOIL STRIPPING/RESPAVING.

SANITARY SEWER NOTES

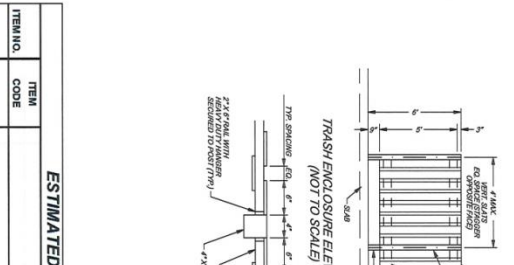
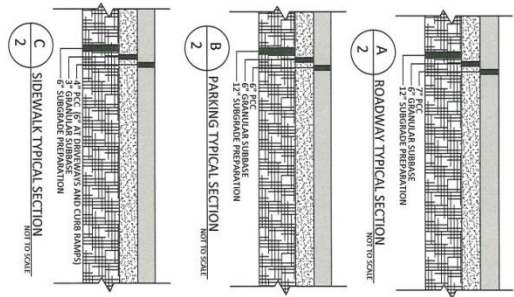
- INSTALLATION SHALL BE COMPLETED IN ACCORDANCE WITH DIVISIONS 3, 4 AND 6 OF THE SPECIFICATIONS.
- PRIOR TO INSTALLING PIPE, THE CONTRACTOR SHALL PLUG THE DOWNSTREAM CONNECTION POINTS AND LEAVE PLUGGED UNTIL NEW SEWER PIPE AND MANHOLE CASTINGS ARE IN PLACE. ALL WATER SHALL BE PUMPED OUT AND THE SEWER SHALL BE CLEAN BEFORE REMOVING PLUGS.

WATER MAIN NOTES

- WATER MAIN INSTALLATION SHALL BE COMPLETED IN ACCORDANCE WITH DIVISIONS 3 AND 5 OF THE SPECIFICATIONS.
- 1.40CTS USED FOR CONNECTING WATER PIPE AND FITTINGS SHALL BE CON-BLUE, AS MANUFACTURED BY NCS INDUSTRIES, OR APPROVED EQUAL.
- WATER VALVE BOXES SHALL BE CAST IRON IN ACCORDANCE WITH SECTION 509.0 OF THE SPECIFICATIONS. OR TRENCH ADAPTOR (AS VALVE OPERATING NUT WITHIN 2 FEET OF PROPOSED GRADE).
- METAL WATER MAIN FITTINGS AND METAL ACCESSORIES SHALL BE WRAPIED WITH 8 MIL POLYETHYLENE SHEETING AND SECURED WITH TAPE.
- WATER MAIN SHALL BE INSTALLED WITH 5.5 FEET COVER (MINIMUM).
- WATER MAINS SHALL BE CHROMIATED BY THE CONTRACTOR IN ACCORDANCE WITH CITY OF AMES REQUIREMENTS.
- WATER MAIN VALVES SHALL BE OPERATED BY CITY OF AMES PERSONNEL ONLY.

PAVING NOTES

- PAVING SHALL BE COMPLETED IN ACCORDANCE WITH DIVISION 1 OF THE SPECIFICATIONS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS.
- POST-TENSION SHAPES SHALL BE CONSTRUCTED BY THE CURB AND GUTTER CONTRACTOR AND SHALL INCLUDE "DETECTABLE WARNING" TEXTURE.
- ALL UTILITY FITTINGS (WATER VALVE BOXES, MANHOLES, AND STORM SEWER IN-TAKES) SHALL BE SET TO THEIR CORRESPONDING PAVEMENT ELEVATIONS.



ESTIMATED PROJECT QUANTITIES

ITEM NO.	ITEM CODE	ITEM	UNIT	TOTAL	AS BUILT QUANT
DIVISION 2 - EXCAVATION					
1	200.000.0	TOPSOIL, ON-SITE	CY	322	
2	200.000.0	EXCAVATION, CLASS 13	CY	1,402	
3	200.000.0	6\"/>			

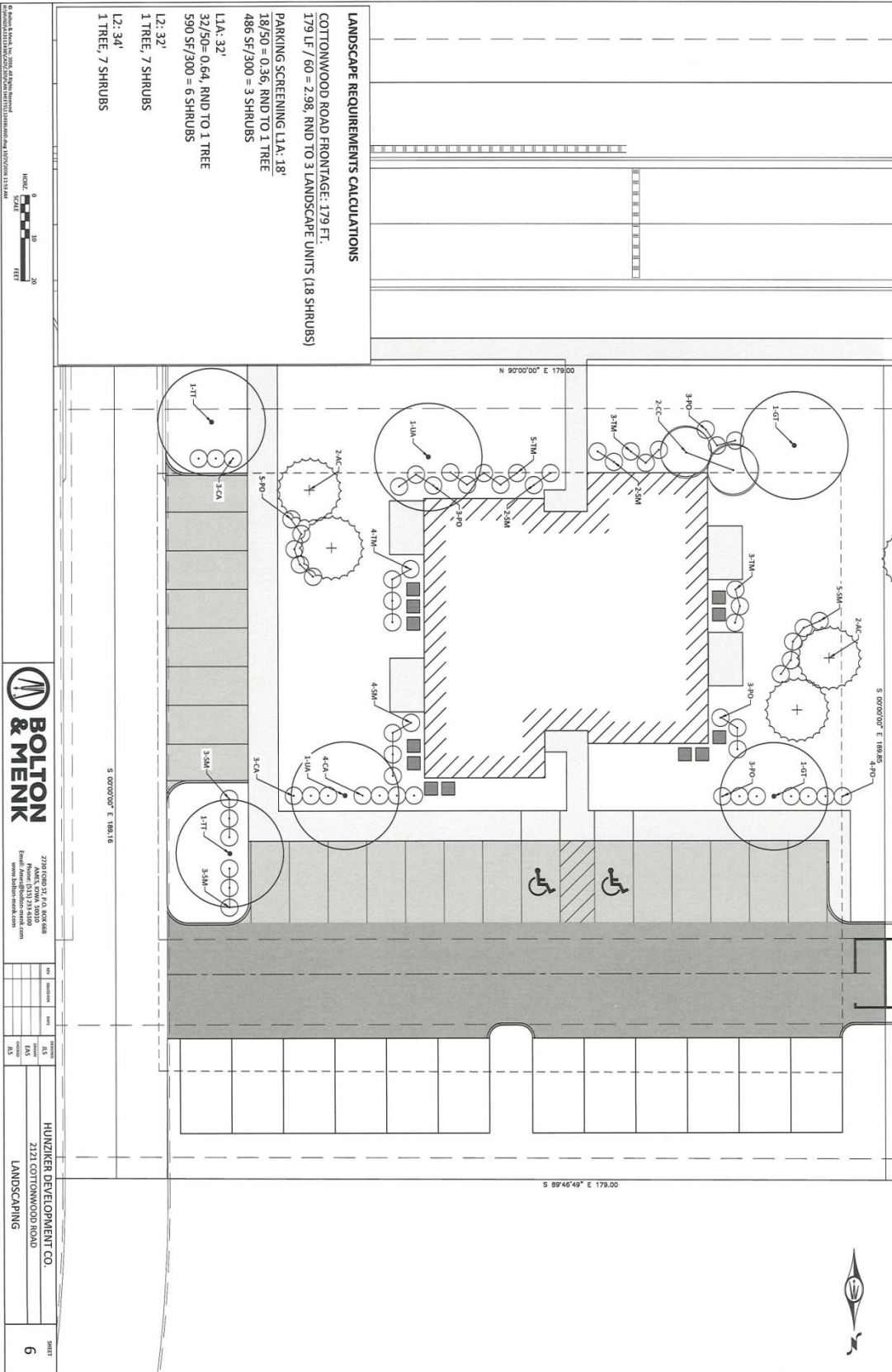


2211 COTTONWOOD ROAD
AMES, IOWA 50010
www.boltonandmenk.com

HUNZIKER DEVELOPMENT CO.
2211 COTTONWOOD ROAD
GENERAL NOTES

SHEET 2

Attachment B: Major Site Development Plan (Landscaping)



Attachment B: Major Site Development Plan (Landscape Details)

MASTERS PLANT SCHEDULE - TREES & SHRUBS

On-Site Schedule Name	Common Name	Unit Size	Spacing	Characteristics
LOT 1 PLANT SCHEDULE				
2.1T	Milliornotus	16, 1.5' DBH	see plan	
2.1M	Ulmus americana 'Yuccifolius'	16, 1.5' DBH	see plan	
2.1T	Quercus macrocarpa 'Imperial Spycop'	16, 1.5' DBH	see plan	
2.1C	Empetrum & Hesperis Trees	16, 7' DBH	see plan	
4.1C	Alnus 'Corymbosa'	16, 7' DBH	see plan	
Shrub				
13.1A	Cornus alba 'Variegata'	16, 3' dia.	see plan	
13.1M	Syringa pubescens 'Subcordata'	16, 3' dia.	see plan	
13.1T	Prunella 'Moulin Rouge'	16, 3' dia.	see plan	
13.1M	Hydrangea 'Nancy' var. 'White'	16, 3' dia.	see plan	
13.1M	Yucca 'Sword' 'Dante's Blush'	16, 3' dia.	see plan	

GENERAL NOTES: LANDSCAPE PLANTING

- MASTER PLANT SCHEDULE ALL TREES AND SHRUBS ARE LISTED IN THE MASTER PLANT SCHEDULE. IF THERE IS A CONFLICT BETWEEN THE SCHEDULE AND THE DRAWINGS AND THE QUANTITIES SHOWN IN THE PLAN SCHEDULE, THE PLAN QUANTITIES SHALL PREVAIL.
- PLANTING LOCATIONS, STAKE ALL TREE LOCATIONS AND A TYPICAL SHRUB BED LAYOUT AND OBTAIN APPROVAL OF THE OWNER'S REPRESENTATIVE PRIOR TO PLANTING.
- SHRUBS AND SHRUBS SHALL BE PLANTED AND MULCHED IN ONE CONTINUOUSLY MULCHED BED. TREES LOCATED IN THIRF AREAS SHALL BE MULCHED TO 18 INCHES FROM TRUNK (FOR "DIA BINS") IN ALL DIRECTIONS AND DEPTH TO 3".
- MULCHING SOIL MIX AND SHRUBBED HARDWOOD MULCH SHALL BE INCIDENTAL TO THE PLANTING DETAILS AND SHALL BE INSTALLED AS PER SPECIFICATIONS.
- PLANT MATERIAL PROTECTIVE SCREENERING FOR THE MECHANICAL UNITS MUST BE AT LEAST 6" HIGHER THAN THE MECHANICAL UNITS AT THE TIME OF PLANTING.

2 TREE PLANTING DETAIL
SCALE: N.T.S.

DO NOT CUT LEAVES

REMOVE TOP 1/2 OF TREE BUCKET FROM THE TRUNK & FILL BURLAP FROM THE TRUNK 1" TO 2" ABOVE SURROUNDING FINISH GRADE

REMOVE TOP 1/2 OF TREE BUCKET FROM THE TRUNK & FILL BURLAP FROM THE TRUNK 1" TO 2" ABOVE SURROUNDING FINISH GRADE

4" MULCH DEPTH FULL BACK FROM TREE TRUNK

4" TREE TAP ON EXTERIOR

USE 3 GUY ASSEMBLIES FOR EXTERIOR

STEEL POST NOTCHED OR SHIMMED FOR SOIL WIND

SOIL BACKFILL PER SPECIFICATIONS

3"-0" GUY HEIGHT

4"-0" POST HEIGHT

5 PLANTING EDGE AT EXISTING CONC. WALK
SCALE: N.T.S.

1" BETWEEN TOP OF WALK AND TOP OF MULCH

PROVIDE 6" BETWEEN ARMORED SOIL EXCAVATION AND EXISTING WALK EDGE

3" DEPTH

PREPARED PLANTING BED SOIL

HARDWOOD MULCH

1 PLANTING ON SLOPES
SCALE: N.T.S.

EXISTING GRADE

SOIL RIDGE TO HOLD WATER IN BASIN

SOIL BACKFILL PER SPECIFICATIONS

ON 2:1 SLOPES OR GREATER, DO NOT CONSTRUCT THE UPPER HALF OF THE WATERING BASIN.

6 TYPICAL SECTION: SHRUB/PERENNIAL BED PLANTING
SCALE: N.T.S.

SHRUB/PERENNIAL BED

HARDWOOD MULCH

ADJACENT LAWN

NOTES:
SPRINKLER DUG EDGE TO BE INCIDENTAL TO PLANT MATERIAL COSTS AND SHALL BE LISTED ON ALL PLANTING DETAILS.
SPRINKLER BEDS SHALL BE SHOWN ON THE PLANTING PLANS.

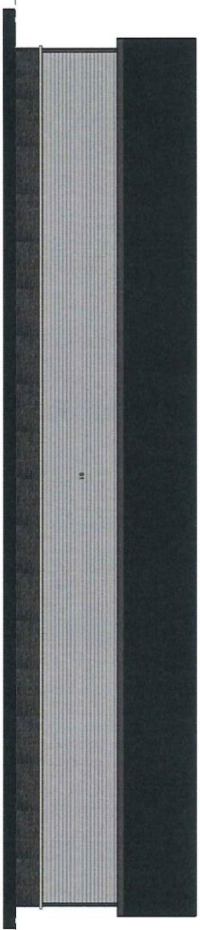
BOLTON & MENK

27390 ROAD 51, P.O. BOX 668
MANKATO, MN 56001
Phone: 507.333.3333
Fax: 507.333.3333
www.boltonandmenk.com

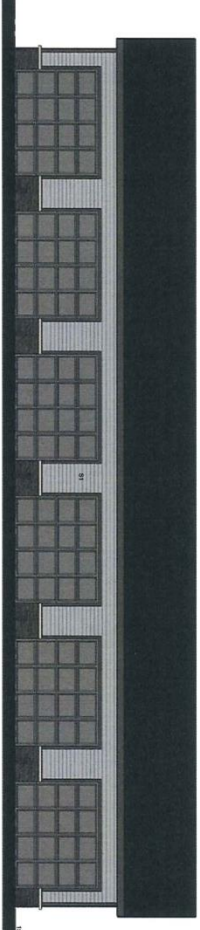
HUNZIKER DEVELOPMENT CO.
2121 COTTONWOOD ROAD
LANDSCAPE DETAILS

SHEET
7

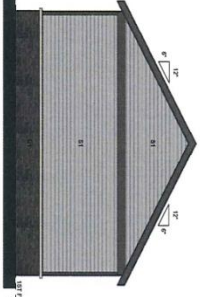
Attachment D: Garage Elevations



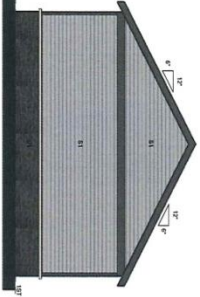
2 NORTH ELEVATION
SCALE: 1/4" = 1'-0"



4 SOUTH ELEVATION
SCALE: 1/4" = 1'-0"



1 WEST ELEVATION
SCALE: 1/4" = 1'-0"



3 EAST ELEVATION
SCALE: 1/4" = 1'-0"

PRELIMINARY- NOT FOR CONSTRUCTION

A5000

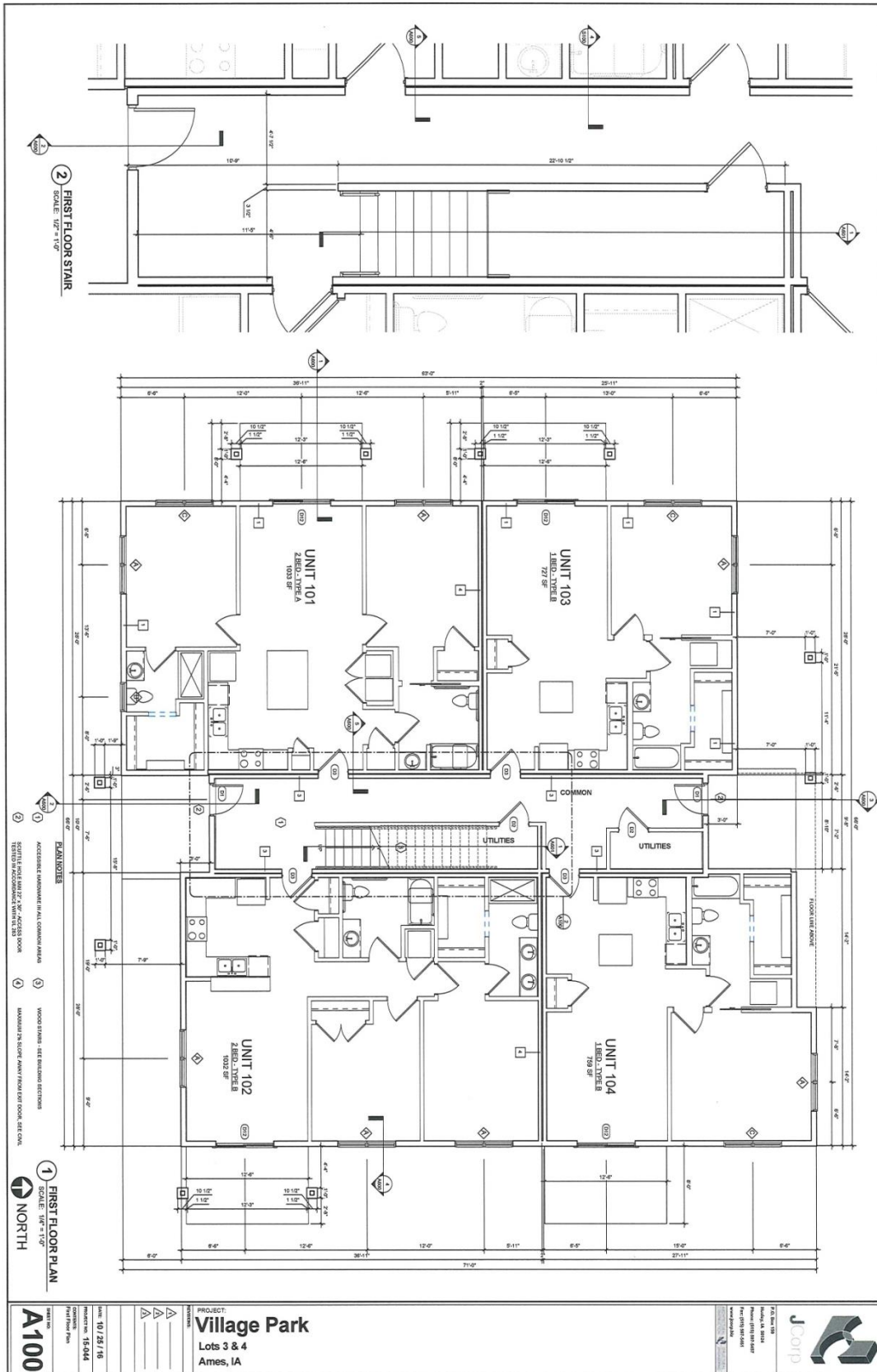
PROJECT:
Village Park Garages
Lots 3 & 4
Ames, IA

JCorp

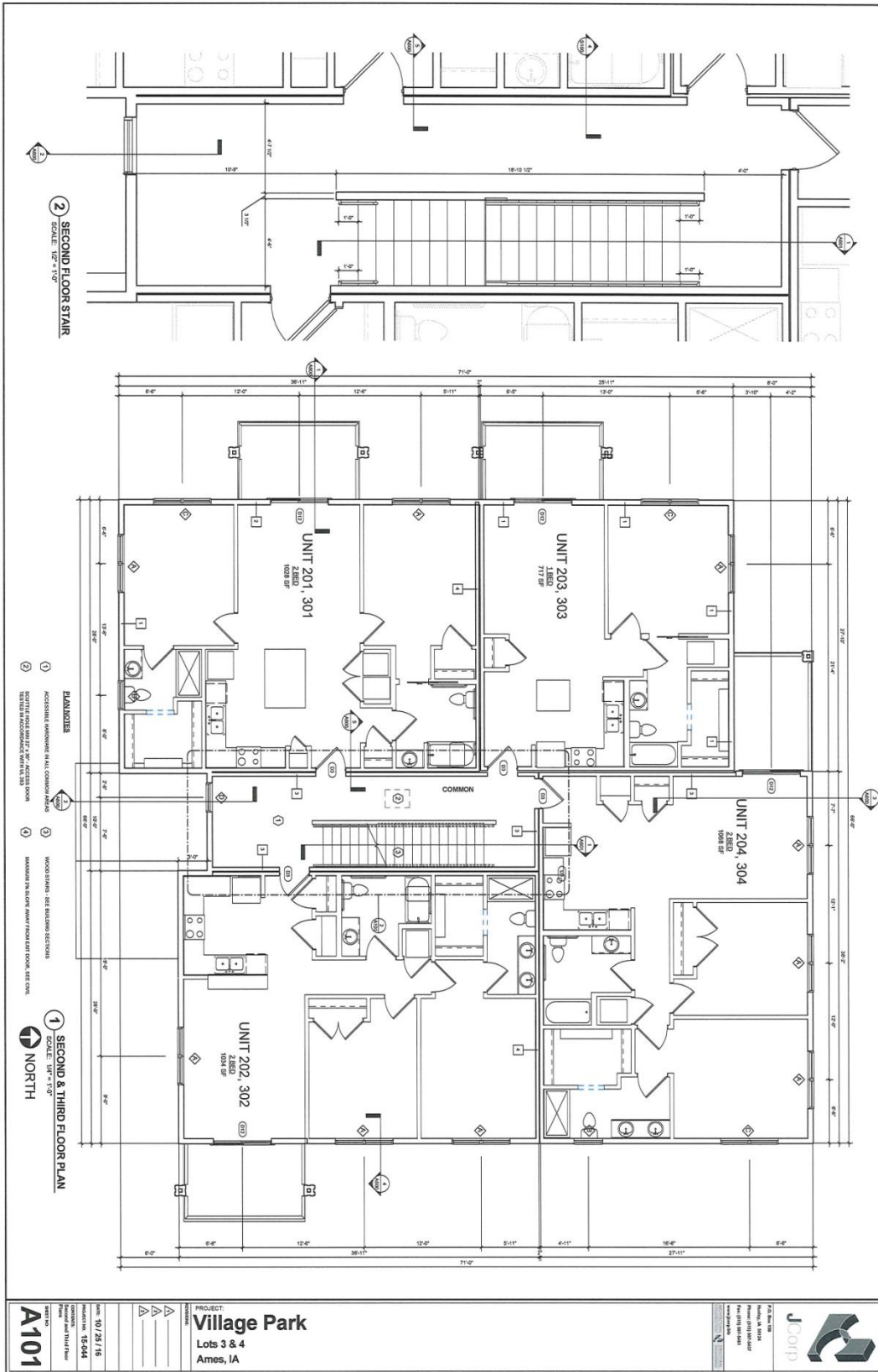
720 East 10th
Ames, IA 50010
Phone: 515.339.4444
Fax: 515.339.4444
www.jcorp.com

DATE: 10/23/18
DRAWING NO: 18-044

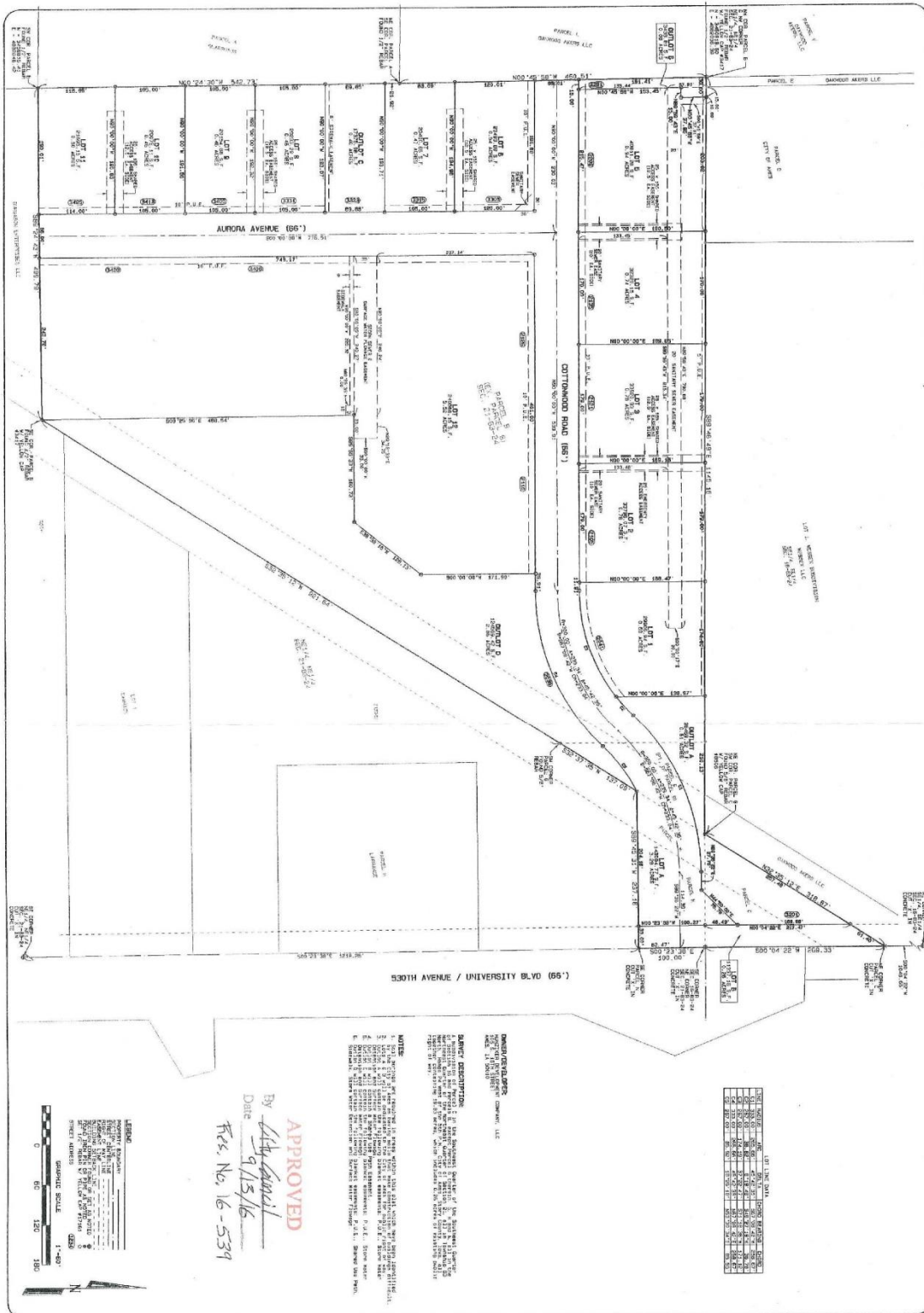
Attachment E: Floor Plans (First Floor)



Attachment E: Floor Plans (Second & Third Floor)



Attachment F: Village Park Subdivision Final Plat



LEGEND

PROPERTY BOUNDARIES
EXISTING EASEMENTS
EXISTING UTILITIES
EXISTING CONCRETE DRIVEWAYS
EXISTING DRIVEWAYS
EXISTING SIDEWALKS
EXISTING CURBS
EXISTING STREETS
EXISTING ALLEYS
EXISTING FENCES
EXISTING TREES
EXISTING ROCKS
EXISTING STRUCTURES
EXISTING UTILITIES
EXISTING CONCRETE DRIVEWAYS
EXISTING DRIVEWAYS
EXISTING SIDEWALKS
EXISTING CURBS
EXISTING STREETS
EXISTING ALLEYS
EXISTING FENCES
EXISTING TREES
EXISTING ROCKS
EXISTING STRUCTURES

GRAPHIC SCALE: 1"=40'

0 50 100 150

APPROVED

By *Ashley Carroll*

Date *9/15/16*

Fax No. 16-539

NOTES:

1. THIS FINAL PLAT IS THE RESULT OF A SURVEY CONDUCTED BY STUMBO & ASSOCIATES LAND SURVEYING, INC. (S&A) ON BEHALF OF THE SUBDIVIDER, PARCEL C IN THE SE1/4, SE1/4 OF SEC. 16-B3-24, 5 PARCELS B, G, M & N IN THE NE1/4, NE1/4 OF SEC. 21-03-24, CITY OF AMES, STORY COUNTY, IOWA.
2. THE SUBDIVIDER HAS REVIEWED THIS FINAL PLAT AND HAS APPROVED IT AS ACCURATE AND COMPLETE.
3. THE SUBDIVIDER HAS REVIEWED THIS FINAL PLAT AND HAS APPROVED IT AS ACCURATE AND COMPLETE.
4. THE SUBDIVIDER HAS REVIEWED THIS FINAL PLAT AND HAS APPROVED IT AS ACCURATE AND COMPLETE.
5. THE SUBDIVIDER HAS REVIEWED THIS FINAL PLAT AND HAS APPROVED IT AS ACCURATE AND COMPLETE.
6. THE SUBDIVIDER HAS REVIEWED THIS FINAL PLAT AND HAS APPROVED IT AS ACCURATE AND COMPLETE.

SUBJECT DESCRIPTION:

PARCEL C IN THE SE1/4, SE1/4 OF SEC. 16-B3-24, 5 PARCELS B, G, M & N IN THE NE1/4, NE1/4 OF SEC. 21-03-24, CITY OF AMES, STORY COUNTY, IOWA.

DATE OF SURVEY: 8/15/16

DATE OF PLAT: 9/15/16

LOT 15 AREA

AREA	ACRES
15.1	0.10
15.2	0.10
15.3	0.10
15.4	0.10
15.5	0.10
15.6	0.10
15.7	0.10
15.8	0.10
15.9	0.10
15.10	0.10
15.11	0.10
15.12	0.10
15.13	0.10
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15.97	0.10
15.98	0.10
15.99	0.10
15.100	0.10

STUMBO & ASSOCIATES
LAND SURVEYING
 510 S. 17TH STREET, SUITE 102, AMES, IOWA 50010
 PHONE 515-262-5369 FAX 515-263-4403

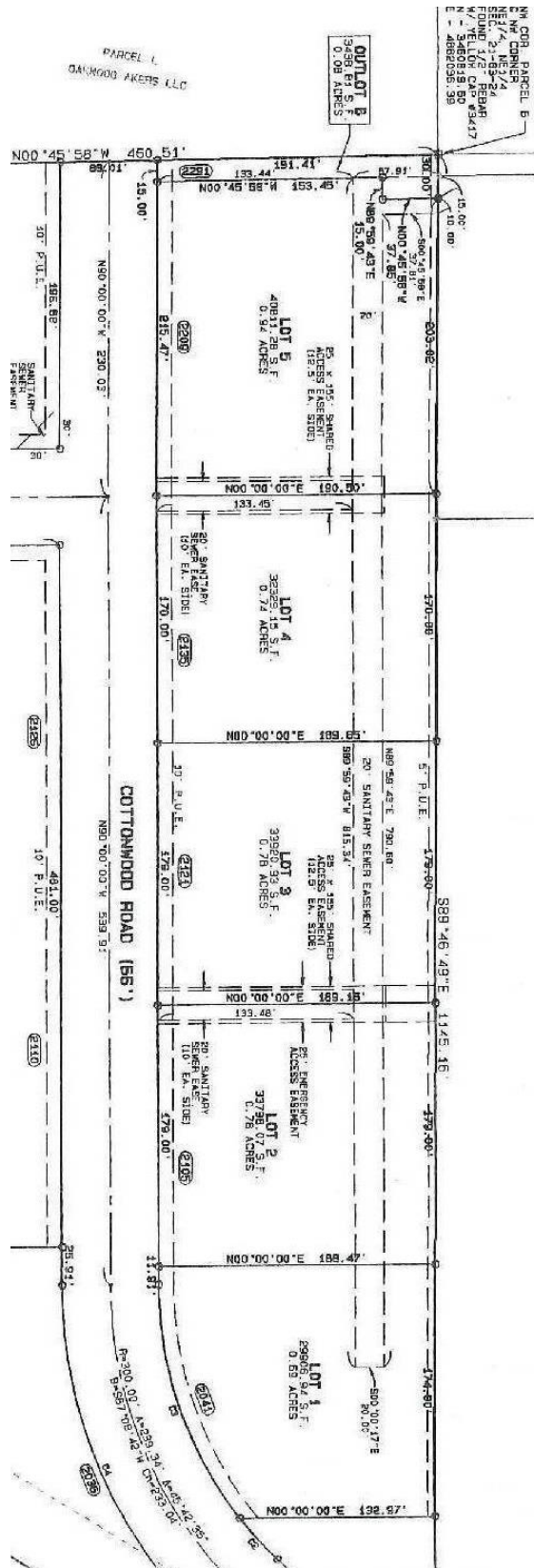
FINAL PLAT
VILLAGE PARK SUBDIVISION
 PARCEL C IN THE SE1/4, SE1/4 OF SEC. 16-B3-24
 5 PARCELS B, G, M & N IN THE NE1/4, NE1/4 OF
 SEC. 21-03-24, CITY OF AMES, STORY COUNTY, IOWA
 UOB #16504FP DATE: 8/15/16 PAGE: 1 of 1

Confirmation: I hereby certify that this land surveying record was prepared and the related survey work was performed by me or under my direct personal supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Iowa.

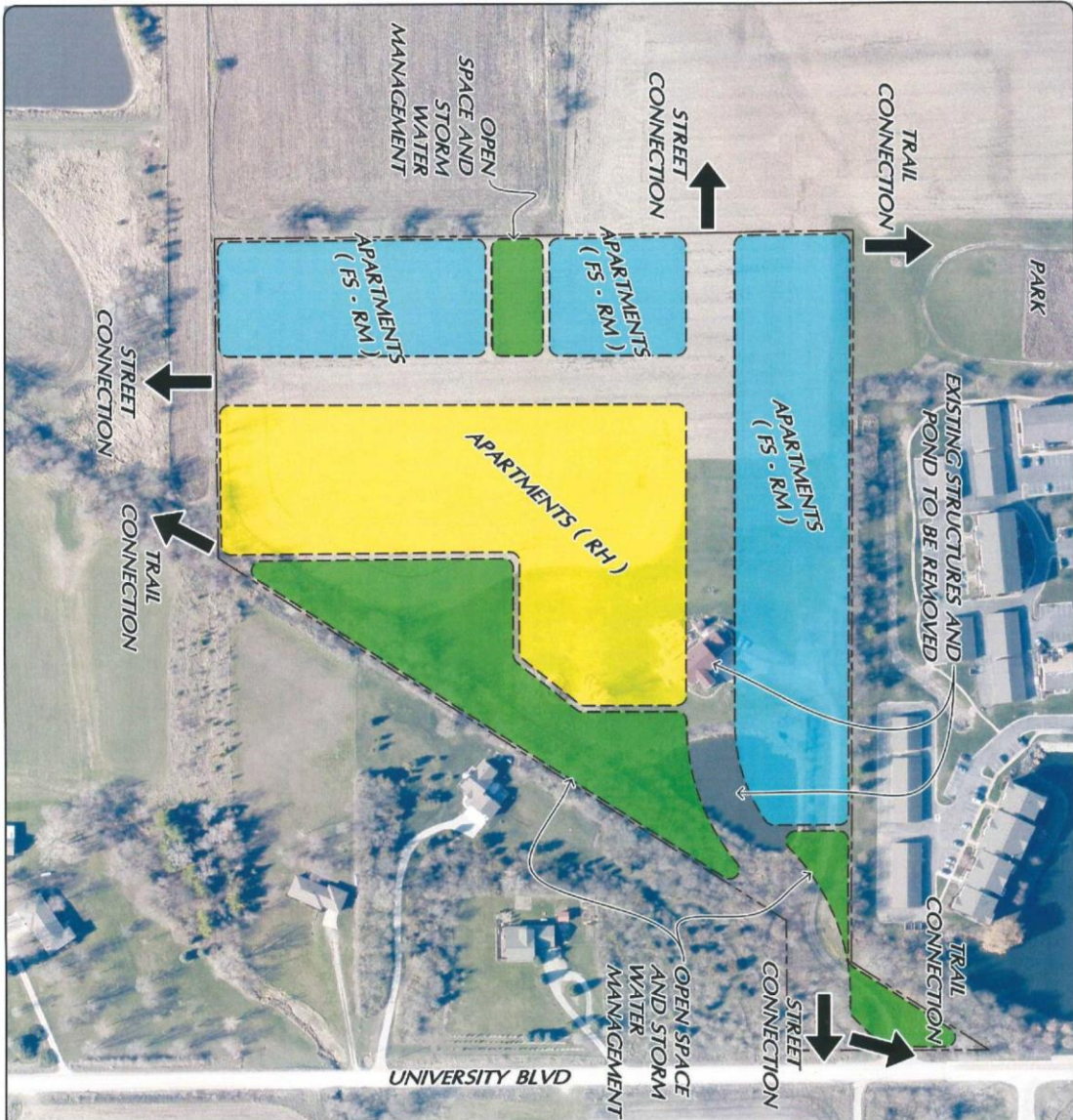
P. R. Carroll Date *8/15/16*

STUMBO & ASSOCIATES LAND SURVEYING, INC. License #17161

Attachment F: Village Park Subdivision (Lot 1, 2, 3, 4 & 5)



Attachment H: Master Plan for Village Park Subdivision



VILLAGE PARK SUBDIVISION MASTER PLAN

OWNER / APPLICANT
 HUNZIKER DEVELOPMENT CO. LLC
 105 S. 16TH STREET
 SUITE A
 AMES, IA 50010

PREPARED BY
 FOX ENGINEERING
 414 S. 14TH STREET
 SUITE 107
 AMES, IA 50010

ZONING
 EXISTING - AGRICULTURAL (A1)
 PROPOSED - RESIDENTIAL MEDIUM DENSITY (FS-RM)
 AND RESIDENTIAL HIGH DENSITY (RH)
 AS SHOWN

LEGAL DESCRIPTION
 PARCEL C IN SECTION 16, T83N, R24W; AND PARCELS B, G, M,
 AND N IN SECTION 21, T83N, R24W; ALL IN THE CITY OF AMES,
 STORY COUNTY, IOWA

RESIDENTIAL USE SUMMARY

	APPROX NET AREA (AC)	APPROX # OF UNITS	APPROX DENSITY
APARTMENTS (FS-RM)	7.2	100 - 120	13 - 17
APARTMENTS (RH)*	5.5	135 - 155	25 - 28

* NOTE: BUILDINGS IN RH WILL BE LIMITED TO 3 STORIES AND 36 UNITS PER BUILDING



DATE	REVISION	BY	DATE
		SKR	03/15/15
		SKS	03/16/15
		CHK	
		APP	
		DES	
		CON	
		INT	
		REV	
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		DES	
		CON	
		INT	
		REV	
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