

COUNCIL ACTION FORM

SUBJECT: CYRIDE ROOF REPLACEMENT PLANS AND SPECIFICATIONS

BACKGROUND:

CyRide was recently awarded grant money for roofing improvements to the existing bus maintenance facility. The ballasted type roof was last replaced on the older areas of the CyRide Facility in 1999. In recent years, the rubber membrane under the ballast has started to crack and leak, especially at exposed corners. CyRide's architectural and engineering consultant has recommended replacing the roof with a mechanically fastened PVC system, similar to what has been installed on newer areas of the facility.

In early 2016, CyRide staff applied for an Iowa Department of Transportation Public Transportation Infrastructure Grant (PTIG) for roofing improvements. In July it was announced that CyRide would be a recipient of \$320,000 in requested grant money.

To accomplish the reroofing project, CyRide has accumulated the local match and has the following dollars available:

Funds Available	Dollars
State PTIG Funds	\$ 320,000
Local Funds	\$ 55,000
Total Available	\$ 375,000

Architectural drawings and specifications are now on file in the Office of the City Clerk. Bid letting will be September 28th, with bids due on October 26, 2016. Bid results will be reported to Council on November 15, 2016.

ALTERNATIVES:

1. Approve plans and specifications for the CyRide Reroofing 2016 Project, establish October 26, 2016 as the bid due date, and establish November 15, 2016, as the date to report bid results to Council.
2. Direct staff to continue to work with the architect to refine the project plans.

MANAGER'S RECOMMENDED ACTION:

This roofing project is needed to preserve the investment in the bus storage facility, and grant funding is available to augment local funds.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative No. 1 as described above.

COUNCIL ACTION FORM

SUBJECT: PUBLIC WORKS TREE TRIMMING & REMOVAL PROGRAM

BACKGROUND:

The Public Works Department requires tree trimming, tree removal, and stump grinding to be performed along the City's rights-of-way each year. The 2016/17 budget includes \$82,000 for this work, and the estimated amount needed for the tree trimming and removal contract is \$70,000. This leaves \$12,000 for other miscellaneous work.

This work was put out for competitive bids, and the following bids were received on September 16, 2016: \$39.50

2016/17 CONTRACT PERIOD

<u>Bidder</u>	<u>Bid Amount</u>
LawnPro L.L.C., Colo IA	\$ 49.75/Crew Hour
Finco Tree/Wood Service, Kelly IA	\$125.00/Crew Hour
Weiss Tree Service, Inc., Nevada IA	\$160.00/Crew Hour

LawnPro was the service provider in the previous 3-year contract that has expired in an amount of \$39.50/crew hour. They have contracted this service with the City for over ten years. It is the understanding of staff that the other bidders' amounts reflected larger crews for each work location. While this may lead to the work being done more quickly, it does not balance out for the difference in price. Staff will work with LawnPro to plan the work activities several months in advance and monitor the amount expended under the contract on a monthly basis to keep costs within the contract amount.

ALTERNATIVES:

1. Award a contract for the 2016/17 Tree Trimming and Removal Program to LawnPro L.L.C. of Colo, IA, in an amount not to exceed \$70,000.
2. Reject all bids and attempt to purchase tree trimming on an as-need basis.

MANAGER'S RECOMMENDED ACTION:

This work is an important part of maintaining the City's rights-of-way, and the low bid will provide the largest amount of value for this program.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative No. 1 as stated above.



Memo

City Clerk's Office

TO: Mayor and Members of the City Council

FROM: City Clerk's Office

DATE: September 23, 2016

SUBJECT: Contract and Bond Approval

There are no Council Action Forms for Item Nos. 28 and 29. Council approval of the contract and bond for this project is simply fulfilling an Iowa Code requirement.

/drv

COUNCIL ACTION FORM

SUBJECT: POWER PLANT FUEL CONVERSION – ELECTRICAL INSTALLATION
GENERAL WORK CONTRACT CHANGE ORDER NO. 3

BACKGROUND:

In November of 2013 the City Council decided to convert the City's Power Plant from coal to natural gas. In May of 2014 the City Council selected Sargent & Lundy of Chicago, Illinois, to provide engineering and construction oversight services for the conversion project.

On September 22, 2015, City Council awarded a contract to FPD Power Development, LLC, Minneapolis, MN for the Power Plant Fuel Conversion – Electrical Installation General Work Contract in the amount of \$3,145,149. This specific portion of the conversion project was to hire a contractor to perform the electrical installation work.

The action now being requested is to approve Change Order No. 3 to the Electrical Installation Contract. This Change Order, in the amount of \$123,893.91, is for the following work:

- 1) Convert soot blowing systems from the one-of-a-kind in-house designed PLC system to the new integrated DCS system. Originally, due to an error in the specification and drawings, cables for this system were routed to and terminated in the wrong electrical cabinet. New cables had to be pulled and terminated into the correct electrical cabinet.
- 2) Perform the electrical installation of the excitation systems provided by General Electric (GE). This scope of work was not included in the specification and drawings for the electrical installation contract work. The decision to install new generator excitation systems was made too late to be included in the electrical installation contract.
- 3) Reimburse FPD Power Development, LLC, for sales taxes paid on materials they purchased to fulfill the obligations of their contract with the City. (FPD should have included sales taxes in their bid, but they did not due to unclear instructions regarding sales taxes in the City's Invitation to Bid.)

CHANGE ORDER HISTORY:

Two change orders have previously been issued for this project.

Change Order No. 1 for \$12,044.24 was for FPD to purchase and provide twenty Type K pneumatic positioners for the Unit 7 wind box dampers.

Change Order No. 2 for \$41,265.65 was for FPD to: 1) locate and install prefabricated equipment pedestals in the DCS equipment room for the mounting of DCS network cabinets, plus install an underfloor cable tray system for the routing of cables to the cabinets; 2) provide and install the necessary grounding of the natural gas piping system, starting at our meter/regulation station just outside the Power Plant and continuing along the piping system's route as it traverses through the Power Plant; and 3) source and supply three relay coils and one lock-out relay as spares for Unit 8.

The total cost of both change orders No. 1 and No. 2 was \$53,309.89.

PROJECT COST HISTORY:

The Engineer's estimate of the cost for this phase of the project was \$3,272,793. With this change order, the total costs for the Electrical Installation General Work Contract within the project will be increased to \$3,322,352.80.

Overall, the total project dollar amount committed to date (inclusive of this Change Order No. 3) is \$17,670,369.40. The FY 2015/16 Capital Improvements Plan included \$26,000,000 for the fuel conversion project. This was subsequently adjusted to \$18,112,011. The complete project budget to date is shown on page 3.

ALTERNATIVES:

1. Approve contract Change Order No. 3 with FPD Power Development, LLC, Minneapolis, MN, for the Power Plant Fuel Conversion - Electrical Installation General Work Contract in the amount of \$123,893.91.

2. Reject contract Change Order No. 3.

MANAGER'S RECOMMENDED ACTION:

The activities included in this Change Order were necessary to make the secondary power plant systems work with the new control system and to cover sales taxes.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative No. 1 as stated above.

PROJECT BUDGET

The overall project budget and commitments to date are summarized below. To date, the project budget has the following items encumbered:

\$17,475,000	FY 2015/16 CIP amount budgeted for project \$26,000,000 less reduced bonds issuance by \$8,525,000 based on a new project estimate
\$637,011	Unspent Funds from Power Plant Cooling Tower CIP
\$18,112,011	
<u>Sargent & Lundy, LLC</u>	
\$1,995,000	Encumbered not-to-exceed amount for Engineering Services
\$2,395,000	Engineering Services Contract Change Order No. 1
\$174,000	Engineering Services Contract Change Order No. 2
\$154,000	Engineering Services Contract Change Order No. 3
<u>GE Power Inc.</u>	
\$3,355,300	Contract cost for Natural Gas Conversion Equipment
\$29,869	Equipment Contract Change Order No. 1
(-\$321,600)	Equipment Contract Change Order No. 2
(-\$51,000)	Equipment Contract Change Order No. 3
\$1,620	Equipment Contract Change Order No. 4
\$0	Equipment Contract Change Order No. 5
\$32,679	Equipment Contract Change Order No. 6
\$62,310	Equipment Contract Change Order No. 7
<u>Emerson Process Management Power & Water Solutions, Inc.</u>	
\$1,595,000	Contract cost for DCS equipment
\$39,377	DCS Contract Change Order No. 1
\$12,611	DCS Contract Change Order No. 2
\$0	DCS Contract Change Order No. 3
<u>GE Energy Control Solutions, Inc.</u>	
\$814,920	Contract cost for TCS equipment Bid 1
\$244,731	TCS Bid 1 Contract Change Order No. 1
\$34,000	TCS Bid 1 Contract Change Order No. 2
\$0	TCS Bid 1 Contract Change Order No. 3
\$16,854	TCS Bid 1 Contract Change Order No. 4
<u>General Electric International, Inc.</u>	
\$186,320	Contract Cost for Turbine Steam Seal System - TCS Bid 2
\$24,536	TCS Bid 2 Contract Change Order No. 1

\$150,000	TCS Bid 2 Contract Change Order No. 2
\$0	TCS Bid 2 Contract Change Order No. 3
\$9,208.42	TCS Bid 2 Contract Change Order No. 4

Henkel Construction Co.

\$898,800	Contract cost for Control Room Installation General Work Contract
\$66,782	Control Room Contract Change Order No. 1
\$17,683.54	Control Room Contract Change Order No. 2

TEI Construction Services, Inc.

\$1,572,019	Contract cost for Mechanical Installation General Work Contract
\$8,750	Mechanical Contract Change Order No. 1
\$156,131	Mechanical Contract Change Order No. 2
\$187,984	Mechanical Contract Change Order No. 3
\$9,785.37	Mechanical Contract Change Order No. 4
\$3,032.17	Mechanical Contract Change Order No. 5
\$7,725.98	Mechanical Contract Change Order No. 6
\$3,032.16	Mechanical Contract Change Order No. 7
\$21,673.58	Mechanical Contract Change Order No. 8
\$175,496.89	Mechanical Contract Change Order No. 9

FPD Power Development, LLC

\$3,145,149	Contract cost for Electrical Installation General Work Contract
\$12,044.24	Electrical Contract Change Order No. 1
\$41,265.65	Electrical Contract Change Order No. 2
\$123,893.90	Electrical Contract Change Order No. 3

Graybar Electric

\$98,560	Contract cost for UPS System
(-\$1,010)	UPS System Contract Change Order No. 1

Hertz Equipment Rental Corporation

<u>\$166,835.50</u>	Contract cost for Portable Electric Space Heaters
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\$17,670,369.04	Costs committed to date for conversion
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\$441,641.96	Remaining Project Balance to cover miscellaneous equipment and modifications to the power plant needed for the fuel conversion.
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To: Mayor and City Council

From: Diane Voss, City Clerk

Date: September 23, 2016

Subject: Item #31: Completion of Bid No. 2 Turbine Steam Seal System

Staff is still working on the Council Action Form for this item. It will be sent to you on Monday.

Thank you.

/drv

COUNCIL ACTION FORM

SUBJECT: MINOR SUBDIVISION FINAL PLAT FOR AMES COMMUNITY DEVELOPMENT PARK SUBDIVISION, FIFTH ADDITION

BACKGROUND:

The property owner, Dayton Park LLC, is requesting approval of a Final Plat for Ames Community Development Park Subdivision, Fifth Addition, which is a Minor Subdivision of 6.63 acres of land, at 810 Bell Avenue (*see Attachment A – Location & Zoning Map*). A Minor Subdivision includes three or fewer lots and does not require additional public improvements. A minor subdivision does not require a preliminary plat, and may be approved by Council as a final plat only, subject to the applicant completing the necessary requirements. The subject site is also within the Ames Community Development Park TIF Area which has certain obligations related to the development of and disposition of lots in this area. Specifically, the Agreement requires that lots less than 1.5 acres be part of the requirement for construction of speculative buildings on the prescribed timeline of the agreement. Larger lots are not subject to the speculative building requirement.

A Plat of Survey was approved, by the City Council, on October 28, 2014, to consolidate Lots 8 and 13, in the Ames Community Development Park, Fourth Addition into Parcel 'A'. Lot 8, 2810 Wakefield Circle, included 1.35 acres, and Lot 13, 2824 Wakefield Circle, included 5.27 acres. The Plat of Survey established the two combined lots as Parcel 'A' at 810 S. Bell Avenue (*see Attachment C – Approved Plat of Survey for Parcel 'A', 810 S. Bell Avenue*). **The lots were combined due to the developer's belief that a single large tenant would develop the larger site. The intended project has fallen through and the developer now seeks to re-subdivide the lots to a configuration that is similar to the originally platted lot sizes.**

The proposed Final Plat of Ames Community Development Park Subdivision, Fifth Addition, divides Parcel 'A' into Lots 1 and 2. Lot 1, 2810 Wakefield Circle, includes 1.896 acres, and Lot 2, 2824 Wakefield Circle, includes 4.732 acres (*see Attachment B – Proposed Final Plat of Ames Community Development Park, Fifth Addition*). With the proposed lots both exceeding 1.5 acres in size, neither of the lots will be subject to the TIF Area development agreements provisions for construction of speculative buildings. This is the same circumstance that occurred when the lots were combined in 2014.

The proposed subdivision complies with all relevant and applicable design and improvement standards of the Subdivision Regulations, to the City's Land Use Policy Plan, to other adopted City plans, ordinances and standards, and to the City's Zoning Ordinance. Staff notes that when the subdivision was originally approved it included providing for sidewalks along the east side of Bell Avenue and the south side of Wakefield Circle. The installation of sidewalks was granted a deferral at the time of the

previous final plat approval to not occur until time of building construction.

ALTERNATIVES:

1. The City Council can approve the Final Plat for Ames Community Development Park, Fifth Addition, based upon the findings and conclusions stated above.
2. The City Council can deny the Final Plat for Ames Community Development Park, Fifth Addition, if the Council finds that the proposed subdivision does not comply with applicable ordinances, standards or plans.
3. The City Council can refer this request back to staff or the applicant for additional information.

MANAGER'S RECOMMENDATION:

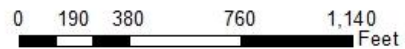
The proposed Final Plat for Ames Community Development Park, Fifth Addition, is consistent with the City's existing subdivision and zoning regulations, other City ordinances and standards, the City's Land Use Policy Plan, and the City's other duly adopted plans. No public improvements are required in conjunction with this Subdivision consistent with the previous sidewalk deferral that was granted. All other public improvements required by the prior subdivision plat have been completed.

Therefore, it is the recommendation of the City Manager that the City Council accept Alternative #1.

Attachment A Location & Zoning Map

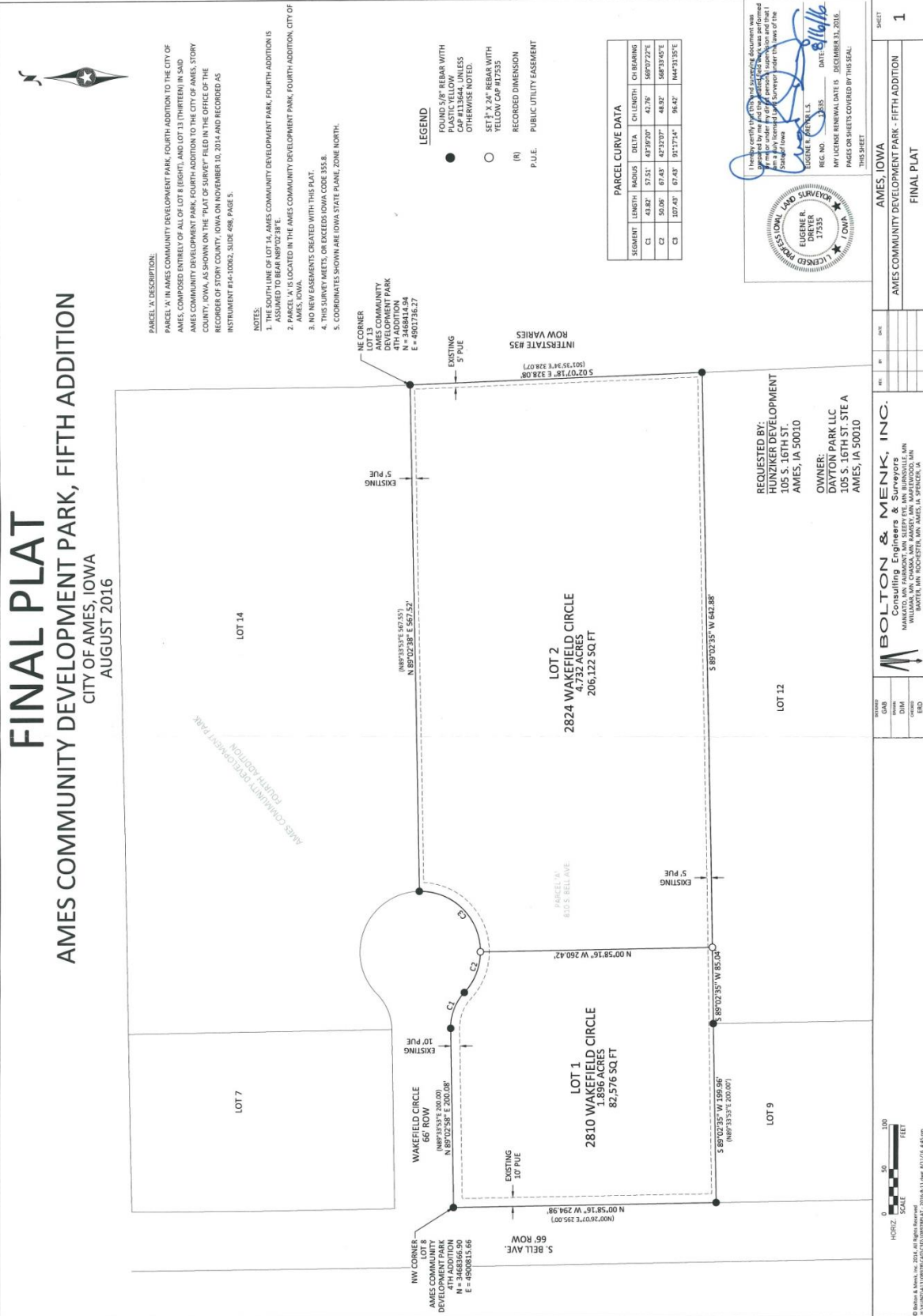


Location & Zoning Map Ames Community Development Park 5th Add.



Attachment B

Proposed Final Plat of Ames Community Development Park, Fifth Addition



Attachment D Applicable Laws

The laws applicable to this case file are as follows:

Code of Iowa, Chapter 354.8 states in part:

A proposed subdivision plat lying within the jurisdiction of a governing body shall be submitted to that governing body for review and approval prior to recording. Governing bodies shall apply reasonable standards and conditions in accordance with applicable statutes and ordinances for the review and approval of subdivisions. The governing body, within sixty days of application for final approval of the subdivision plat, shall determine whether the subdivision conforms to its comprehensive plan and shall give consideration to the possible burden on public improvements and to a balance of interests between the proprietor, future purchasers, and the public interest in the subdivision when reviewing the proposed subdivision and when requiring the installation of public improvements in conjunction with approval of a subdivision. The governing body shall not issue final approval of a subdivision plat unless the subdivision plat conforms to sections 354.6, 354.11, and 355.8.

Ames *Municipal Code* Section 23.303(3) states as follows:

(3) City Council Action on Final Plat for Minor Subdivision:

(a) All proposed subdivision plats shall be submitted to the City Council for review and approval in accordance with Section 354.8 of the Iowa Code, as amended or superseded. Upon receipt of any Final Plat forwarded to it for review and approval, the City Council shall examine the Application Form, the Final Plat, any comments, recommendations or reports examined or made by the Department of Planning and Housing, and such other information as it deems necessary or reasonable to consider.

(b) Based upon such examination, the City Council shall ascertain whether the Final Plat conforms to relevant and applicable design and improvement standards in these Regulations, to other City ordinances and standards, to the City's Land Use Policy Plan and to the City's other duly adopted plans. If the City Council determines that the proposed subdivision will require the installation or upgrade of any public improvements to provide adequate facilities and services to any lot in the proposed subdivision or to maintain adequate facilities and services to any other lot, parcel or tract, the City Council shall deny the Application for Final Plat Approval of a Minor Subdivision and require the Applicant to file a Preliminary Plat for Major Subdivision.

COUNCIL ACTION FORM

**SUBJECT: MINOR SUBDIVISION FINAL PLAT FOR DAUNTLESS
SUBDIVISION 11th ADDITION**

BACKGROUND:

Iowa Youth Basketball Foundation, property owner, is requesting approval of a Final Plat for Dauntless Subdivision, 11th Addition, a minor subdivision that would divide Parcel 'A' of Lot 3, Dauntless Subdivision, 6th Addition, into two new lots (see *Attachment B – Proposed Final Plat*). A minor subdivision includes three, or fewer, lots and does not require additional public improvements. A minor subdivision does not require a Preliminary Plat, and may be approved by the City Council as a Final Plat, only, subject to the applicant completing the necessary requirements. Following City Council approval, the Final Plat must then be recorded with the County Recorder to become an officially recognized subdivision plat.

Parcel 'A', is presently occupied by "The Iowa Youth Basketball Facility", which was constructed in 2013, and an indoor tennis practice facility that is presently under construction. Iowa State University has entered into a lease with an option to purchase the tennis facility. Parcel 'A' includes approximately 4.23 acres, which is divided into Lot 1 (3.16 acres), and Lot 2 (1.07 acres). Lot 1 is located at 1010 Dickinson Avenue, and is occupied by "The Iowa Youth Basketball Facility." Lot 2 is located at 1025 South Dakota Avenue, and will be occupied by the ISU Tennis Practice Facility (see *Attachment A – Location & Zoning Map*). The Sukup Basketball Complex is located outside the boundaries of the proposed subdivision, on Parcel 'B', abutting the north boundary of the proposed subdivision, but will have a driveway and walkway connection to the proposed Lot 2.

Proposed Lot 1 has frontage on Dickinson Avenue and Lot 2 has frontage on South Dakota Avenue, however access to South Dakota is restricted by the City. Access for Lots 1 and 2 will both be from Dickinson Avenue. To provide access to Lot 2 from Dickinson Avenue, an ingress/egress easement, 25 feet wide, is proposed across Lot 1.

The proposed subdivision is located within the "CCN" (Community Commercial Node) zoning district, and within the "O-GSW" (Southwest Gateway Overlay District). Site Development Plans have been approved by the Planning & Housing Department for the Iowa Youth Basketball Facility and for the tennis practice facility as development of one lot. The proposed subdivision alters the development requirements as each of the lots must meet zoning requirements for the occupancy and use of the site. The unique element of zoning related to the two already approved Site Development Plans is the use of "collective parking," which allowed for a 15% reduction in the minimum number of

required off-street parking spaces. With the 15% reduction for collective parking, the number of required parking spaces is reduced to 149 spaces and 20 spaces for a total of 169 spaces across the current site.

The proposed Final Plat, places 174 parking spaces on Lot 1, for The Iowa Youth Basketball Facility, which exceeds the number of parking spaces required by twenty-five spaces. A total of nine parking spaces will be constructed on Lot 2, for the Tennis Practice Facility. The tennis facility would be eleven spaces short of the minimum of twenty spaces required for the stand alone use of Lot 2. To address this shortage of parking spaces for the Tennis Practice Facility, Miles E. Lackey, Chief Financial Officer and Chief of Staff for Iowa State University, has submitted a letter with the Final Plat documents (*see Attachment C – Parking for Tennis Performance Center*), stating that the Sukup Basketball Complex (located adjacent to the north boundary of the subdivision at 1011 South Dakota Avenue) has excess parking available for use by the Tennis Performance Center (a.k.a. Tennis Practice Facility). **Iowa State University representatives request that the City Council accept their commitment as satisfying any potential future need for joint parking because of the lease and option to purchase that is in place for the tennis facility and that it will be used for University purposes.**

Public utilities serve both parcels, and sidewalk has been constructed along the frontage for Lot 1, on Dickinson Avenue, and along the frontage for Lot 2, on South Dakota Avenue. The former water main easement, shown on the Plat of Survey for Parcel A, has been vacated by the City Council. A new water main easement on Lot 2, and crossing Lot 1 to Dickinson Avenue, consistent with the location of the new water main, is shown on the Final Plat, and is included as part of the public utility easements to be recorded with the Final Plat.

Please note that street trees are not required as part of the subdivision in commercially zoned areas.

ALTERNATIVES:

1. The City Council can approve the final plat for Dauntless Subdivision, 11th Addition, based upon the findings and conclusions stated above, with the following conditions:
 - A. That the City Council accept the written confirmation from Miles E. Lackey, Chief Financial Officer and Chief of Staff for Iowa State University, that parking spaces at the Sukup Basketball Complex, 1011 South Dakota Avenue, are available for use by visitors of the Tennis Performance Center, to compensate for the deficiency of eleven (11) parking spaces on Lot 2 of Dauntless Subdivision, 11th Addition (*see Attachment C – Parking for Tennis Performance Center*); and,

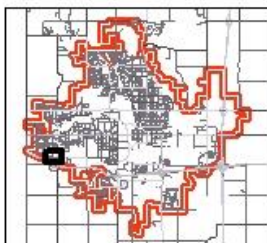
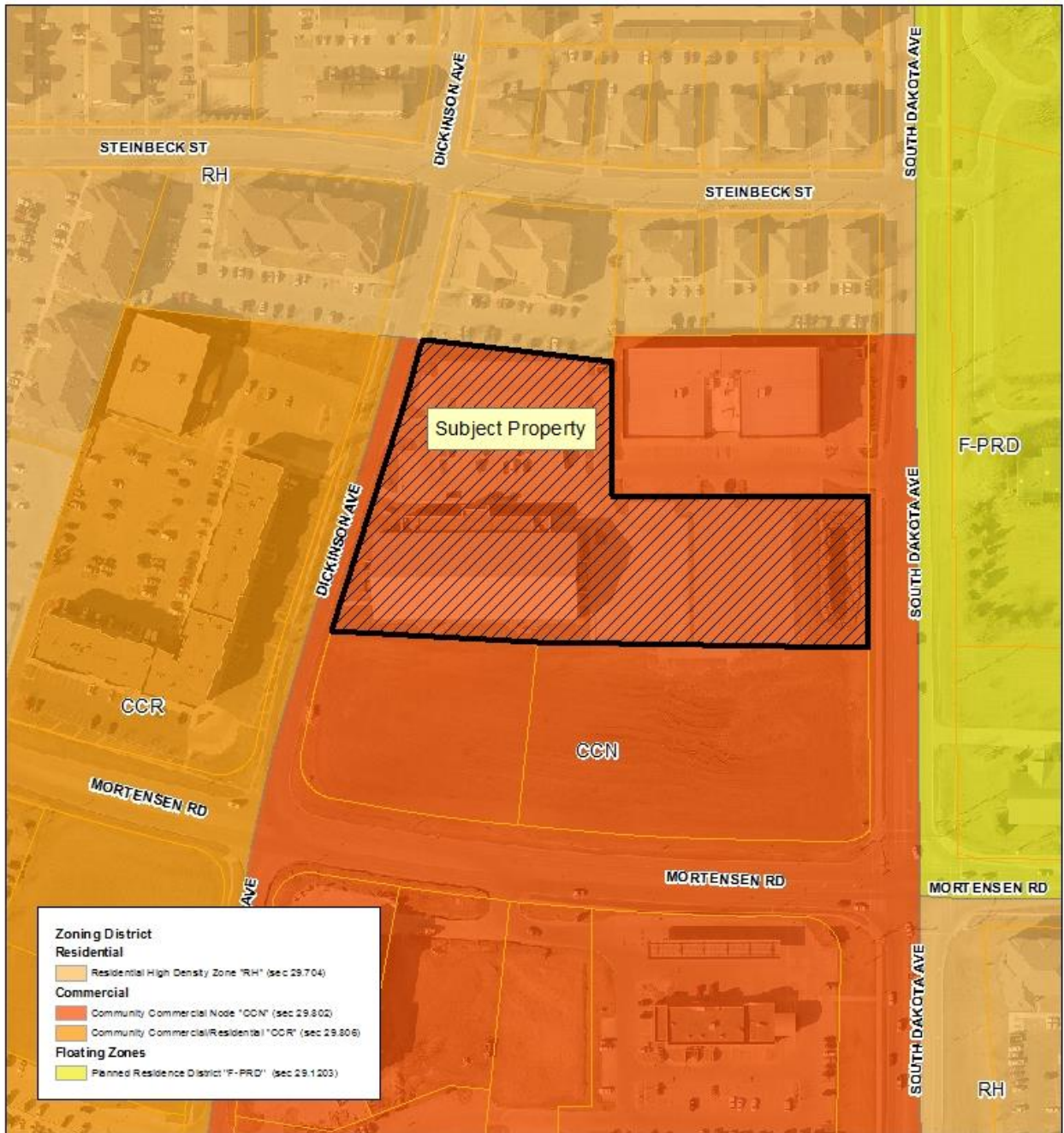
- B. That the northing and easting coordinates be added to the northwest, northeast, and southwest corners of the Final Plat, prior to recording the Final Plat with the Story County Recorder's Office; and,
 - C. That the full street address be added to Lots 1 and 2 on the Final Plat, prior to recording the Final Plat with the Story County Recorder's Office.
2. The City Council can approve the final plat for Dauntless Subdivision 11th Addition, based upon the findings and conclusions stated above, and based upon modifications to the conditions of approval.
 3. The City Council can deny the final plat for Dauntless Subdivision 11th Addition, if the City Council finds that the proposed subdivision does not comply with applicable ordinances, standards or plans.
 4. The City Council can refer this request back to staff or the applicant for additional information.

MANAGER'S RECOMMENDATION:

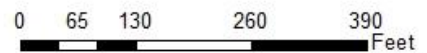
The proposed final plat for Dauntless Subdivision 11th Addition is consistent with the City's existing subdivision and zoning regulations with acceptance of the request by Iowa State University for their commitment to providing for joint use parking. Because of the lease and option to buy the facility once it is upon its own lot, staff supports the written confirmation from Iowa State University that parking spaces are available at the Sukup Basketball Complex, at 1011 South Dakota Avenue, to compensate for the deficiency of eleven (11) parking spaces for the Tennis Performance Center, on Lot 2 of the proposed subdivision. Additionally, the provision of the access easement to Dickinson ensures the site meets access requirements upon recording of the final plat.

Therefore, it is the recommendation of the City Manager that the City Council accept Alternative #1 and its related conditions.

Attachment A Location & Zoning Map



Location & Zoning Map Dauntless Subdivision, 11th Add.



Attachment C
Parking For Tennis Performance Center

IOWA STATE UNIVERSITY
OF SCIENCE AND TECHNOLOGY

Office of the President
1750 Beardshear Hall
Ames, Iowa 50011-2035
515 294-2042

August 31, 2016

RECEIVED

SEP 02 2016

CITY OF AMES, IOWA
DEPT. OF PLANNING & HOUSING

Mr. Kelly Diekmann
Director of Planning & Housing
515 Clark Avenue
Ames, IA 50010

Re: Parking for Tennis Performance Center

Dear Mr. Diekmann:

Iowa State University owns property at 1011 South Dakota Avenue. Iowa State University's Sukup Basketball Complex is located on this property.

The Iowa Youth Basketball Foundation owns property directly south of the Sukup Basketball Complex. Pursuant to a lease executed by the Board of Regents on behalf of Iowa State University and the Iowa Youth Basketball Foundation, the Iowa Youth Basketball Foundation will construct a Tennis Performance Center on the property that Iowa State University will then lease for its intercollegiate women's tennis team. Iowa State University also has the option to purchase the property.

I understand that your department has raised concerns about the sufficiency of the parking at the Tennis Performance Center. The Sukup Basketball Complex has excess available parking for use by the Tennis Performance Center. I am writing to confirm that Iowa State University is willing to allow visitors of the Tennis Performance Center to use the parking at the Sukup Basketball Complex.

I trust that this will address your concerns, but I am willing to discuss further if you wish.

Sincerely,



Miles E. Lackey
Chief Financial Officer and Chief of Staff

Attachment D Applicable Laws

The laws applicable to this case file are as follows:

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(3) City Council Action on Final Plat for Minor Subdivision:

(a) All proposed subdivision plats shall be submitted to the City Council for review and approval in accordance with Section 354.8 of the Iowa Code, as amended or superseded. Upon receipt of any Final Plat forwarded to it for review and approval, the City Council shall examine the Application Form, the Final Plat, any comments, recommendations or reports examined or made by the Department of Planning and Housing, and such other information as it deems necessary or reasonable to consider.

(b) Based upon such examination, the City Council shall ascertain whether the Final Plat conforms to relevant and applicable design and improvement standards in these Regulations, to other City ordinances and standards, to the City's Land Use Policy Plan and to the City's other duly adopted plans. If the City Council determines that the proposed subdivision will require the installation or upgrade of any public improvements to provide adequate facilities and services to any lot in the proposed subdivision or to maintain adequate facilities and services to any other lot, parcel or tract, the City Council shall deny the Application for Final Plat Approval of a Minor Subdivision and require the Applicant to file a Preliminary Plat for Major Subdivision.

COUNCIL ACTION FORM

SUBJECT: REZONE OF 2728 LINCOLN WAY, 112 AND 114 S. HYLAND AND 115 S. SHELDON AVENUE FROM RH (RESIDENTIAL HIGH DENSITY) AND O-UIW (UNIVERSITY WEST IMPACT OVERLAY) TO CSC (CAMPUSTOWN SERVICE CENTER)

BACKGROUND:

This proposed 1.8 acre development site is made up of seven properties south of Lincoln Way between Hyland and Sheldon Avenues. **The proposed request is to rezone four of the development properties to the Campustown Service Center zoning district from RH and O-UIW to allow for development of all seven properties.** The remaining three properties along Lincoln Way are already zoned CSC (See Attachment A – Location Map). The properties currently have a mix of commercial uses, a gas station and multi-family housing. Approximately 0.6 acres of the area is currently used for commercial uses and 1.2 acres is zoned for residential high density.

On August 9, 2016 City Council approved a Minor LUPP Amendment (Attachment B – Existing Land Use Designation) for the development site to the Downtown Services Center land use designation to allow for the associated changes needed to pursue their development concept for construction of a mixed use development.

During preliminary meetings the developer described an interest in a mixed-use development concept consisting of a 20-room hotel, a small amount of commercial square footage, a residential lobby, leasing offices, and approximately 500 bedrooms and amenity space, all within a 75-foot six-story building. The developer desires CSC zoning for the site to maximize the development potential of the site with mixed use development similar to what has been built recently along Lincoln Way near Lynn Avenue. Although the site has a mix of CSC and RH zoning, the differences between the two zones in setbacks, heights and parking necessitate a change to CSC to develop the site as one project to meet the developer's interests rather than as multiple projects. Development in CSC allows for urban development with no building setbacks, commercial uses on the ground floor and apartments above, and reduced parking requirements of one parking space per apartment unit, with no required parking for commercial uses. The maximum height will be 75 feet within CSC zoning compared to existing RH-O-UIW zoning height limits of 45 feet.

The attached addendum includes a description of the rezoning request and analysis of the rezoning proposal, including conformance to the LUPP policies. Analysis of the request contemplates the suitability of the specific site for the proposed mix of uses, as well as the Goals and Policies of the LUPP (Attachment F). The suitability has been evaluated through use of the RH Evaluation Tool Checklist as directed by the City Council on January 27, 2015 when apartment uses are part of a project. Although the request is ultimately for a commercial zoning district, the predominant use on this site is

intended to be housing. Staff also notes that the Planning and Zoning Commission and the City Council both recently reviewed a text amendment option for CSC zoning related to this project proposal that will allow for residential on the ground floor when across from existing residential zoning.

The Planning and Zoning Commission reviewed this request at its September 7th meeting. The Commission heard comments from neighborhood residents and property owners concerning the lack of parking in the area and the potential for increased on-street parking demand, the lack of transition from the CSC zoning allowances for 75 foot buildings compared to the residential neighborhood to the west, and for adequate sidewalk widths along the project site to support urbanized development. With these concerns, there were also comments focused around the desire to coordinate the project with the completion of the Focus Area studies of Lincoln Way Corridor study. The Commission's discussion then focused on the specific concerns of the compatibility of the project and difficulties with infill development for a CSC zoned site on the edge of Campustown. **After a failed motion to approve the rezoning, the Commission voted 4-3 to recommend the City Council not approve the rezoning and defer action until the results of the Lincoln Way Corridor Study are available.**

ALTERNATIVES:

1. The City Council can approve the request for rezoning from Residential High Density (RH) with the University West Impact Overlay (O-UIW) to Campustown Service Center (CSC).
2. The City Council can approve the request for rezoning from Residential High Density (RH) with the University West Impact Overlay (O-UIW) to Campustown Service Center (CSC), with conditions.
3. The City Council can deny the request for rezoning Residential High Density (RH) with the University West Impact Overlay (O-UIW) to Campustown Service Center (CSC), if the Council finds that the City's regulations and policies are not met.
4. The City Council can defer action on this request and refer it back to City staff and/or the applicant for additional information.

MANAGER'S RECOMENDATION:

In this case, the RH checklist shows that, as an existing infill opportunity, the site is able to be served with existing infrastructure and access to existing transit. The site scores high on the tool because of its proximity to a variety of daily services and employment centers, including the ISU campus. The site also scores high on supporting economic development with the opportunity for a mixed use development type on the property. Individual site layout and design issues will need to be considered in more depth when site plan details are available, but the project concept articulated by the applicant will take advantage of the full height and parking allowances provided for by the CSC zoning district.

Staff notes that residents to the west of this site continue to be concerned about traffic levels for through traffic in this area and parking issues that are present in the neighborhood. Comments also call out concerns about the height and setback differences between CSC zoning and the abutting residential zoning to the west. Input from the Campustown Action Association to date has been generally supportive of adding commercial square footage to Campustown with the information that is currently available about the developer's plans.

Although there have been a large number of apartments constructed within Ames over the past three years (over 1,000 additional beds within Campustown alone), staff believes there is still a need to provide for apartment housing to meet projected demand. Adding student housing across from campus is a positive for adding overall apartment supply that would meet unique student demands and have the potential to relieve pressure on other apartment buildings further from campus that may then be available for other types of occupants. **It is still critical that the development of this site meets the commercial and pedestrian character of the core of Campustown as a transition site between commercial areas to the east and residential areas to the west.** The developer desires to enter into a development agreement with the City to address some of the design issues and support creation of an Urban Revitalization Area for the site to allow for property tax abatement. However, these issues are not addressed as part of the rezoning request and would be part of a subsequent action.

Therefore, it is the recommendation of the City Manager that the City Council accept Alternative #1, thereby approving the request for rezoning from Residential High Density (RH) with the University West Impact Overlay (O-UIW) to Campustown Service Center (CSC).

ADDENDUM

Existing Land Use Policy Plan. The LUPP designation of the entire subject area is Downtown Service Center. This designation applies to the entire area lying south of Lincoln Way from S. Hyland Avenue to Lynn Avenue (Campustown). An LUPP map of the immediate area can be found in Attachment B. Back in 2006 this area was rezoned to a new CSC zone separate from downtown, created from what was previously known as the Downtown/Campustown Service Center (DCSC) zone.

The LUPP indicates the Downtown Services Center designation as “specialized business services, governmental services and retail commercial uses that are associated with highly intense activities and central location. Specialized mixing of activities, parking and design provisions may apply. Floor area ratios are 1.0 and higher.”

Existing Uses of Land. The subject site is currently a mix of small commercial uses and small multi-family housing. The properties to the north, directly across Lincoln Way, contain the mix of uses. The property to the south is all multi-family housing. Properties to the west are either single-family owner occupied or rental properties with some duplex and small multi-family housing options. The property to the east of the subject sites are owned by the Collegiate United Methodist Church.

Existing Zoning. The existing zoning of the subject property is Residential High Density with the University West Impacted Overlay. The developers on the project are seeking rezoning to CSC which is supported under the newly designated Downtown Services Center Land Use designation of the LUPP. The proposed area for rezoning to CSC is reflected in Attachment D.

Infrastructure. Access to existing sewer and water infrastructure is available to the site from both Lincoln Way and S. Hyland Avenue. Public Works had received general information from the developer regarding sewer loading information for the development. The assessment of capacity in west Ames found there are projected deficiencies in the main trunk line along Lincoln Way when accounting for planned and proposed development.

One of the distinctive elements of Campustown is the larger sidewalks that are present to meet the pedestrian demands of the area. The subject site currently has an 8-foot shared use path sidewalk along its Lincoln Way frontage with no on-street parking as exists in the core of Campustown. Staff does not believe parking will be added to Lincoln Way along this site, but that the sidewalk does need to be widened to fit the character of the proposed project as a large mixed use building and the needs of the community for comfortable and safe walking and biking in this area. The change to CSC would reinforce the need to look at a wider sidewalk compared to other zoning districts.

A storm water management plan will be required to meet minimum City standards for storm water quantity and quality. At this time detailed storm water plans have not been developed, but it is intended that techniques such as underground detention will likely

be required to meet minimum City standards. Public Works will review and approve such requirements prior to approval of the Minor Site Development plan for the project.

Traffic/Access. The development site has frontage on Lincoln Way, S. Sheldon and S. Hyland Avenue. It is anticipated that parking for the site for both residential and commercial uses will be from S. Hyland and S. Sheldon. No access is planned from Lincoln Way for the project.

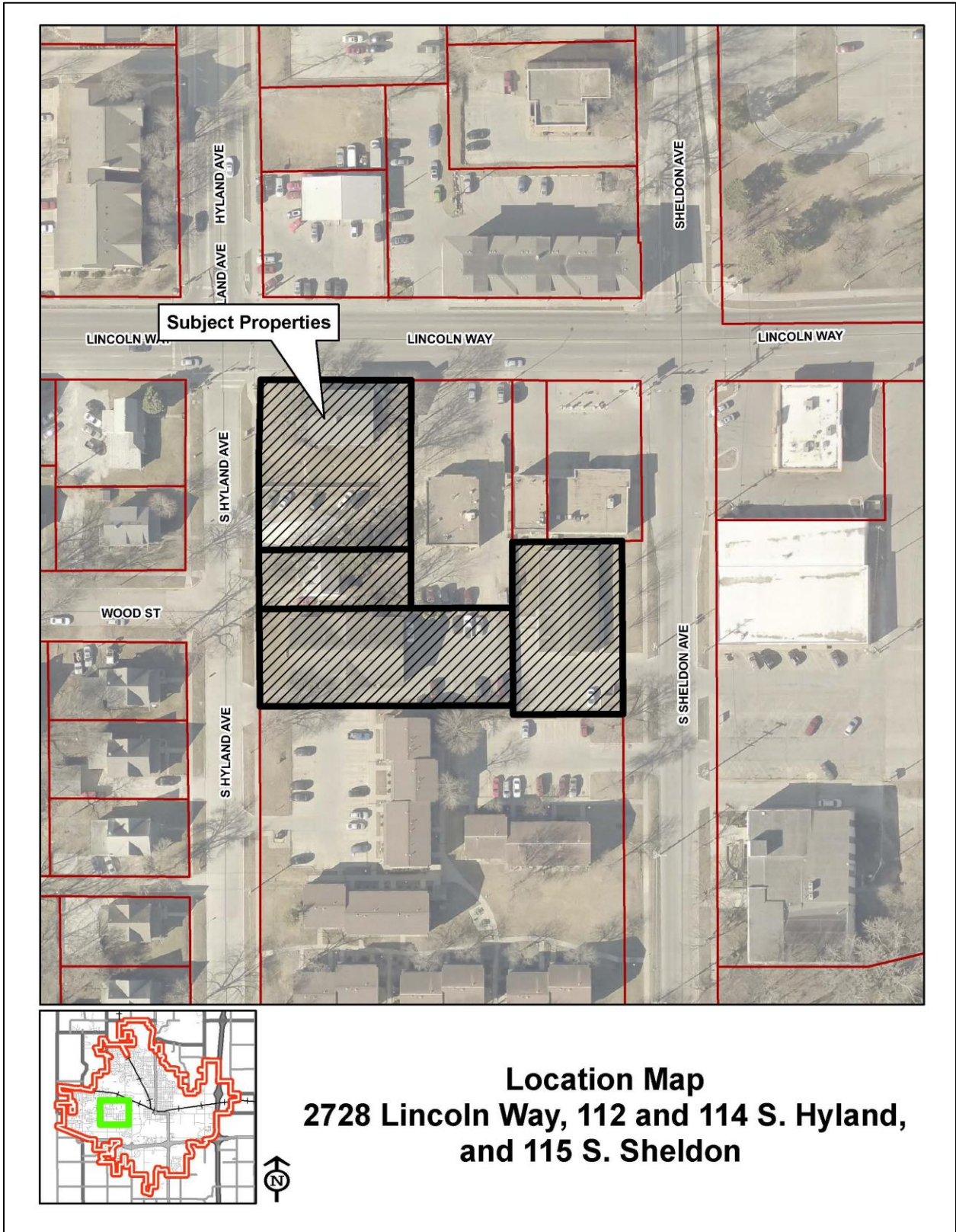
Staff believes that reviewing localized effects of operations near the site and accessing the site is a priority for understanding the potential traffic impacts of development. A traffic study is usually triggered when at least 100 peak hour trips are added to the transportation network. The scope of evaluation then depends on the specific types of trips, nearby operations, and potential for project specific impacts. Public Works Department has requested a specific evaluation of the projected trip generation to scope a traffic assessment for the project. A review of traffic and any future evaluation requirements for traffic impact will need to be completed prior to project approval.

RH Site Evaluation Matrix

The RH Evaluation Tool is an evaluation of a specific site's attributes based upon the principles of the Goals and Objectives of the LUPP. With this request there are minimal details available to complete the checklist regarding design of the project. However, location/surroundings, transportation, housing types and opportunity for mixed use would rank high for this project based on location of the project near campus and commercial development areas and the site being located on a major transit route. If the Council believes that potentially adding additional student housing is desirable and that the design controls of the CSC zoning district and potentially a development agreement support redeveloping the site, the RH matrix indicates this could be a good site for such an intense use. The developer seeks CSC because of the intensification benefits of the CSC zoning with reduced setbacks and lower parking requirements compared to other zoning categories. The RH Checklist, as completed by staff, is included as Attachment G.

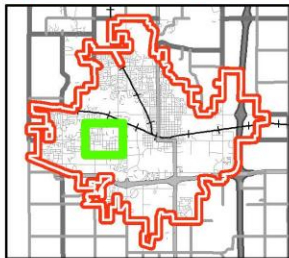
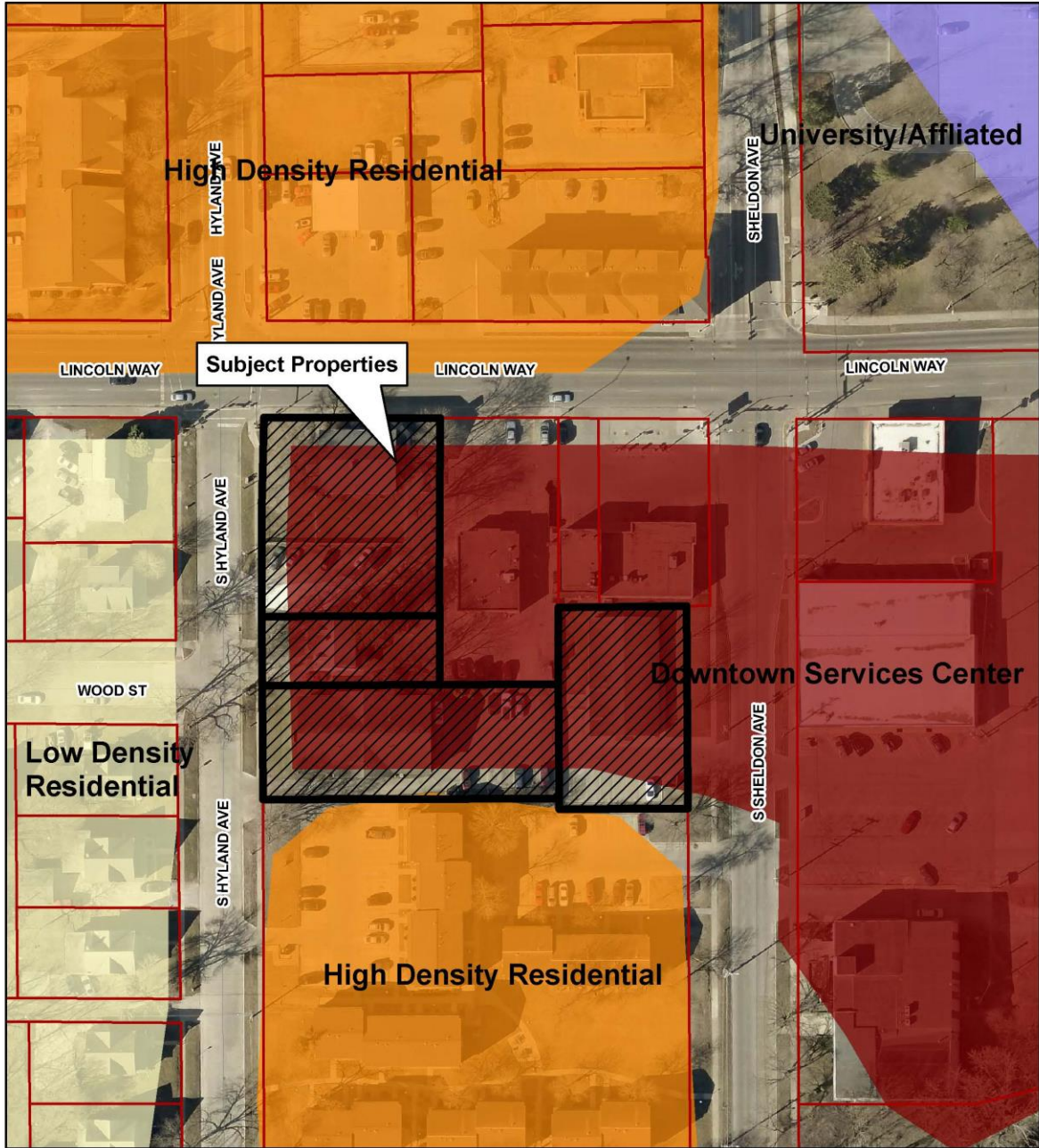
Public Notice. Notice was mailed to property owners within 200 feet of the subject site and a sign was posted on the subject property. As of this writing, no comments have been received since the noticed was mailed. Comments described above were from previous steps in the project's review and initiation with the City Council.

Attachment A
Location Map



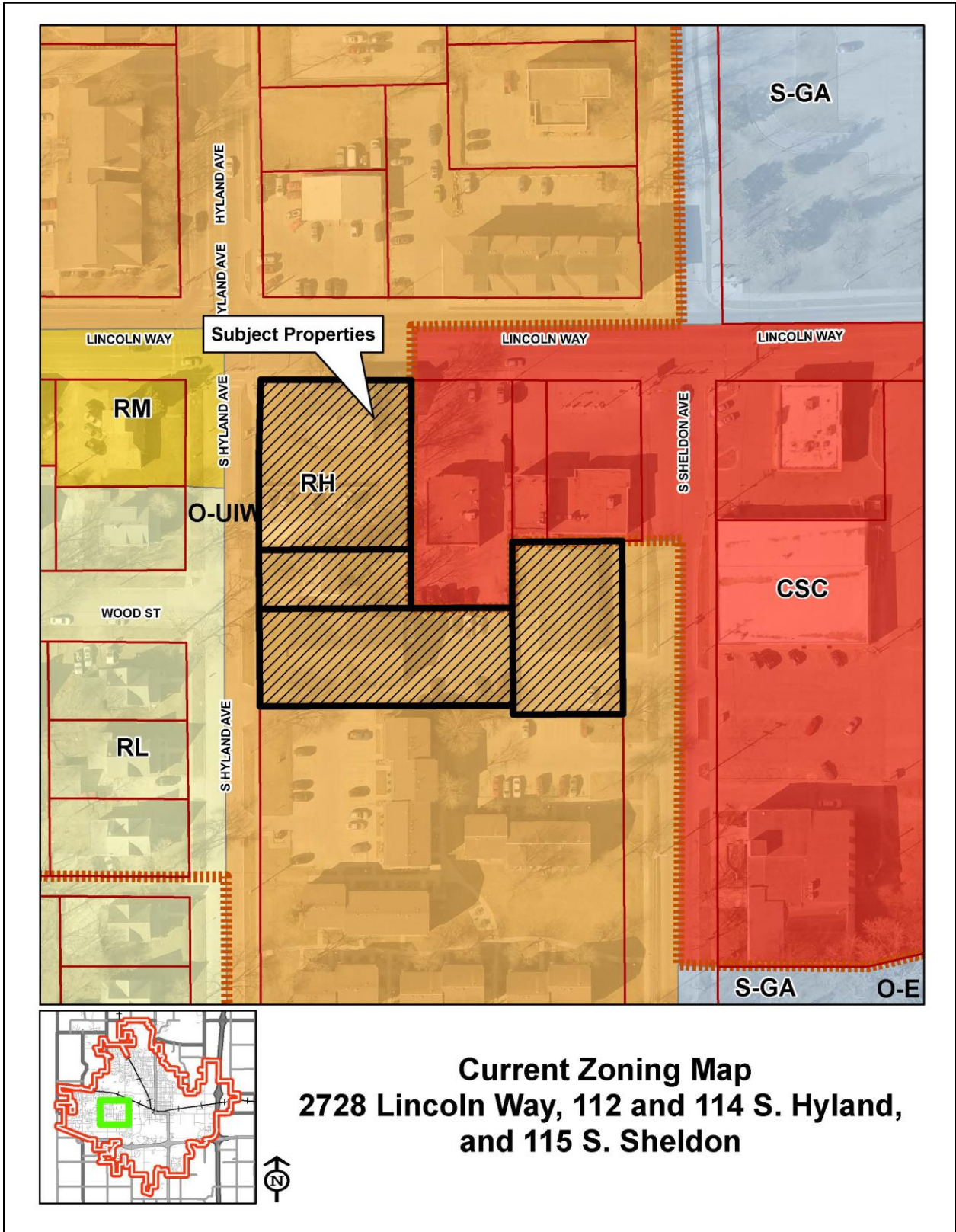
Location Map
2728 Lincoln Way, 112 and 114 S. Hyland,
and 115 S. Sheldon

Attachment B
Land Use Policy Plan Map



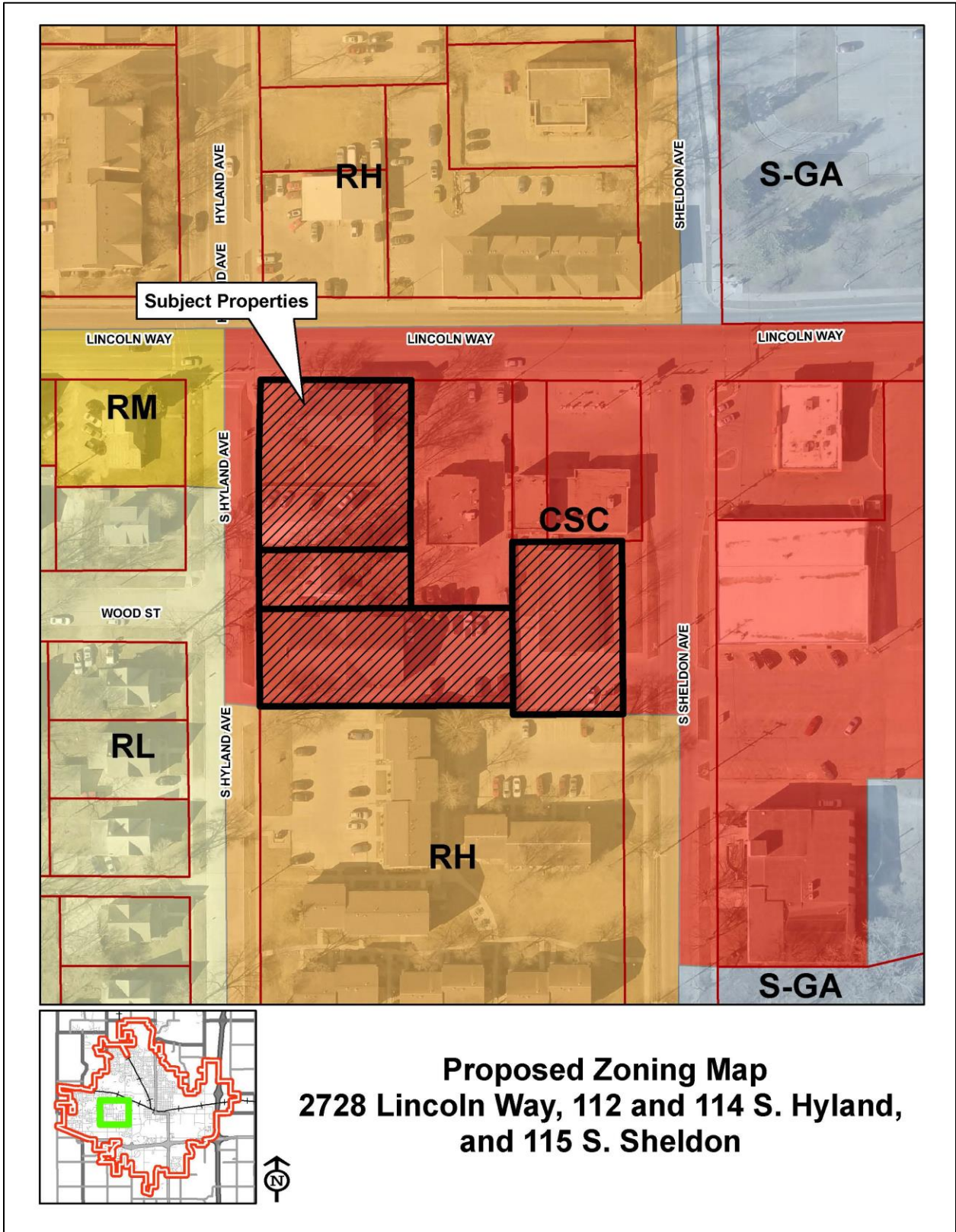
LUPP Map
2728 Lincoln Way, 112 and 114 S. Hyland,
and 115 S. Sheldon

**Attachment C
Existing Zoning**



**Current Zoning Map
2728 Lincoln Way, 112 and 114 S. Hyland,
and 115 S. Sheldon**

**Attachment D
Proposed Zoning**



**Proposed Zoning Map
2728 Lincoln Way, 112 and 114 S. Hyland,
and 115 S. Sheldon**

Attachment F ***Applicable Regulations***

- Land Use Policy Plan (LUPP) Goals, Policies and the Future Land Use Map:

The Land Use Policy Plan (LUPP) Future Land Use Map identifies the land use designations for the property proposed for rezoning.

- Ames *Municipal Code* Chapter 29, Section 1507, Zoning Text and Map Amendments, includes requirements for owners of land to submit a petition for amendment, a provision to allow the City Council to impose conditions on map amendments, provisions for notice to the public, and time limits for the processing of rezoning proposals.
- Ames *Municipal Code* Chapter 29, Section 809, Campustown Service Center, includes a list of uses that are permitted in the CSC zoning district and the zone development standards that apply to properties in those zones.

Attachment G RH Site Evaluation Tool

RH Site Evaluation Matrix	Project Consistency		
	High	Average	Low
Location/Surroundings			
Integrates into an existing neighborhood with appropriate interfaces and transitions High=part of a neighborhood, no significant physical barriers, includes transitions; Average=adjacent to neighborhood, some physical barriers, minor transitions; Low=separated from an residential existing area, physical barriers, no transitions available	X		
Located near daily services and amenities (school, park ,variety of commercial) High=Walk 10 minutes to range of service; Average=10 to 20 minutes to range of service; Low= Walk in excess of 20 minutes to range of service. *Parks and Recreation has specific service objectives for park proximity to residential	X		
Creates new neighborhood, not an isolated project (If not part of neighborhood, Does it create a critical mass or identifiable place, support to provide more services?)		X	
Located near employment centers or ISU Campus (High=10 minute bike/walk or 5 minute drive; Average is 20 minute walk or 15 minute drive; Low= exceeds 15 minute drive or no walkability)	X		
Site			
Contains no substantial natural features on the site (woodlands, wetlands, waterways)	X		
Located outside of the Floodway Fringe	X		
Separated adequately from adjacent noise, business operations, air quality (trains, highways, industrial uses, airport approach)	X		
Ability to preserve or sustain natural features			X
Housing Types and Design			
Needed housing or building type or variety of housing types		X	
Architectural interest and character			X
Site design for landscape buffering			X
Includes affordable housing (Low and Moderate Income))			X

Transportation			
Adjacent to CyRide line to employment/campus High=majority of site is 1/8 miles walk from bus stop; Average= majority of site 1/4 mile walk from bus stop; Low= majority of site exceeds 1/4 miles walk from bus stop.	X		
CyRide service has adequate schedule and capacity High=seating capacity at peak times with schedule for full service Average=seating capacity at peak times with limited schedule Low=either no capacity for peak trips or schedule does not provide reliable service		X	
Pedestrian and Bike path or lanes with connectivity to neighborhood or commute	X		
Roadway capacity and intersection operations (existing and planned at LOS C)		X	
Site access and safety		X	
Public Utilities/Services			
Adequate storm, water, sewer capacity for intensification High=infrastructure in place with high capacity Average=infrastructure located nearby, developer obligation to extend and serve Low=system capacity is low, major extension needed or requires unplanned city participation in cost.			X
Consistent with emergency response goals High=Fire average response time less than 3 minutes Average=Fire average response time within 3-5 minutes Low=Fire average response time exceeds 5 minutes, or projected substantial increase in service calls	X		
Investment/Catalyst			
Support prior City sponsored neighborhood/district investments or sub-area planning		X	
Creates character/identity/sense of place		X	
Encourages economic development or diversification of retail commercial (Mixed Use Development)	X		

ORDINANCE NO. _____

AN ORDINANCE AMENDING THE OFFICIAL ZONING MAP OF THE CITY OF AMES, IOWA, AS PROVIDED FOR IN SECTION 29.301 OF THE *MUNICIPAL CODE* OF THE CITY OF AMES, IOWA, BY CHANGING THE BOUNDARIES OF THE DISTRICTS ESTABLISHED AND SHOWN ON SAID MAP AS PROVIDED IN SECTION 29.1507 OF THE *MUNICIPAL CODE* OF THE CITY OF AMES, IOWA; REPEALING ALL ORDINANCES AND PARTS OF ORDINANCES IN CONFLICT HEREWITH AND ESTABLISHING AN EFFECTIVE DATE

BE IT HEREBY ORDAINED by the City Council of the City of Ames, Iowa;

Section 1: The Official Zoning Map of the City of Ames, Iowa, as provided for in Section 29.301 of the *Municipal Code* of the City of Ames, Iowa, is amended by changing the boundaries of the districts established and shown on said Map in the manner authorized by Section 29.1507 of the *Municipal Code* of the City of Ames, Iowa, as follows: That the real estate, generally located at 2728 Lincoln Way, 112 and 114 South Hyland Avenue, and 115 South Sheldon Avenue, is rezoned from Residential High Density (RH) and University West Impact Overlay (O-UIW) to Campustown Service Center (CSC).

Real Estate Description:

2728 Lincoln Way

LOTS TWO (2) AND THREE (3) AND THE NORTH 13 FEET OF LOT FOUR (4) OF LEE'S SUBDIVISION OF THE NORTH 213 FEET (EXCEPT THE NORTH 33 FEET THEREOF) OF THE W3/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF SECTION 9, TOWNSHIP 83 NORTH, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, IOWA.

112 S. Hyland Avenue

THE SOUTH 47 FEET OF LOT FOUR (4) OF LEE'S SUBDIVISION OF THE NORTH 213 FEET, EXCEPT THE NORTH 33 FEET THEREOF, OF THE W3/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M., AMES, STORY COUNTY, IOWA.

114 S. Hyland Avenue

SOUTH 77.5 FEET OF NORTH 290.5 FEET OF WEST 3/5THS PF EAST HALF (E1/2) NORTHEAST QUARTER (NE1/4) NORTHWEST QUARTER (NW1/4) NORTHWEST QUARTER (NW1/4) OF SECTION 9, TOWNSHIP 83 NORTH, RANGE 24 WEST OF THE 5TH P.M., AMES, STORY COUNTY, IOWA

115 S. Sheldon Avenue

OFFICE AND CONDOMINIUM UNITS 101, 102 AND 103 IN TPM CONDOMINIUMS, A CONDOMINIUM IN THE CITY OF AMES, STORY COUNTY, IOWA, TOGETHER WITH ALL APPURTENANCES THERETO INCLUDING AN UNDIVIDED FRACTIONAL INTEREST IN THE COMMON ELEMENTS, AREAS AND FACILITIES AS DETERMINED FOR SAID UNIT BY THE PROVISIONS OF, AND IN ACCORDANCE WITH, THE DECLARATION OF SUBMISSION TO HORIZONTAL PROPERTY REGIME FOR TPM CONDOMINIUMS FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON DECEMBER 30, 2005 AS INST. NO. 05-16146 (AND ANY SUPPLEMENTS AND AMENDMENTS THERETO). TPM CONDOMINIUMS, AS PRESENTLY CONSTITUTED, IS LOCATED ON A PART OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M., IN THE CITY OF AMES, STORY COUNTY, IOWA; DESCRIBED AS FOLLOWS; COMMENCING AT A POINT ON THE EAST LINE OF HYLAND AVENUE, IN THE CITY OF AMES, WHICH IS 257.5 FEET SOUTH OF THE SE CORNER OF THE INTERSECTION OF LINCOLN WAY AND HYLAND AVENUE; THENCE S89°31'30"E, 199.3 FEET ALONG THE NORTH LINE OF THE SOUTH 370 FEET OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, IOWA, THENCE NORTH 129.6 FEET ALONG THE WEST LINE OF THE E2/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SAID SEC 9, THENCE S89°29'40"E, 87 FEET ALONG A LINE WHICH IS 128 FEET SOUTH OF A PARALLEL TO THE SOUTH LINE OF LINCOLN WAY, THENCE SOUTH, 136.6 FEET, THENCE N89°29'40"W, 87 FEET, THENCE NORTH, 7 FEET ALONG THE WEST LINE OF THE E2/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SE. 9 TO THE POINT OF BEGINNING. PARCEL CONTAINS 11,878 SQUARE FEET, OR 0.273 ACRE.

Section 2: All other ordinances and parts of ordinances in conflict herewith are hereby repealed to the extent of such conflict.

Section 3: This ordinance is in full force and effect from and after its adoption and publication as provided by law.

ADOPTED THIS _____ day of _____, _____.

Diane R. Voss, City Clerk

Ann H. Campbell, Mayor

COUNCIL ACTION FORM

SUBJECT: REQUEST TO INITIATE VACATION AND CONVEYANCE OF CITY RIGHT OF WAY ON SOUTH SHELDON AVENUE

BACKGROUND:

River Caddis, Inc, contract purchaser and developer, of property located at 2700, 2702, 2718 & 2728 Lincoln Way, 112 & 114 S. Hyland Avenue, and 115 S. Sheldon Avenue, is requesting that the City Council initiate the vacation and sale of excess right-of-way along the west side of South Sheldon Avenue in Campustown. The developer currently has the seven noted properties totaling 1.8 acres under purchase contract with the intent to develop a six-story mixed use development. **During the recent Site Development Plan review for the project, the developer requested placement of stormwater treatment measures within the right-of-way. Staff was not able to authorize such a facility, but noted that the right-of-way area fronting the property at 115 S. Sheldon Avenue could potentially be excess right-of-way that the developer could offer to purchase and then use for stormwater treatment.**

Public Works staff has reviewed the subject right-of-way area with the current street configurations of South Sheldon Avenue and has determined that the area being requested for purchase is an area of excess right-of-way width not needed for any future street expansion. The area is approximately 15 feet wide and 137 feet long. The sale of this portion of right-of-way would align the property line with the existing property to the north. A location map is included as Attachment 1.

If City Council agreed to initiate the vacation and sale of this portion of City right-of-way, staff would perform a valuation of the requested area for purchase by the developer. In accordance with City policy, Public Works staff would initiate the process of contacting all right-of-way users to determine any utilities within the area for the purpose of vacation of the area and would return to City Council in October to set a public hearing date for the vacation and future sale of the property.

Any easements needed in this area for sidewalk or future utility connections would be addressed and recorded as part of the Site Development Plan for the project.

ALTERNATIVES:

1. Direct staff to initiate the process for the vacation and sale of a portion of right-of-way along the west side of South Sheldon Avenue abutting the property at 115 S. Sheldon Avenue.

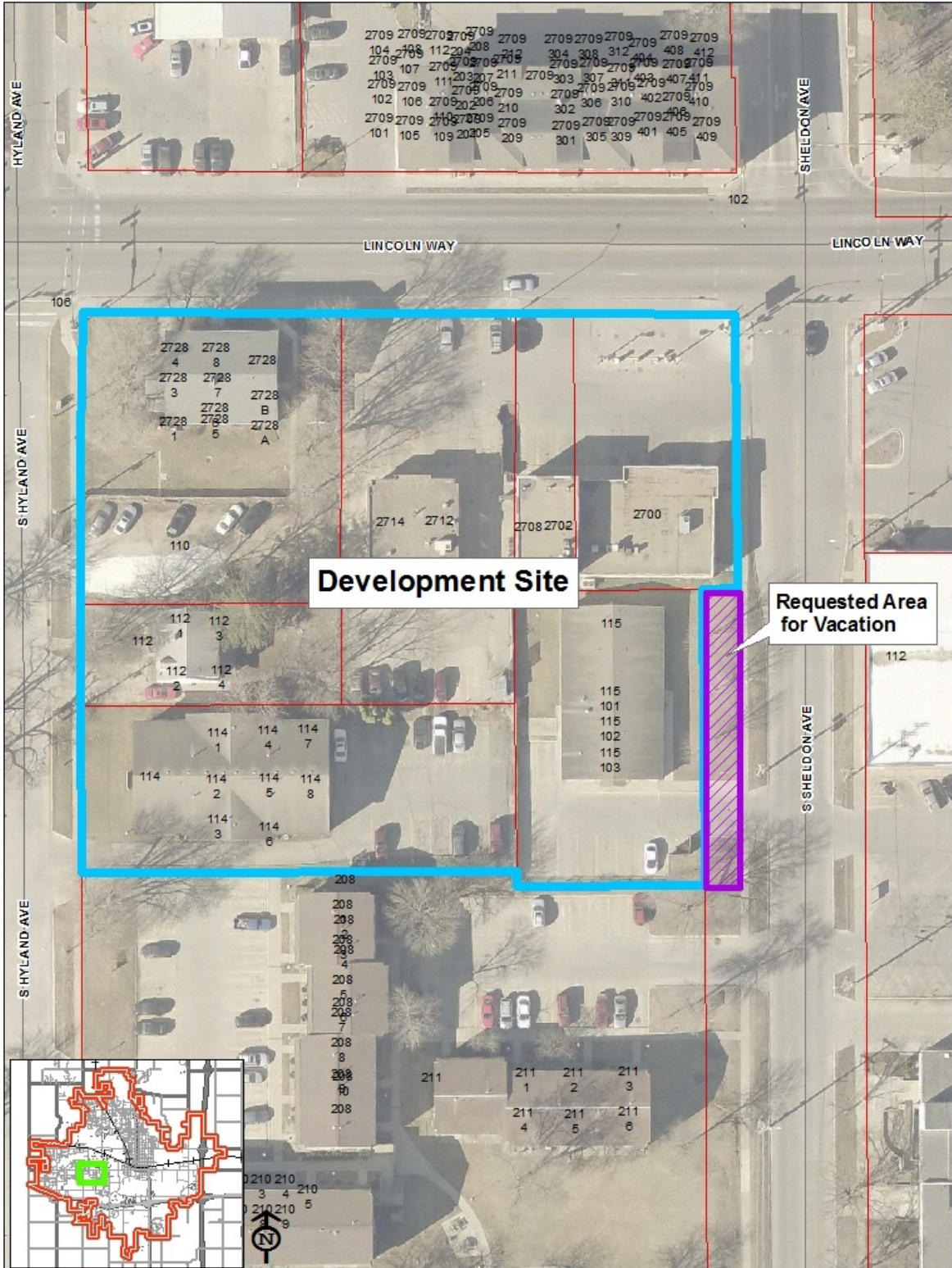
2. Retain the land and deny the initiation of the vacation process for the right-of-way along South Sheldon Avenue.

MANAGER'S RECOMMENDED ACTION:

This area of right-of-way along South Sheldon Avenue is an area of excess width and is not intended for future public street purposes.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative # 1 as described above.

Attachment 1 Location Map





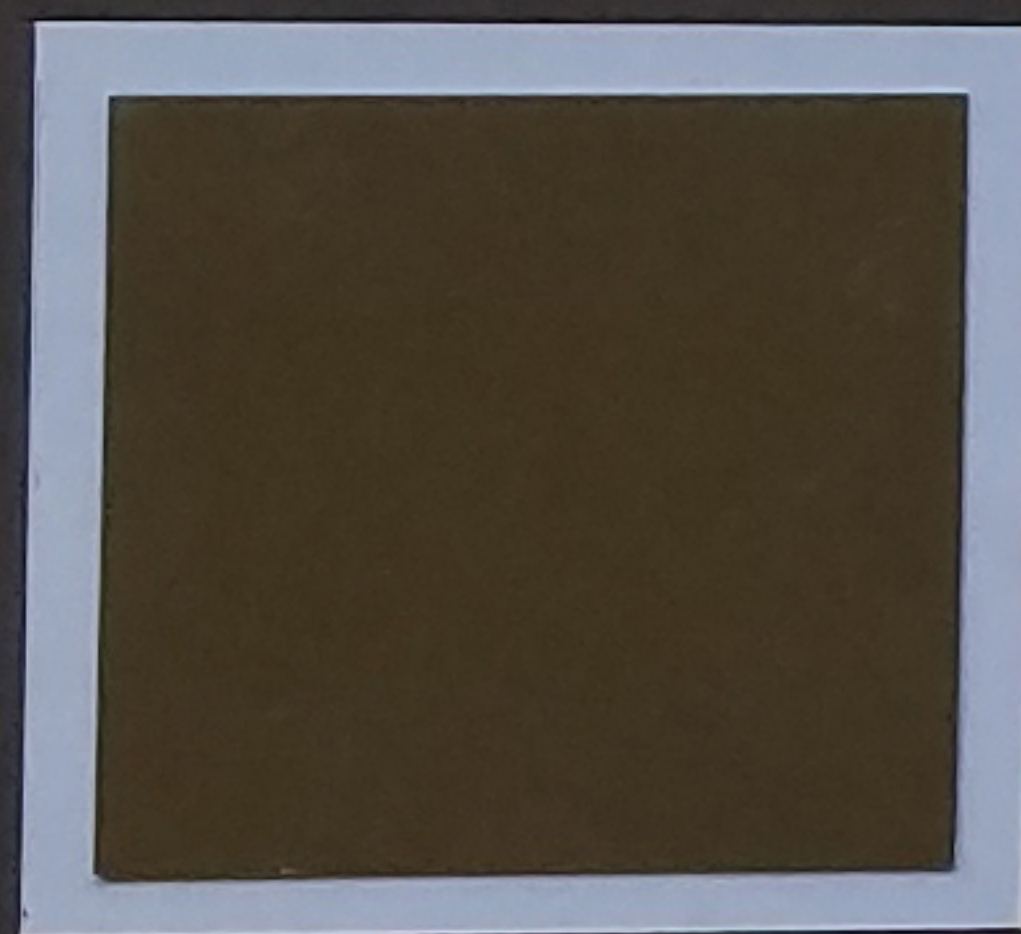
OPUS
THE SPUR GROUP

River Caddis Mixed Use Development
Ames, Iowa

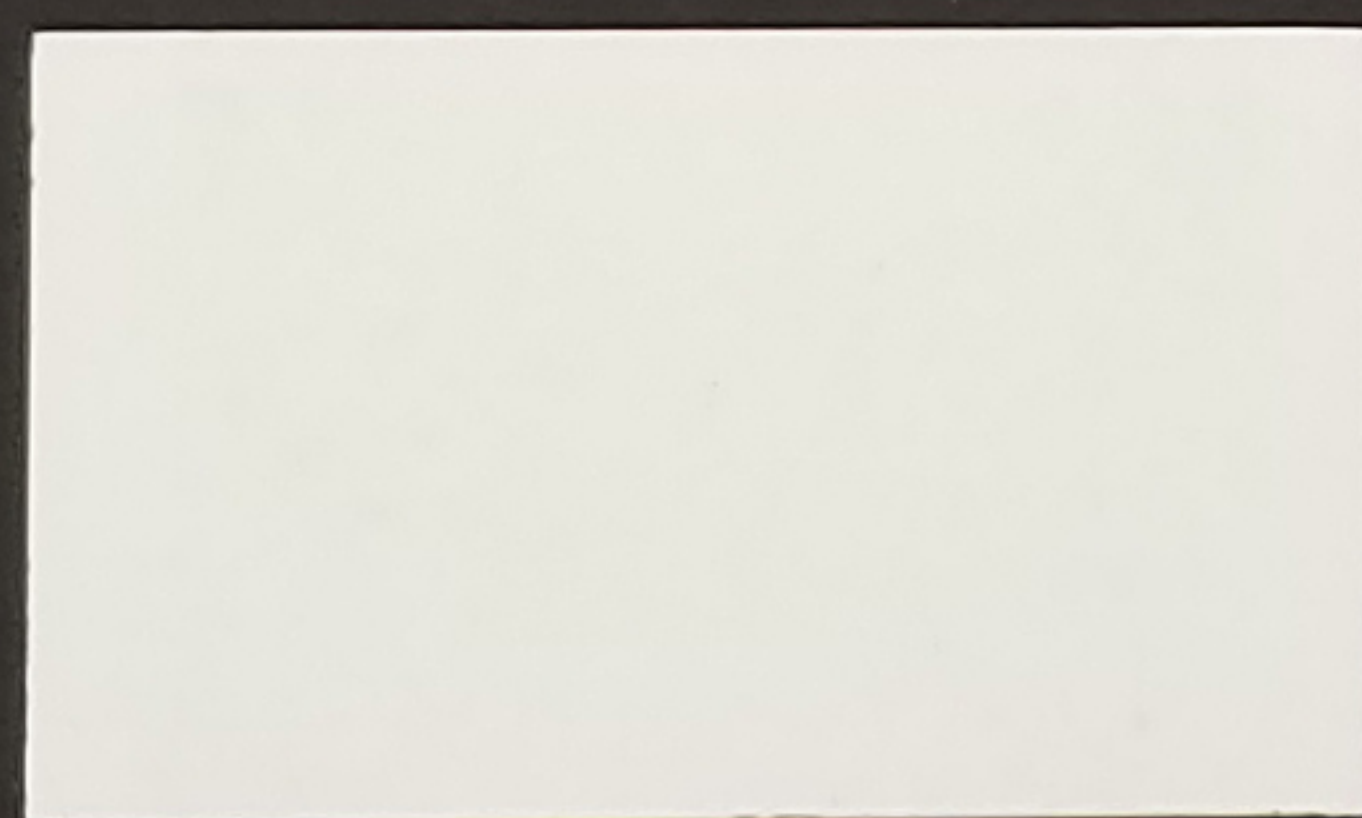
Sheldon Avenue and Lincoln Way
09.23.2016



4 FABRIC CANOPY
Black



5 ALUMINUM WINDOW FRAME
Storefront
Bronze



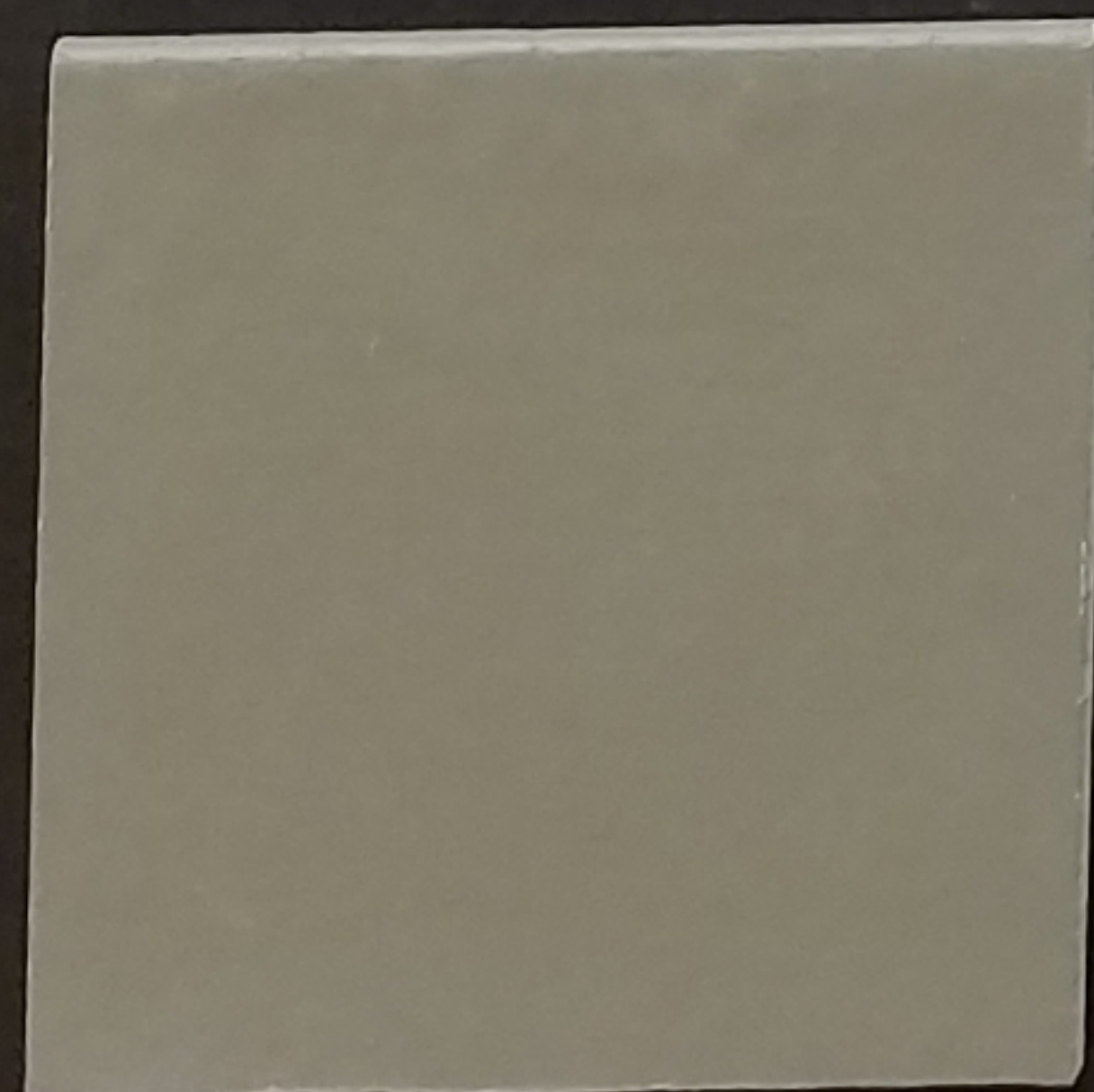
6 PREFINISHED METAL
PANEL
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7 VINYL WINDOW FRAME
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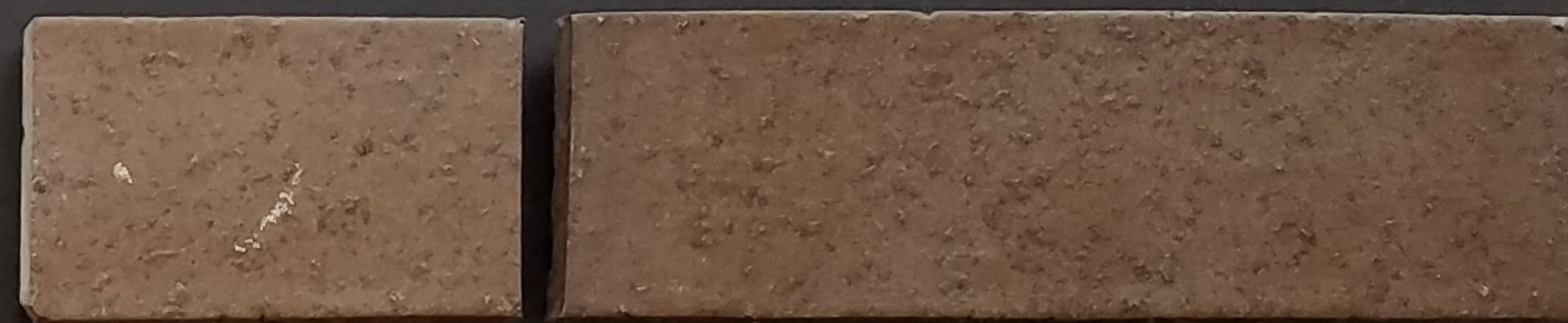
8 FIBER CEMENT PANEL
Color 1
White



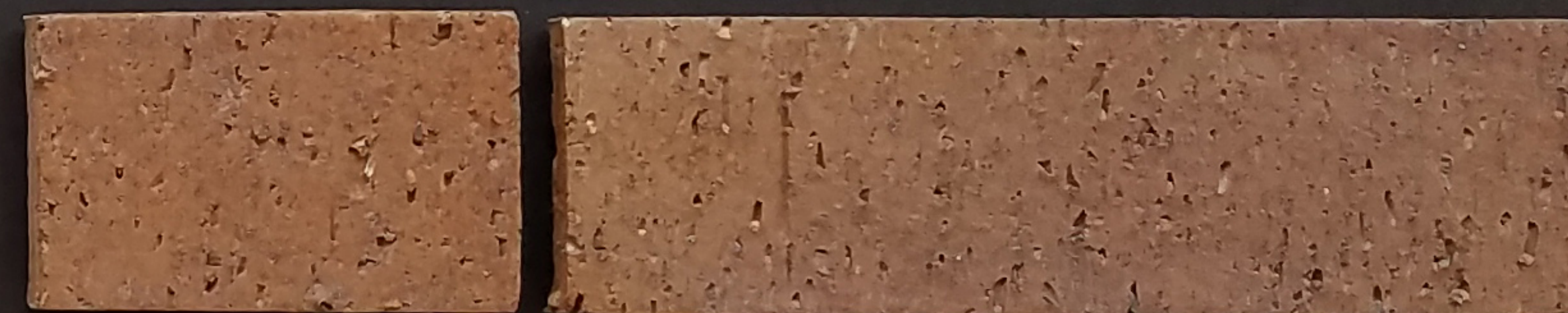
9 FIBER CEMENT PANEL
Color 2
Aged Powder



10 VISION GLASS
Lo E Clear



1 BRICK
Color 1 - Dark Brown
Norman Size - Smooth Finish

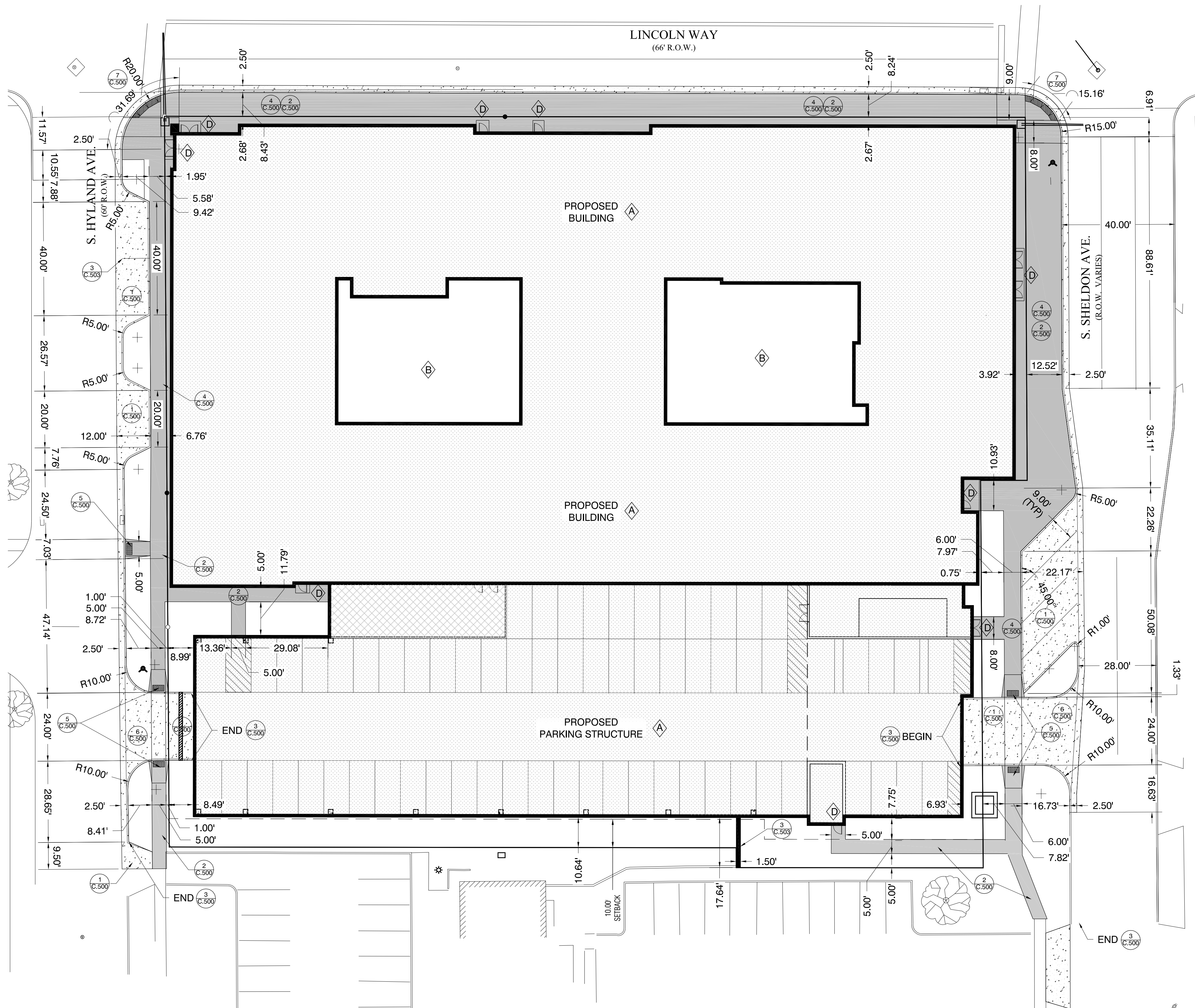


2 BRICK
Color 2 - Medium Red
Norman Size - Velour



3 BRICK
Color 3 - Ash
Norman Size - Velour

PLOT DATE: 9/1/2016 12:08:16 PM



GENERAL LAYOUT NOTES:

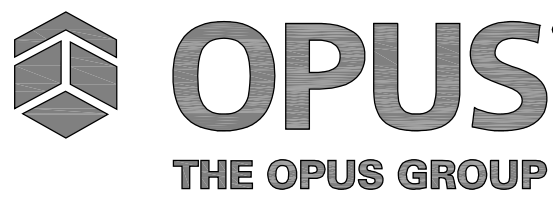
1. ALL DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES & STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH APPROPRIATE SAFETY REGULATIONS.
3. ALL DISTURBED AREAS SHALL BE SEEDED, FERTILIZED & MULCHED UNLESS OTHERWISE NOTED IN PLANS. REFER TO LANDSCAPING PLAN.
4. ANY DAMAGE TO THE PAVEMENT NOT SHOWN FOR REMOVAL ON THE PLANS DUE TO CONSTRUCTION ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE. ALL EFFORTS SHALL BE MADE TO NOT DISTURB EXISTING PAVEMENT.
5. COORDINATE GAS, ELECTRIC, TELEPHONE, & FIBER OPTIC WITH RESPECTIVE UTILITY COMPANIES.
6. TRASH ROOM LOCATED INSIDE BUILDING. REFER TO ARCHITECTURAL PLANS.

CONSTRUCTION NOTES

- Ⓐ REFER TO THE ARCHITECTURAL AND STRUCTURAL PLANS FOR PARKING STRUCTURE SUBGRADE, SUBBASE, FOUNDATION, AND SLAB DETAILS. REFER TO MECHANICAL PLANS FOR PARKING GARAGE SPINKLER SYSTEM (PER 2009 INTERNATIONAL FIRE CODE STANDARDS)
- Ⓑ LEVEL 1 COURTYARD. REFER TO ARCHITECTURAL PLANS.
- Ⓒ RELOCATE TRAFFIC SIGNAL POLE (BY CITY). SETBACK REQUIREMENTS LINCOLN WAY (ARTERIAL 30MPH) = 7 FT (MIN) FROM EDGE OF TRAVELED WAY
S. SHELDON AVE. & S. HYLAND AVE. (25MPH COLLECTOR) = 5.5 FT (MIN) FROM EDGE OF TRAVELED WAY
- Ⓓ INSTALL STOOPS. REFER TO ARCHITECTURAL PLANS

CONSTRUCTION DETAILS

- 1 INSTALL 7" PCC PAVEMENT
- 2 INSTALL 5" PCC SIDEWALK
- 3 INSTALL STANDARD 6" CURB & GUTTER UNIT
- 4 INSTALL CLASS A SIDEWALK
- 5 INSTALL ACCESSIBLE SIDEWALK RAMP
- 6 INSTALL CONCRETE DRIVEWAY TYPE 'B' WITH RADII
- 7 INSTALL CURB RAMPS FOR CLASS A SIDEWALK
- 8 BLOCK RETAINING WALL



Opus AE Group, L.L.C.
10350 Bren Road West
Minnetonka, MN 55343-0110
952-696-4444

Opus Design Build, L.L.C.
1820 NW 118th St. Suite 150
Clive, IA 50325
515-440-1482

CONSULTANT
CLAPSADDLE-GARBER
ASSOCIATES, INC.
1523 S. BELL AVE., SUITE 101
AMES, IOWA 50010

PROJECT
**River Caddis
Mixed-Use
Development**
PROJECT ADDRESS
2700 LINCOLN WAY
AMES, IOWA

PROJECT NUMBER
30984

ISSUE RECORD
09/06/2016 Minor Site Dev Plan

**NOT FOR
CONSTRUCTION**

DATE
09/06/16
PROJECT MANAGER
30984
DRAWN BY
LAW / RWA
CHECKED BY
MMB

REGISTRATION

SHEET TITLE
**LAYOUT
PLAN**

SHEET NUMBER

C.200

CONSULTANT

PROJECT
**River Caddis
Mixed-Use
Development**

PROJECT ADDRESS
2700 LINCOLN WAY
AMES, IOWA

PROJECT NUMBER
30984

ISSUE RECORD
09/06/2016 Minor Site Dev Plan

**NOT FOR
CONSTRUCTION**

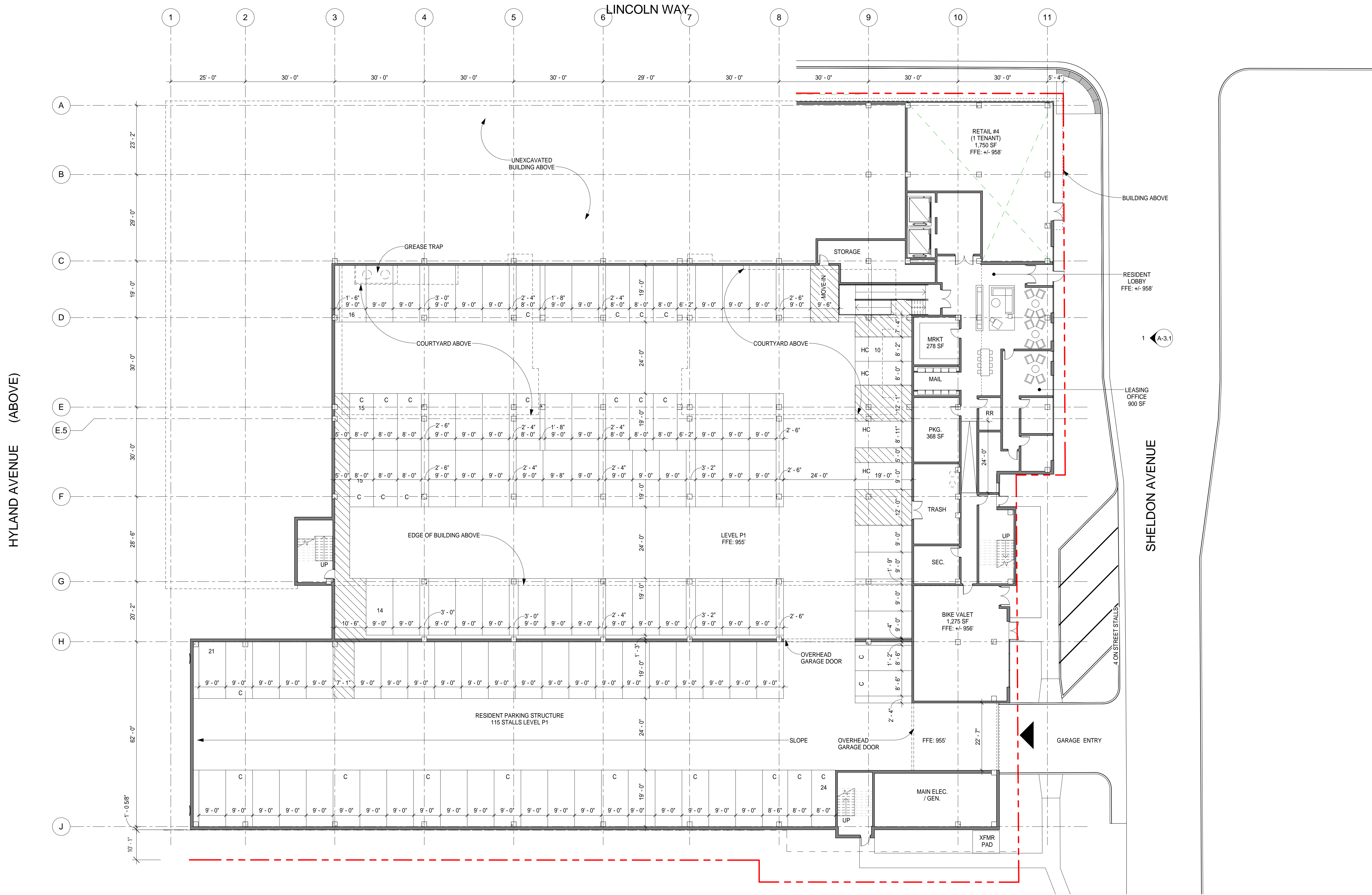
DATE
09/06/16
PROJECT MANAGER
30984
DRAWN BY
M. Hart
CHECKED BY
J. Fourniea

REGISTRATION

SHEET TITLE
**P-1 PARKING
LEVEL PLAN**

SHEET NUMBER

A-2.1



1 LEVEL P1
1/16" = 1'-0"

HYLAND AVENUE (ABOVE)

SHELDON AVENUE

LINCOLN WAY

CONSULTANT

PROJECT

**River Caddis
Mixed-Use
Development**

PROJECT ADDRESS
2700 LINCOLN WAY
AMES, IOWA

PROJECT NUMBER
30984

ISSUE RECORD

09/06/2016 Minor Site Dev Plan

**NOT FOR
CONSTRUCTION**

DATE
09/06/16
PROJECT MANAGER
30984
DRAWN BY
M. Hart
CHECKED BY
J. Fourniea

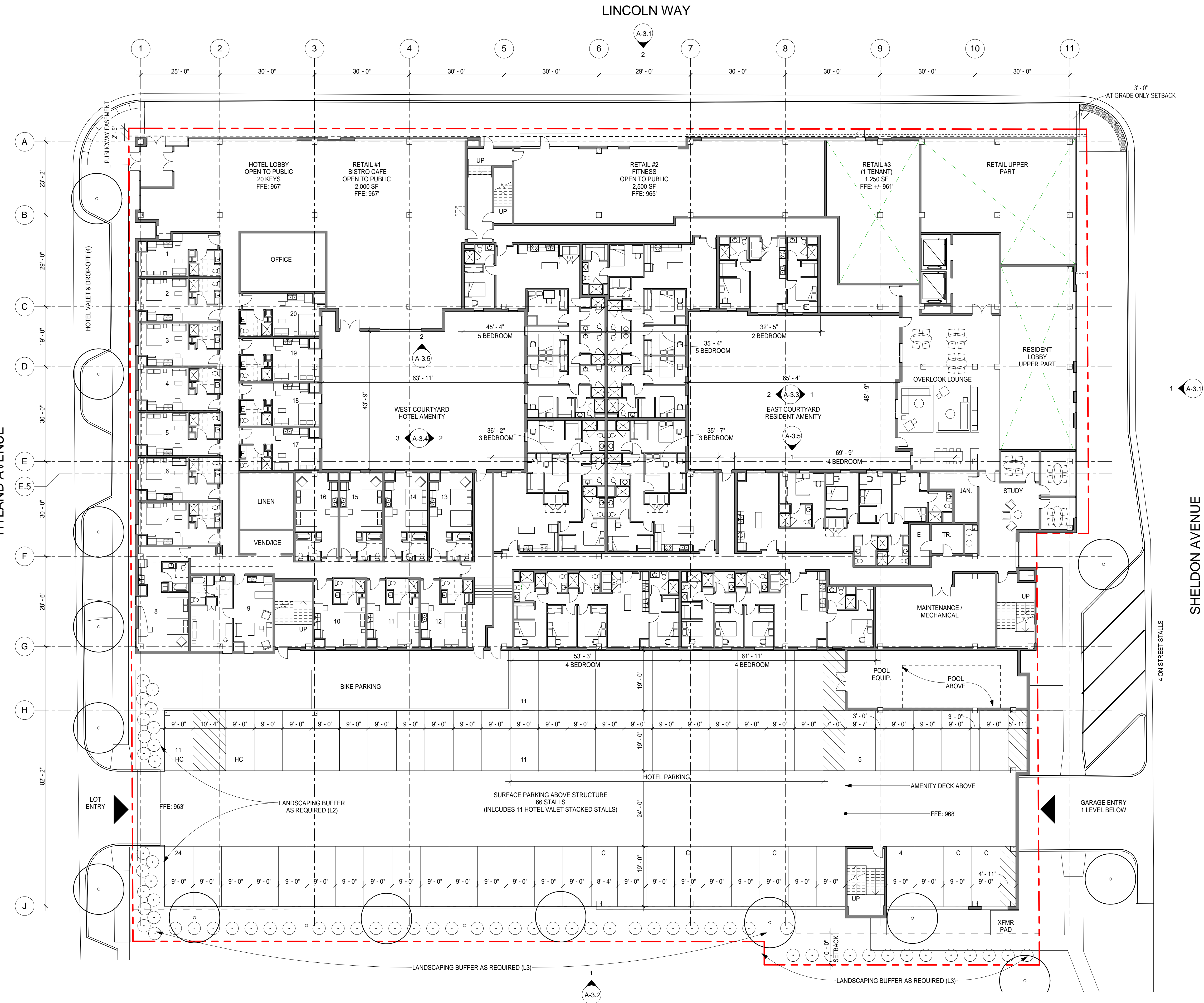
REGISTRATION

SHEET TITLE
LEVEL 1 PLAN

SHEET NUMBER

A-2.2

PLOT DATE: 9/2/2016 3:05:29 PM



1 LEVEL 1
1/16" = 1'-0"

CONSULTANT

PROJECT

**River Caddis
Mixed-Use
Development**

PROJECT ADDRESS
2700 LINCOLN WAY
AMES, IOWA

PROJECT NUMBER
30984

ISSUE RECORD

09/06/2016 Minor Site Dev Plan

**NOT FOR
CONSTRUCTION**

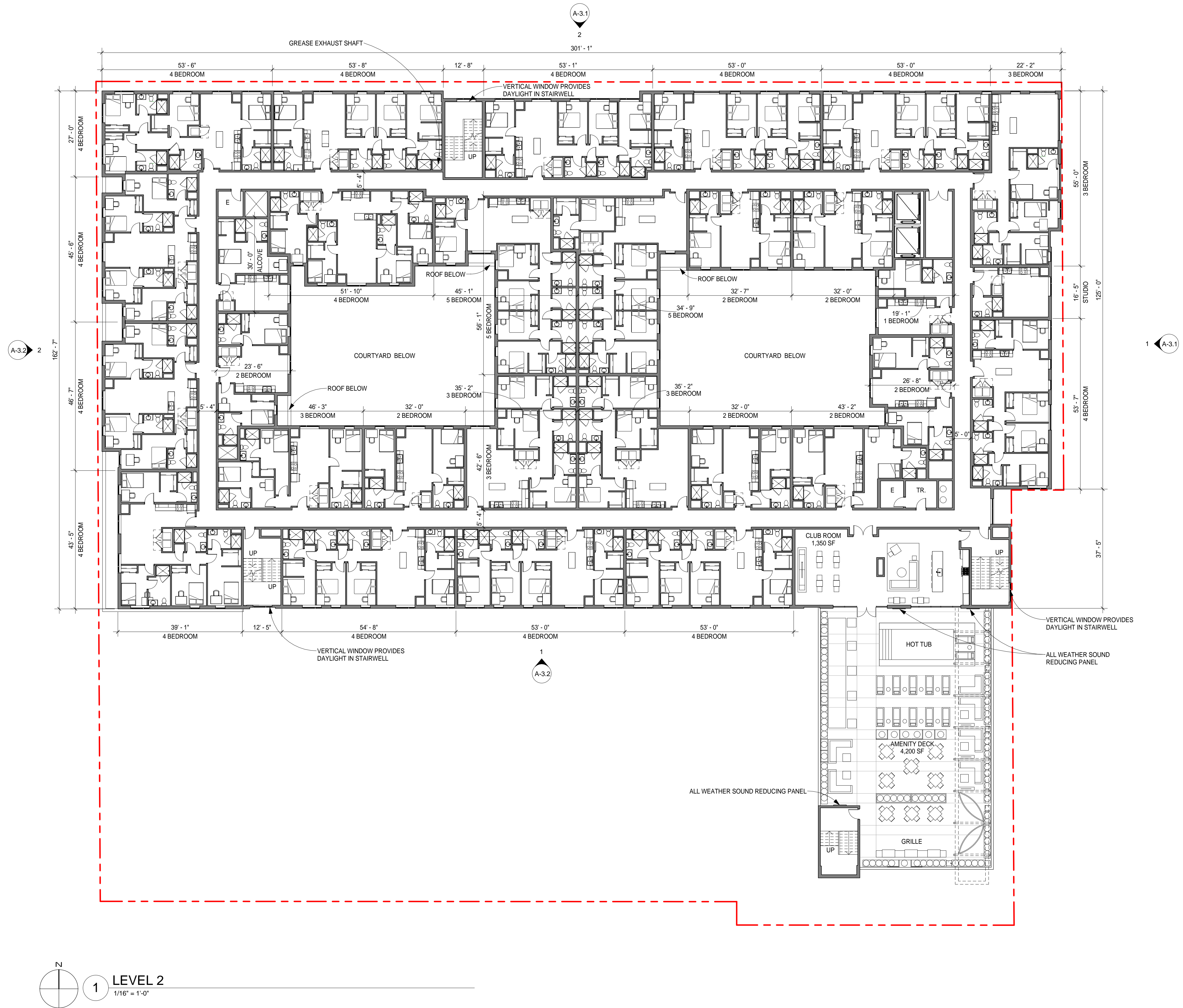
DATE
09/06/16
PROJECT MANAGER
30984
DRAWN BY
M. Hart
CHECKED BY
J. Fourniea

REGISTRATION

SHEET TITLE
LEVEL 2 PLAN

SHEET NUMBER

A-2.3



CONSULTANT

PROJECT

**River Caddis
Mixed-Use
Development**

PROJECT ADDRESS
2700 LINCOLN WAY
AMES, IOWA

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30984

ISSUE RECORD

09/06/2016 Minor Site Dev Plan

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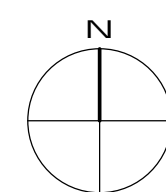
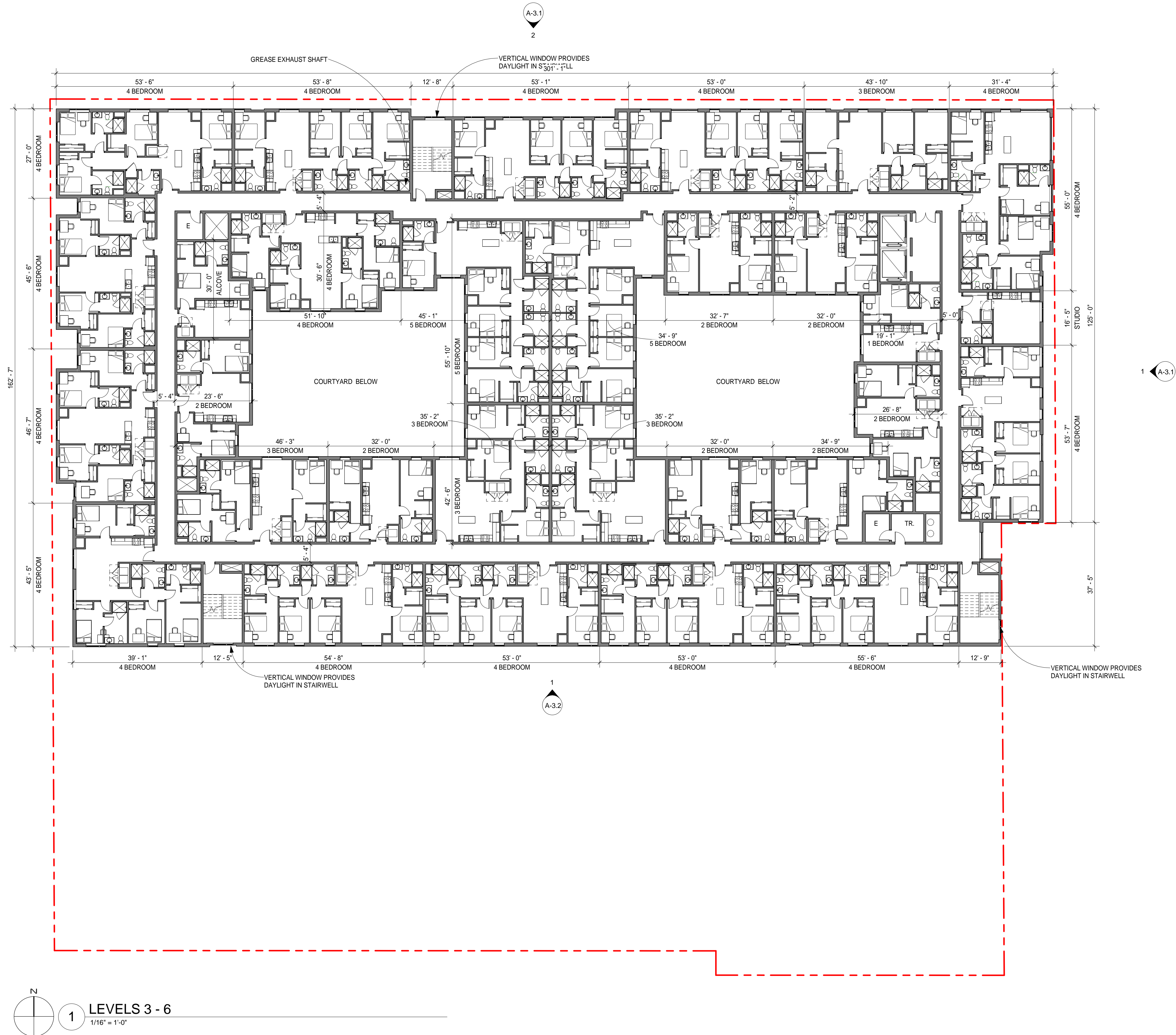
DATE
09/06/16
PROJECT MANAGER
30984
DRAWN BY
M. Hart
CHECKED BY
J. Fourniea

REGISTRATION

SHEET TITLE
LEVELS 3 - 6 PLAN

SHEET NUMBER

A-2.4



1 LEVELS 3 - 6
1/16" = 1'-0"

**NOT FOR
CONSTRUCTION**



EAST ELEVATION 1/16" = 1' - 0"



NORTH ELEVATION 1/16" = 1' - 0"

CONSULTANT

PROJECT

**River Caddis
Mixed-Use
Development**

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REGISTRATION

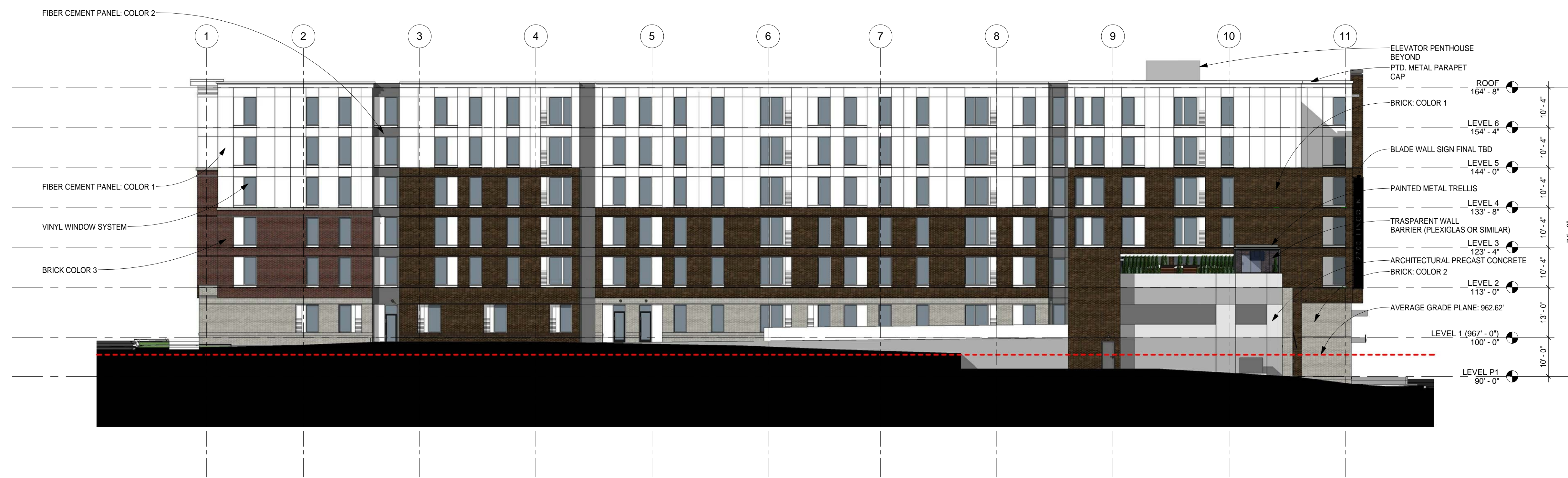
SHEET TITLE
**EXTERIOR
ELEVATIONS**

SHEET NUMBER

A-3.2



WEST ELEVATION 1/16" = 1' - 0"



SOUTH ELEVATION 1/16" = 1' - 0"



THE RUSTY BICYCLE

EXIT

ENTER



COUNCIL ACTION FORM

SUBJECT: SET PUBLIC HEARING DATE FOR URBAN REVITALIZATION AREA AT 2700, 2702, 2718 & 2728 LINCOLN WAY, 112 & 114 S. HYLAND AVENUE, AND 115 S. SHELDON AVENUE

BACKGROUND:

River Caddis, Inc, contract purchaser and developer, are requesting the designation of an Urban Revitalization Area for the properties located at 2700, 2702, 2718 & 2728 Lincoln Way, 112 & 114 S. Hyland Avenue, and 115 S. Sheldon Avenue. The subject site consists of seven properties with a total area of 1.8 acres that are currently zoned Residential High Density (RH) with the University West Impacted Overlay (O-UIW) and Campustown Service Center (CSC). The request for rezoning of the RH properties to CSC is additionally on tonight's Council's agenda for first reading of an ordinance. A location map is included as an exhibit to the Draft URA Plan in Attachment 1.

The history of the site includes a prior use as small commercial retail uses, a gas station, and multi-family housing. The current contract purchaser/developer will remove the existing buildings and consolidate the lots with the intent of developing a single mixed residential and commercial project on the subject area. On June 10th, City Council directed staff to proceed with creation of an Urban Revitalization Area for the project. **In lieu of listing specific design criteria, the Council directed that a site and architectural plan exhibit be the design criteria for the for the revitalization area.**

City Council also reviewed the developer's updated concept on August 9th and gave direction to address the following issues as part of the draft URA Plan.

- 1) Creation of a minimum of one tenant space that is less than 1,200 square feet to support a small business need.
- 2) Creation of a minimum of one full restaurant space with initial installation of mechanical chases and other related improvements for full cooking abilities.
- 3) Utilize public safety measures from the Campustown URA matrix for video surveillance, wider doors, hallways, stairwells, fixed windows, balconies, 4" window openings, and restrictions on access of residents to commercial areas.
- 4) Provided additional details on the rooftop amenity deck and include safety and noise reduction measures, subject to review by the Police Department (see attached concept plan detail)
- 5) Incorporate architectural treatments with clay brick and architectural metals, elements to break up long facades into individual elements, transparent windows, and other elements that enhance and create building identity. (see attached concept plan)
- 6) Sign Program to limit signage to Lincoln Way and Sheldon and to comply with the Campustown URA guidelines.

- 7) Provide a wider sidewalk along Lincoln Way to maximize the available pedestrian space.
- 8) Include a hotel use of a minimum of 20 rooms with on-site staff.

The developer has continued to refine their plan and updated their concept drawings to reflect the comments by the City Council and staff with changes to coloring of the building to browns, patterns of brick, storefront glazing, addition of parapet edges, and awnings. The attached plans represent the site plan for access, parking, and floor plans. Additionally, the architectural elevations are also included. A building materials board example is also included for reference. Below is a current rendering of the project.



A separate plan for the amenity deck is included as well. The amenity deck now includes 4-foot parapet walls with additional plexi-glass or similar transparent barriers along the east and south sides. The design incorporates tensile fabric coverings along the east side and layering of vegetation to protect the edge of the space. The developer has included acoustic mats to help absorb sound. The space also incorporate video surveillance and will have limited hours of operations of 10pm on weekdays and 11pm on weekends. The Police Department has reviewed the concept and believes it is workable with assurances that measures can be taken to modify its design or hours of use if there are frequent incidents regarding the deck.

URBAN REVITALIZATION PLAN:

The draft Urban Revitalization Plan (Attachment 2) meets state requirements and also includes qualifying criteria. These criteria are found in Attachment 3 of the Urban Revitalization Plan and require substantial conformance to the Concept Plans as

depicted in Attachment 4 of the Draft Urban Revitalization Plan and the attached specific criteria for use and design. The draft plan also identifies that it will expire on December 31, 2021, noting that any eligible property will continue to receive previously approved tax abatement.

If City Council is satisfied with the draft Urban Revitalization Plan and qualifying criteria, it must next set a public hearing date to consider approval of the plan.

The next available Council meeting that meets public notice requirements is November 8, 2016. At that meeting, the City Council will conduct a public hearing, act on a resolution to approve the Urban Revitalization Plan, and act on the first reading of an ordinance exempting the improvements from property taxes within the Urban Revitalization Area for projects that meet the qualifying criteria.

ALTERNATIVES:

1. If the City Council accepts the draft Urban Revitalization Plan for 2700, 2702, 2718 & 2728 Lincoln Way, 112 & 114 S. Hyland Avenue, and 115 S. Sheldon Avenue, it can set November 15, 2016 as the date of the public hearing for the adoption of the Plan and the approval of the Urban Revitalization Area.
2. The City Council may amend the draft Urban Revitalization Plan for 2700, 2702, 2718 & 2728 Lincoln Way, 112 & 114 S. Hyland Avenue, and 115 S. Sheldon Avenue and direct staff to set November 15, 2016 as the date of the public hearing for the adoption of the Plan and the approval of the Urban Revitalization Area.
3. The City Council may choose not to proceed with establishing an Urban Revitalization Area for the subject site.

MANAGER'S RECOMMENDED ACTION:

Assuming the City Council is satisfied with the refinements that were made to the design of the project based on the direction that was given in August for drafting an Urban Revitalization Plan for 2700, 2702, 2718 & 2728 Lincoln Way, 112 & 114 S. Hyland Avenue, and 115 S. Sheldon Avenue, it is the recommendation of the City Manager that the City Council adopt Alternative #1 as described above.

ATTACHMENT 1: DRAFT URA PLAN

Urban Revitalization Plan

Approved by the Ames City Council on _____, 2016
In accordance with Chapter 404, Code of Iowa

Legal Descriptions (See Attachment 1: Location Map)

PARCEL 1: 2718 Lincoln Way

LOT ONE (1) IN LEE'S SUBDIVISION OF THE NORTH 213 FEET (EXCEPT THE NORTH 33 FEET) OF THE W3/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, IOWA.

PARCEL 2: 2702 Lincoln Way

BEGINNING AT A POINT ON THE WEST LINE OF THE EAST 2/5 OF THE EAST HALF (E1/2) OF THE NORTHEAST QUARTER (NE1/4) OF THE NORTHWEST QUARTER (NW1/4) OF THE NORTHWEST QUARTER (NW1/4) OF SECTION NINE (9). TOWNSHIP EIGHTY-THREE (83) NORTH. RANGE TWENTY-FOUR (24) WEST OF THE 5TH P.M., CITY OF AMES, STORY COUNTY, IOWA, AND AT THE INTERSECTION OF THE SOUTH LINE OF LINCOLN WAY IN THE CITY OF AMES, IOWA RUNNING THENCE SOUTH 128 FEET, THENCE NORTH 128 FEET, THENCE WEST TO THE PLACE OF BEGINNING.

PARCEL 3: 2700 Lincoln Way

BEGINNING AT A POINT 28 FEET EAST OF THE WEST LINE OF THE EAST 2/5 OF THE EAST HALF (E1/2) OF THE NORTHEAST QUARTER (NE1/4) OF THE NORTHWEST QUARTER (NW1/4) OF THE NORTHWEST QUARTER (NW1/4) OF SECTION NINE (9). TOWNSHIP EIGHTY-THREE (83) NORTH. RANGE TWENTY-FOUR (24) WEST OF THE 5TH P.M., CITY OF AMES, STORY COUNTY, IOWA, AND AT THE INTERSECTION OF THE SOUTH LINE OF LINCOLN WAY IN THE CITY OF AMES, IOWA RUNNING THENCE SOUTH 128 FEET, THENCE EAST 75 FEET, THENCE NORTH 128 FEET, THENCE WEST 75 FEET TO THE PLACE OF BEGINNING.

PARCEL 4: 2728 Lincoln Way

LOTS TWO (2) AND THREE (3) AND THE NORTH 13 FEET OF LOT FOUR (4) OF LEE'S SUBDIVISION OF THE NORTH 213 FEET (EXCEPT THE NORTH 33 FEET THEREOF) OF THE W3/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SECTION 9, TOWNSHIP 83 NORTH, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, IOWA.

PARCEL 5: 112 S. Hyland Avenue

THE SOUTH 47 FEET OF LOT FOUR (4) OF LEE'S SUBDIVISION OF THE NORTH 213 FEET, EXCEPT THE NORTH 33 FEET THEREOF, OF THE W3/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M., AMES, STORY COUNTY, IOWA.

PARCEL 6: 115 S. Sheldon Avenue

OFFICE AND CONDOMINIUM UNITS 101, 102 AND 103 IN TPM CONDOMINIUMS, A CONDOMINIUM IN THE CITY OF AMES, STORY COUNTY, IOWA, TOGETHER WITH ALL APPURTENANCES THERETO INCLUDING AN UNDIVIDED FRACTIONAL INTEREST IN THE COMMON ELEMENTS, AREAS AND FACILITIES AS DETERMINED FOR SAID UNIT BY THE PROVISIONS OF, AND IN ACCORDANCE WITH, THE DECLARATION OF SUBMISSION TO HORIZONTAL PROPERTY REGIME FOR TPM CONDOMINIUMS FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON DECEMBER 30, 2005 AS INST. NO. 05-16146 (AND ANY SUPPLEMENTS AND AMENDMENTS THERETO). TPM CONDOMINIUMS, AS PRESENTLY CONSTITUTED, IS LOCATED ON A PART OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M., IN THE CITY OF AMES, STORY COUNTY, IOWA; DESCRIBED AS FOLLOWS; COMMENCING AT A POINT ON THE EAST LINE OF HYLAND AVENUE, IN THE CITY OF AMES, WHICH IS 257.5 FEET SOUTH OF THE SE CORNER OF THE INTERSECTION OF LINCOLN WAY AND HYLAND AVENUE; THENCE S89°31'30"E, 199.3 FEET ALONG THE NORTH LINE OF THE SOUTH 370 FEET OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF SEC. 9-T83N-R24W OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, IOWA, THENCE NORTH 129.6 FEET ALONG THE WEST LINE OF THE E2/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF SAID SEC 9, THENCE S89°29'40"E, 87 FEET ALONG A LINE WHICH IS 128 FEET SOUTH OF A PARALLEL TO THE SOUTH LINE OF LINCOLN WAY, THENCE SOUTH, 136.6 FEET, THENCE N89°29'40"W, 87 FEET, THENCE NORTH, 7 FEET ALONG THE WEST LINE OF THE E2/5 OF THE E1/2 OF THE NE1/4 OF THE NW1/4 OF THE NW1/4 OF SE. 9 TO THE POINT OF BEGINNING. PARCEL CONTAINS 11,878 SQUARE FEET, OR 0.273 ACRE.

PARCEL 7: 114 S. Hyland Avenue

SOUTH 77.5 FEET OF NORTH 290.5 FEET OF WEST 3/5THS PF EAST HALF (E1/2) NORTHEAST QUARTER (NE1/4) NORTHWEST QUARTER (NW1/4) NORTHWEST QUARTER (NW1/4) OF SECTION 9, TOWNSHIP 83 NORTH, RANGE 24 WEST OF THE 5TH P.M., AMES, STORY COUNTY, IOWA

Owners and Addresses

Property	Owner Name	Owner Address
Parcel 1	TOMCO, LLC	414 S. Duff Avenue Ames, IA 50010
Parcel 2	TOMCO, LLC	414 S. Duff Avenue Ames, IA 50010
Parcel 3	TOMCO, LLC	414 S. Duff Avenue Ames, IA 50010
Parcel 4	Quam Limited Partnership	390 Oakwood Trail

		Fairview, TX, 75069
Parcel 5	Quam Limited Partnership	390 Oakwood Trail Fairview, TX, 75069
Parcel 6	TPM Properties, Inc	115 S. Sheldon Avenue, Suite 100 Ames, IA 50014
Parcel 7	Bird Dog One Cooperative Housing	2115 Ashmore Circle Ames, IA 50014

Assessed Valuations

Property	Land Value	Building Value	Total Value
Parcel 1	245,100	54,900	300,000
Parcel 2	62,900	102,000	164,900
Parcel 3	199,300	117,900	317,200
Parcel 4	184,200	55,700	239,900
Parcel 5	41,900	83,100	125,000
Parcel 6	127,600	246,300	373,900
Parcel 7	159,600	399,000	558,600

Zoning and Land Uses (See Attachment 2: Zoning of Proposed URA)

Property	Existing		Proposed	
	Zoning	Land Use	Zoning	Land Use
Parcel 1	CSC	Commercial: Retail	CSC	Commercial\Residential Mixed Use
Parcel 2	CSC	Commercial: Retail	CSC	Commercial\Residential Mixed Use
Parcel 3	CSC	Commercial: Retail and Gas Station	CSC	Commercial\Residential Mixed Use
Parcel 4	RH/O-UIW	Multi-family Residential	CSC	Commercial\Residential Mixed Use
Parcel 5	RH/O-UIW	Multi-family Residential	CSC	Commercial\Residential Mixed Use
Parcel 6	RH/O-UIW	Multi-family Residential	CSC	Commercial\Residential Mixed Use
Parcel 7	RH/O-UIW	Multi-family Residential	CSC	Commercial\Residential Mixed Use

HOC= Highway Oriented Commercial
RH=High Density Residential
O-UIW=West University Impacted Overlay

Proposed Expansion of Services

The proposed urban revitalization area will continue to receive all services from the City of Ames. There is no proposed extension or increase in the level of service.

Applicability

Revitalization is applicable only to new construction and only in conformance with the approved site development plan and that the principal buildings have received building certificates of occupancy. Revitalization is available to all allowed uses on the site that meet the qualifying criteria found in Attachment 3 of this Plan.

Relocation Plan

There are four properties within the boundaries of the URA that include residential units. No relocations are anticipated, in the event relocation is necessary it is at the sole expense of the property owner.

Tax Exemption Schedule

The property owner may choose one of the following options:

All qualified real estate located in the designated Urban Revitalization Area is eligible to receive a partial exemption from taxation on the Actual Value added by the improvements as specified by the schedules below. Any qualified real estate may elect one of the three schedules.

The exemption period for ten (10) years. The amount of the partial exemption is equal to a percent of the Actual Value added by the improvements, determined as follows:

For the first year	80%
second	70%
third	60%
fourth	50%
fifth	40%
sixth	40%
seventh	30%
eighth	30%
ninth	20%
tenth	20%

The exemption period for five (5) years.

For the first year	100%
second	80%
third	60%
fourth	40%
fifth	20%

Required Increase in Valuation

The project shall require an increased in assessed valuation of at least fifteen percent.

Federal, State or Private Funding

No federal, state, or private funding (other than the developer's financing) is anticipated for this project.

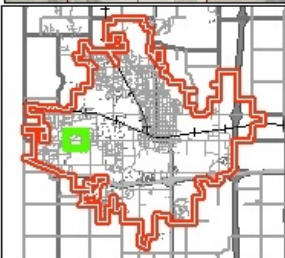
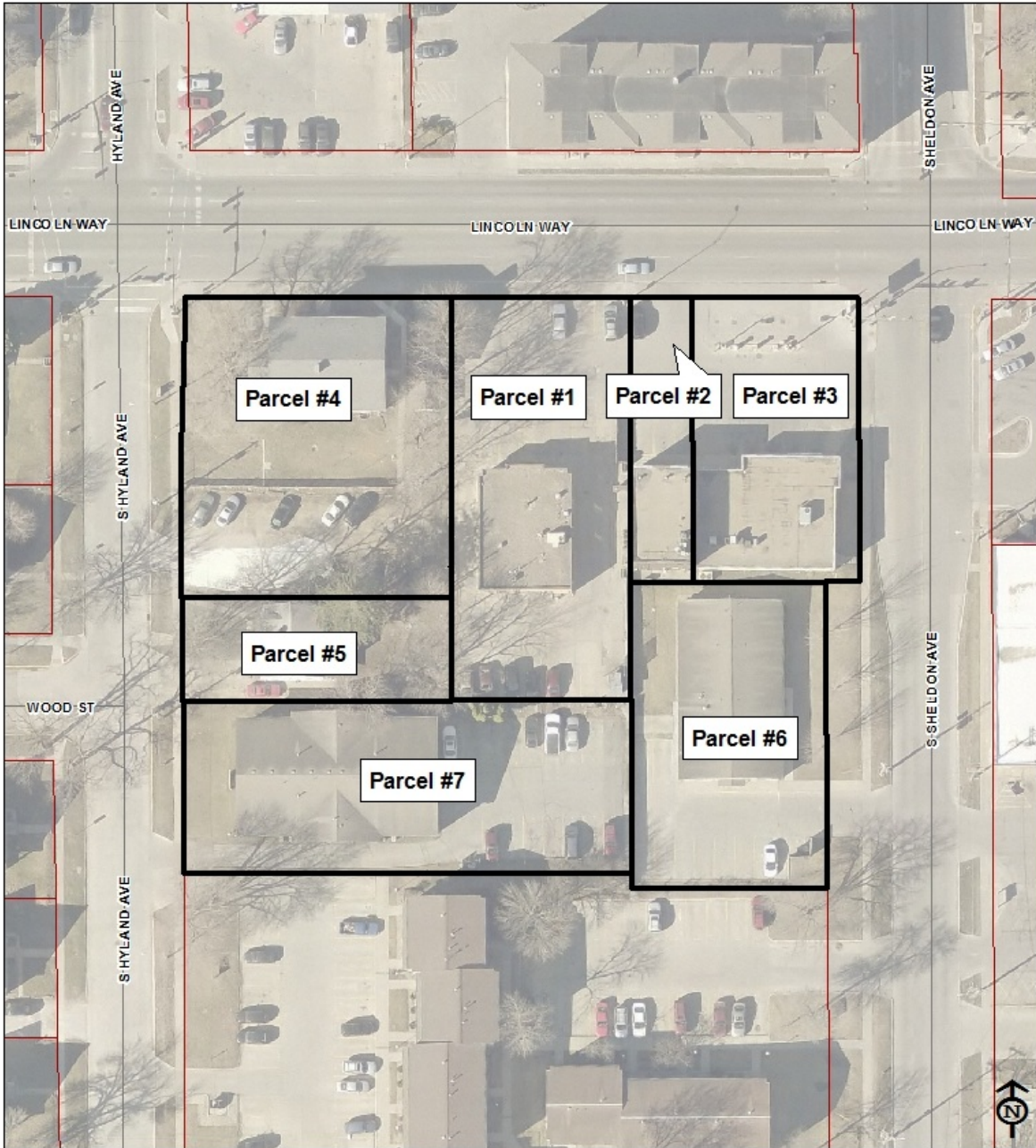
Duration

The Urban Revitalization Area shall expire on December 31, 2021. All projects seeking tax abatement must have been completed prior to expiration. Projects already determined to be eligible for tax abatement shall continue to receive tax abatement consistent with the chosen schedule for abatement and in accordance with state law.

Additional Criteria

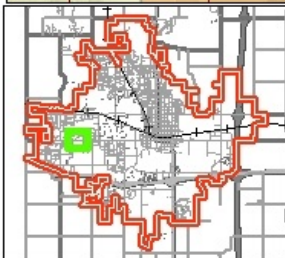
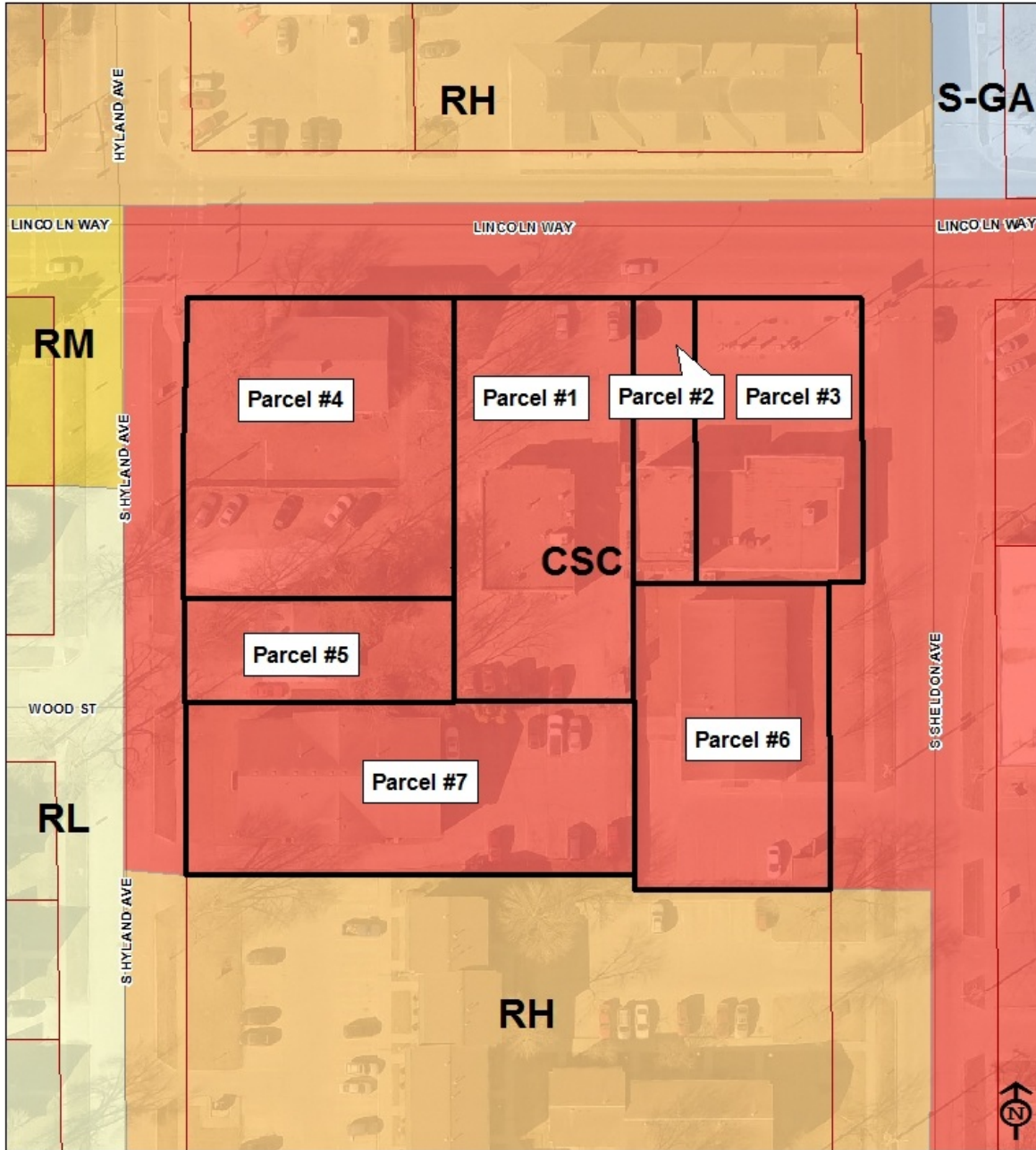
In order to be eligible for tax abatement, a project must be consistent with City ordinances and also meet the criteria in Attachment 3.

ATTACHMENT 1: LOCATION OF PROPOSED URBAN REVITALIZATION AREA



Location Map
2700, 2702, 2718 & 2728 Llincoln Way,
112 & 114 S. Hyland Avenue, and
115 S. Sheldon Avenue

ATTACHMENT 2: ZONING OF PROPOSED URBAN REVITALIZATION AREA



**Proposed Zoning Map
2700, 2702, 2718 & 2728 Lincoln Way,
112 & 114 S. Hyland Avenue, and
115 S. Sheldon Avenue**

ATTACHMENT 3: ADDITIONAL QUALIFYING CRITERIA

- A. The project shall be built in substantial compliance with the approved Concept Plan for site layout and architectural appearance and amenity deck as depicted in Attachment 4: Concept Plan.
- B. The project must be constructed and maintain enhanced public safety features as follows:
 - 1. Limit commercial space in the same building to the ground floor.
 - 2. Provide separate entrances for commercial and residential uses.
 - 3. Residential entrances are visible from the street and provide secure access.
 - 4. Prevent access from the exterior to the interior through doors that serve only as fire exits.
 - 5. Prohibit public access to structured parking, using overhead door and secure access control.
 - 6. Provide interior transparent glass windows into all stairwells.
 - 7. Provide camera monitoring of all pedestrian and vehicle entrances and areas.
 - 8. Minimum widths of all exit routes: 48” for halls, 42” for doors, 60” between rails for stairs.
 - 9. No balconies are permitted.
 - 10. Provide for natural daylight requirements of applicable codes with exterior windows with an allowance for internal bedrooms to have a transom or approved equivalent to meet this requirement.
 - 11. On facades facing any street use only fixed windows, note modified tamper resistant windows do not comply. Facades above the amenity deck must also be fixed windows.
 - 12. All other windows must be designed to prevent passing of sphere larger than 4” diameter. The window must be manufactured to restrict opening of the window permanently , modified windows with restrictors or tamper proof screws do not comply.
 - 13. Prevent by physical means access to all roofs.
 - 14. Where access is not required, provide security fencing controlling access to all areas between new or existing buildings.
 - 15. Provide exterior lighting along the commercial facades and residential entrances.
- C. The project must include the installation of necessary mechanical vent/exhaust equipment for at least one full kitchen restaurant use.
- D. The commercial area of the floor plan must have one tenant space that does not exceed 1,200 square feet and is available for general lease by a permitted commercial or retail use. Accessory functions or related businesses to the apartments or hotel do not qualify as meeting this requirement of available for general lease.
- E. Provide a minimum of at minimum of 10-feet of sidewalk clearance along Lincoln Way and maximize this distance to a minimum of 13 feet or more if approved for

exceptions to parking lot design.

- F. The project shall utilize a Sign Program for commercial tenants that provide a cohesive design and lighting style to the site. Sign Program will allow for wall signage per the Sign Code, with no commercial signage along the Hyland or rear façade of the building. The Sign Program must be approved by the Planning Director.

COUNCIL ACTION FORM

REQUEST: REZONE OF PROPERTY AT 3115, 3119, 3301, 3325, 3409, AND 3413 SOUTH DUFF AVENUE FROM HIGHWAY-ORIENTED COMMERCIAL (HOC) AND AGRICULTURE (A) TO HIGH DENSITY RESIDENTIAL (RH) AND HIGHWAY-ORIENTED COMMERCIAL (HOC) WITH A MASTER PLAN

BACKGROUND:

The owners of parcels addressed as 3115, 3119, 3301, 3325, 3409, and 3413 South Duff Avenue, Dickson and Luann Jensen, have requested rezoning of this land comprising a total of 45.92 gross acres. Two of the properties (3301 and 3325) are currently zoned Agriculture (A) and the remaining four are zoned Highway-Oriented Commercial (HOC). (See Attachment A for current zoning and location.) This site was recently the subject of a Land Use Policy Plan Amendment designating much of the area for high density residential development, while retaining a portion of South Duff Avenue frontage as Highway-Oriented Commercial. **The proposed rezoning includes a contract to address terms of needed public improvements in the area and for limitations on use of the site.**

These properties lie along the west side of South Duff Avenue (US Highway 69). The subject site has two separate frontages on South Duff Avenue. The site lies north, west, and south of Story Memorial Gardens, a private cemetery at 3215 South Duff Avenue. The north boundary of the site lies near the Crystal Street intersection with South Duff Avenue. The south boundary of the site lies immediately north of the New Life Church at 3505 South Duff Avenue. The property extends westward to the Ames Municipal Airport property.

The applicant has submitted a rezoning request for 41.30 acres as High Density Residential (RH) with 4.62 acres remaining as Highway-Oriented Commercial (HOC). A master plan was also submitted with the application indicating approximately 500-700 apartment units on the RH portion with commercial to be located at the new intersection with Crystal Street. Crystal Street would provide a northern access point to the project for a looped internal street system that would circulate through the site to a second entrance onto South Duff Avenue at the south end of the site. See Attachment B for the proposed master plan.

The plan reflects conversations with the developer from the past year for a project that would focus only on 1 bedroom and some 2 bedroom unit apartments designed with additional on-site amenities that would be marketable and desirable to a range of households. The developer intends to construct a mix of one and two bedroom apartments that will total approximately 700 beds. The commercial component is also in keeping with prior direction by staff that an area for small scale commercial should be maintained to meet the general needs of the broader area. Upon rezoning of the site to

RH, the development will be subject to administrative approval of a Minor Site Development Plan prior to initiating construction on the site.

The proposed rezoning boundaries do not follow the current property lines. Rather, they are described by metes and bounds irrespective of property lines. The intent is to replat the properties (either by plat of survey or subdivision plat) in the near future to reflect the new zoning boundaries and future development lots.

The attached addendum includes a full description of the master plan and analysis of the rezoning proposal, including a review of needed infrastructure to support the proposed development. The analysis relies upon the previously completed traffic, stormwater, and sewer evaluation that were part of the LUPP amendment approval. The analysis also includes the RH checklist that includes evaluation of a number of issues related to consistency with the LUPP. The checklist was also part of the LUPP amendment process, the most substantial differences in the evaluation relate to having more project specific building information from the Master Plan and commitments from the developer on features of the project as part of the contract rezoning.

As described at the time of the LUPP amendment, more details about infrastructure improvements and commitments for improvements were needed prior to rezoning of the site. **In order to facilitate the installation of the needed infrastructure to meet existing needs, future needs, and needs brought on by this specific development, the City and the developer have prepared a contract rezoning document to identify the specific improvements that must take place and the party responsible to pay for those projects.**

In brief, these improvement obligations for the developer and the City are as follows:

- The Developer will construct storm water improvements as identified in the City's Teagarden Drainage Study, in addition to meeting his own stormwater needs (Developer cost).
- The Developer will install a 5-foot sidewalk along his frontage, as well as along the cemetery frontage (Developer cost).
- The intensity of use will be limited to 700 dwelling units configured as one and two bedrooms apartment dwellings. The project design will incorporate brick materials and on-site amenities with the phasing of residential development.
- The City will widen South Duff Avenue to three lanes from Jewel to Garden Road with the intent to extend it to Ken Maril Road. (City cost).
- The City will install a traffic signal at the Crystal Street intersection (City cost).
- The City will install a shared-use path along the east side of South Duff Avenue from the State Nursery to Jewel with the intent to extend it to Ken Maril Road (City cost).
- In the event the Ames' electric poles on the east side of South Duff require

relocation, the costs will be split 50/50 (not to exceed \$375,000 for the Developer).

The pages 8 & 9 of the Addendum include a more complete explanation of the rationale for these trade-offs. Attachment G is the draft rezoning contract.

Planning and Zoning Commission Recommendation: At a public hearing held on September 7, 2016, the Planning and Zoning Commission voted (5-2) to recommend that the City Council rezone the subject properties with conditions. There were no comments from immediate neighbors at the meeting. One person spoke in favor of the project, noting that the collaborative efforts of the City and the Developer could solve the storm water issues experienced by area homeowners on the east side of South Duff Avenue. Commissioners expressed concern about the size of the development and the impacts on traffic and storm water and how well it rated on the RH checklist. Other commissioners were satisfied that the recommended infrastructure, once installed, will improve existing conditions and could provide for more housing choices within the City.

ALTERNATIVES:

1. The City Council can approve the request for contract rezoning from Agriculture and Highway-Oriented Commercial to High-Density Residential and Highway-Oriented Commercial with a master plan that includes the improvement obligations noted above.
2. The City Council can deny the request for contract rezoning from Agriculture and Highway-Oriented Commercial to High-Density Residential and Highway-Oriented Commercial with the attached master plan.
3. The City Council can defer action on this request and refer it back to City staff and/or the applicant for additional information or with specific direction for changes.

MANAGER'S RECOMMENDED ACTION:

The need for additional housing opportunities was reviewed in conjunction with the LUPP amendment earlier this year. With the ongoing needs for versatile housing options that meet workforce needs, there was support for development of the subject site. The Developer has now proposed a concept that demonstrates how that need is met and a commitment to a specific project. The master plan and contract rezoning elements capture the basic development details for the project with up to 700 units and 1 and 2 bedroom configurations of apartments. Council should note that the language of the contract addresses only minimal details in terms of intensity of use, a requirement for a substantial amount of brick, and a condition that the Developer provide amenities consistent with the conceptual development of the site articulated by the Developer.

The Developer contends the proposal meets the housing interests of the City in that the site itself is adjacent to services and jobs, which are desirable traits. The Developer also believes that, although it is not incorporated into an existing neighborhood, the style and character of the project will yield a desirable living environment despite the potential

undesirable compatibility issues with the external uses of the airport, commercial development in the area, and a cemetery. The proximity of the airport is clearly a unique issue with this request and how its operations impact the desirability of the site for future residents.

As discussed at the time of the LUPP amendment, traffic and storm water improvements are needed to serve this development and to support the broader area. Storm water can be improved with the joint actions of the City and developer based on the recommendations of the Teagarden Storm Water Study. With the specific identified improvements of the Traffic Impact Study, levels of service along South Duff Avenue can be improved and maintained from the opening day of the development through at least year 2035. The recommended conditions for traffic improvements create a list of feasible improvements that can be completed in the near term, rather than asking for fair share contributions to a variety of projects that are not yet part of the City's Capital Improvement Program.

Ensuring that traffic and storm water improvements are completed along with future development of the site is critical to the evaluation of the request. To that end, the City Council should only approve the rezoning with a rezoning contract detailing specific improvements and the responsible party. In the event the City Council does not want to partner in the trade-off of storm water facility improvements for transportation improvements, alternative contract details for the developer's transportation improvement obligations should be agreed upon prior to rezoning of the site.

Staff believes, however, that it is mutually beneficial to divide the responsibilities as described in the attached contract rezoning agreement, since the City would have costs of approximately \$900,000 for storm water improvements on its own that are being traded for transportation improvements along South Duff that may range between \$576,000 and \$951,000, depending on the final design of improvements and the award of Iowa Department of Transportation grants.

Therefore, it is the recommendation of the City Manager that the City Council accept Alternative #1, thereby approving the rezoning from Agriculture and Highway-Oriented Commercial to High-Density Residential and Highway-Oriented Commercial with the attached master plan and zoning contract.

ADDENDUM

Existing Land Use Policy Plan. The Land Use Policy Plan future land use map was recently amended to designate much of this area as High-Density Residential from Highway-Oriented Commercial. The South Duff Avenue frontage lying north of the Story Memorial Gardens cemetery remains as Highway-Oriented Commercial. See Attachment D for an excerpt of the LUPP future land use map. The proposed rezoning is consistent with the LUPP designation.

Existing Uses of Land. Land uses that occupy the subject property and other surrounding properties are described in the following table:

Direction from Subject Property	Existing Land Uses
Subject Property	Farmland, three homes
North	Commercial
East	Residential
South	Church, homes
West	Ames Municipal Airport

Existing Zoning. The current zoning of this property is HOC. See Attachment A for a zoning map.

Master Plan. A master plan is intended to provide a general description of the intended development of a property. A master plan must address natural areas, buildable areas, building types, range of uses and basic access points, as described in zoning requirements of Section 29.1507(4) (see Attachment E). In this case the developer identifies the whole site to be developable areas. There is an existing stream channel in the north area of the site that will likely be part of future storm water improvements and not conserved in its current state as it will be altered to meet the development needs of the site. Review and permitting of those specific changes would occur through the site development permit process.

The submitted master plan proposes areas for residential development on 41.30 acres of the property and commercial development of 4.62 acres. These are gross acreages and does not account for the 50-foot right-of-way easement along the US Highway 69 frontage, future storm water detention areas, and any common open space that might be included in the apartment complex.

The master plan proposes between 500 and 700 apartment units. The developer described to the Planning and Zoning Commission an interest in a combination of one and two bedroom apartment configurations for a total of approximately 700 beds in the project. The proposed density of dwelling units is between 11.63 and 17.50 dwelling units per acre. The densities are calculated on gross areas and, as noted above, do not reflect what will be reserved for other uses, thus reducing the developable acres. RH zoning requires a density of between 11.2 and 38.56 units per net acre. When a site

development plan is submitted showing storm water areas, common open space, and other areas that can be netted out of the development, the densities will increase but remain within the allowable range.

Commercial coverage will be evaluated at the time that a site plan for a specific development is submitted. HOC zoning requires only staff review and approval.

The Master Plan has been incorporated in the Contract Rezoning agreement and includes provision for its implementation in the Intensity and Design section. The agreement also specifies the project must include substantial amounts of brick on the exterior facades of each building and that on-site recreational amenities are provided for with residential development. The intent of these provisions is to capture the Developer's statements for the type of project he envisions and how the City has relied upon that description in the evaluation of the rezoning request as the use and design meeting the housing needs of the city.

Access. The Master Plan includes three access points along South Duff Avenue (US Highway 69). The northern access will be at a signalized intersection in alignment with Crystal Street. This will be the main entrance into the development. Due to access management interests along an arterial street, shared commercial and residential access will likely be required in the north part of the site.

A second access point is proposed just north of the property line with the cemetery and will be evaluated in greater detail at the time of site plan review. This second driveway would not be signalized. The southern access will not be signalized, either. It will be located about midway between Jewel Drive and Garden Road, south of the cemetery.

The RH rezoning area includes a 35-foot "flag pole" along the north boundary and another 35-foot "flag pole" immediately north of the north boundary with the cemetery. The flag poles will be platted as part of the residential lots and will provide the necessary frontage for those lots on a public street. These "flag poles" of residential zoning are not needed to ensure access to South Duff as a residential driveway can be configured through a commercial site, but are desired by the applicant to be shown as residential at this time.

Traffic. A traffic study looked at current levels of service, future levels of service (year 2035) without the development, opening day of the development, and future 2035 levels of service with full build-out of the development. The study identified needed improvements along the US Highway 69 corridor as a result of general growth in the community. However, it also identified specific improvements needed to mitigate the impacts of this development on levels of service on this corridor. The study recommends the following improvements:

- Construct a signal at South Duff Avenue and Highway 30 Eastbound ramp.
- Construct a signal at Crystal Street and Highway 30 intersection to serve the new development.
- Extend the three lane cross section south to the new south entrance to the proposed development, or possibly to Garden Road.

- Construct an additional left turn lane from Airport Road to South Duff Avenue.
- Improved pedestrian and shared use path connections along South Duff Avenue.

However, not all of the above projects with the developer's contributions are able to be completed independently or identified within the City's transportation plans. Therefore, as part of the contract rezoning, staff in consultation with the developer, has identified the following full improvements as appropriately addressing the projects transportation needs and impacts on the system. The list below would be the minimum improvements needed to maintain the quality of the transportation adjacent to the site.

- Construct a signalized Crystal Street and US Highway 69 intersection to serve the new development.
- Extend the three lane cross section south through to the new south entrance to the proposed development and through to Garden Road.
- Improve pedestrian and shared use path connections along South Duff Avenue including extending the shared use path on the west side of South Duff Avenue to Jewel Drive.

The rezoning contract specifies that the shared use path, South Duff widening, and Crystal signal are the responsibility of the City and that frontage improvements on the east side of South Duff are the responsibility of the developer. Without the contract as it is written, the developer would be responsible for those improvements that are caused by the development and the City would be responsible for those that are due to existing conditions and the general expected increase in traffic to the year 2035. The discussion below more fully described the costs associated with the intended improvements.

Storm Water Management. There are three drainage areas on the west side of South Duff Avenue that carry stormwater under the road to the east side. Property owners have expressed concerns that development on the west side of South Duff Avenue will exacerbate existing conditions and lead to localized flooding and wet basements. There is also a concern that, infrequently, the highway is closed due to stormwater overtopping the roadway.

The recommendations of the recently completed Tea Garden storm water study offer the maximum amount of flood protection without creating a larger future hazard of impounding water as a dam or creating a body of standing water that would be a flight hazard to airport operations. Using that study, the developer has done further analysis to determine what storage requirements will be needed to achieve the improvements needed by the City and to mitigate the increased runoff due to this development. The findings of the study illustrate that development of the site could be done in a manner that meets City and downstream interests. If, however, the proposed project does not move forward, the Public Works department is prepared to move forward with a project to improve storm water management in this area. If a cost agreement on cost with the developer is not reached, the City and the developer would separately have to undertake storm water improvements.

Other Infrastructure. City sewer and water service are readily available and require

only minor extensions. Extensions from the northwest corner of the site to a larger electrical service in Airport Road are needed as this is on the periphery of the Ames Electric territory. These costs will be the financial responsibility of City of Ames electric service.

Contract Rezoning. At the time of rezoning, Iowa Statute allows for the City Council to consider agreements for limitations on use of property and improvement needs related to the rezoning request and enter into form of a development agreement as a Contract Rezoning.

There are a number of improvements needed to address the increased traffic and provide better access to this development. Completing these improvements comprehensively provides a general benefit that would be delayed through piecemeal implementation of improvements in the area.

Likewise, there are stormwater improvements needed without this development as indicated in the recent Teagarden Drainage Study. The City estimates that the cost for those improvements if borne by the City is approximately \$900,000. The Developer must also accommodate storm water management requirements above and beyond the improvements needed for the City.

City staff and the developer discussed costs and funding mechanisms for the required improvements. The proposed Contract Rezoning spells out obligations for the developer and for the City. **The developer would have no responsibility for improvements identified as City cost.** In summary, the developer will be responsible for the stormwater improvements—both the City's needs and his own needs while the City will be responsible for much of the transportation improvements to the South Duff Corridor.

With any contract that places an improvement burden upon the City, funding sources are important to identify to understand the impacts on different programs within the City from the new obligations. **Additionally, as part of the South Duff improvements, the City intends to work with the Iowa Department of Transportation (DOT) to complete the widening of roadway from not only Garden Road but to Ken Maril Road as a comprehensive transportation improvement that is beyond the direct development impacts identified within the Traffic Study, but is needed as long term improvement for the area.**

The proposed extended improvements that will be the responsibility of the City include:

- Widening of South Duff Avenue to a two-lane cross section with a center turn lane to Ken Maril Road (est. cost \$1,250,000).
- Traffic signal at Crystal Street and South Duff Avenue (est. cost \$350,000).
- Shared use path from State Nursery to Ken Maril Road (est. cost \$376,000).

The total estimated cost is \$1,976,000 for the above improvements if there is no relocation of Ames Electric transmission lines along the east side of South Duff. In preliminary discussion with the DOT, it is believed that the widening of South Duff will

principally affect the west side of the road and the existing high voltage transmission lines along the east side of the road would not be affected. However, a final design has not been completed for the transportation improvements and there could be unforeseen complications that could potentially necessitate relocation of the electric lines. In this case, staff has proposed that the costs of relocation that are estimated at up to \$750,000 be split between the developer and the City with a cap of \$375,000 on the developer's contribution.

City funding sources will likely include specific state grants, one of which the District 1 office of the Iowa Department of Transportation has already made application for on our behalf to support the extended widening length to Ken Maril Road. Of the estimated cost, the IDOT has indicated that up to \$1,400,000 is available in state funds, **leaving \$576,000 for local contributions to transportation improvements.** The local match funding for these projects will most likely be derived from General Obligation bond or Road Use Tax revenues. Funding for the City's match would be identified as part of the Capital Improvement Program (CIP) budgeting. If the rezoning is approved, staff will prepare projects for City Council approval as part of the CIP for 2017-18.

If the state grants are not at the amount expected, the City retains the right to determine the ultimate extent of improvements and their specific timing based upon funding and project cost. Additionally, the DOT must approve the specifications for any widening since South Duff is a federal highway and the City's improvements are subject to DOT approval. **However, in accordance with the proposed contract rezoning, the City will have an obligation for the traffic signal installation regardless of the other South Duff improvements.**

The contract rezoning requires the Developer to assume all costs associated with storm water improvements, not only for his development, but also to satisfy requirements of the Teagarden Storm Water Study related to the site. Other costs to be borne by the Developer includes utility extensions to bring sanitary sewer and water from the east side of South Duff Avenue to the site and a five-foot sidewalk along the Brick Towne frontage as well as along the cemetery frontage. The developer will need to provide the public water mains within the site to serve fire hydrants.

RH Site Evaluation Matrix. City Council directed that proposals for apartment development be evaluated with the RH matrix. The matrix language was designed to articulate the goals and objectives of the LUPP into specific statements relevant to individual sites and to help establish a common context for evaluation of proposals on a case by case basis. **The purpose of the matrix is to evaluate if a specific site is suitable for multi-family development, the matrix does not set a minimum scoring requirement nor does it evaluate if alternative uses are also suitable for the site proposed as residential.** Staff has "rated" the proposed development, which can be found in Attachment F, and described the highlights of its determinations below. Overall, the site received mixed ratings. While there is more detail with the proposed master plan accompanying this rezoning request than there was at the time of the LUPP amendment, certain details are not included which would impact the scoring, especially specific plans for the Housing Type and Design category.

Location/Surroundings: The site is not well integrated into adjoining neighborhoods. To the west is the airport, and the site is separated from the residential neighborhood to the east by Highway 69. The airport runway is approximately 700 feet from the western most property line of the subject area. Other uses on South Duff Avenue are the cemetery and a church. However, the site is rather large and can develop its own neighborhood identity with appropriate design and layout.

Employment opportunities would exist within the proposed and adjacent commercial areas. Other opportunities exist in east Ames, along the South Duff Avenue corridor and at the ISU Research Park to the west. This site would also be attractive for those wishing to commute to Huxley or Ankeny.

Site: As noted previously, the site has three drainage ways that will need to be improved and around which the development needs to be designed. However, there is no designated floodway fringe on the site. There is a stream channel along the north property line of the site that would likely be modified for a new street intersection and enlarged for storm water detention.

The site will be near the Ames general aviation airport. The airport mostly serves propeller-type airplanes and limited jet service (but no scheduled commercial flights). Because of the location of the development site in relation to the runways, there is no identified aviation hazard which could impact airport operations. The site could generate noise complaints depending on the times of flights and sensitivity of residents.

Housing Types and Design: Housing design is not generally required as part of a rezoning request beyond the description of building types and intensity of use for a Master Plan. But based on our discussion with the developer, the façade materials will likely contain a substantial amount of brick and a commitment to a number of on-site amenities to differentiate the proposed project from other projects in the City. That discussion generated one of the conditions of approval. Other information is not available—thus the low scores on those criteria. The developer proposes to target these apartment units to a workforce tenant. He anticipates mostly one-bedroom units in buildings with up to 72 units in any one building. The statement of intent for smaller unit sizes could be viewed as supporting a needed housing type at the rezoning stage and was rated high in accordance with our desire to broaden housing options from recent trends. Development is not anticipated to be low or moderate income housing.

Transportation: For transit access, staff rated the site as average due to the front half of the site being within a ¼ mile distance of the bus stop. However, the majority of actual apartment unit locations are likely to end up being further than the ¼ mile target, unless CyRide establishes a new bus stop along South Duff Avenue. The nearest Yellow route stop is at the intersection of Jewel Drive and South Duff Avenue. Staff rated the quality of service for the Yellow route as low reflecting the current use of a small bus on the route. With the Yellow route, a person may take a bus and make connections during morning hours or after work to meet an 8 to 5 work schedule. Very limited midday service is provided by the Gray route, and there is no evening or Sunday service.

With added sidewalk and shared use path improvements on South Duff Avenue, access to the north should be much improved. The development should have a well-defined internal shared-use and sidewalk system in order to safely direct users to the South Duff Avenue corridor. Under current conditions though, it has poor connectivity and was rated low.

The commitments that will need to be made for traffic improvements on South Duff Avenue and the various intersections are critical to assuring an improved level of service on this corridor. And the timing of those improvements should be such that they are ahead of the future forecasted demand. With future improvements along the Highway 69 corridor and the site's frontage, this criterion would score higher. The primary concern would be the offsite intersection service levels in the cumulative growth scenarios, with or without this project.

Public Utilities/Services: Staff evaluation of sanitary sewer capacity and water usage does not indicate any constraints for the development. The recommendations of the Teagarden Drainage Study, when implemented with the development, should improve current stormwater drainage issues.

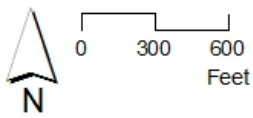
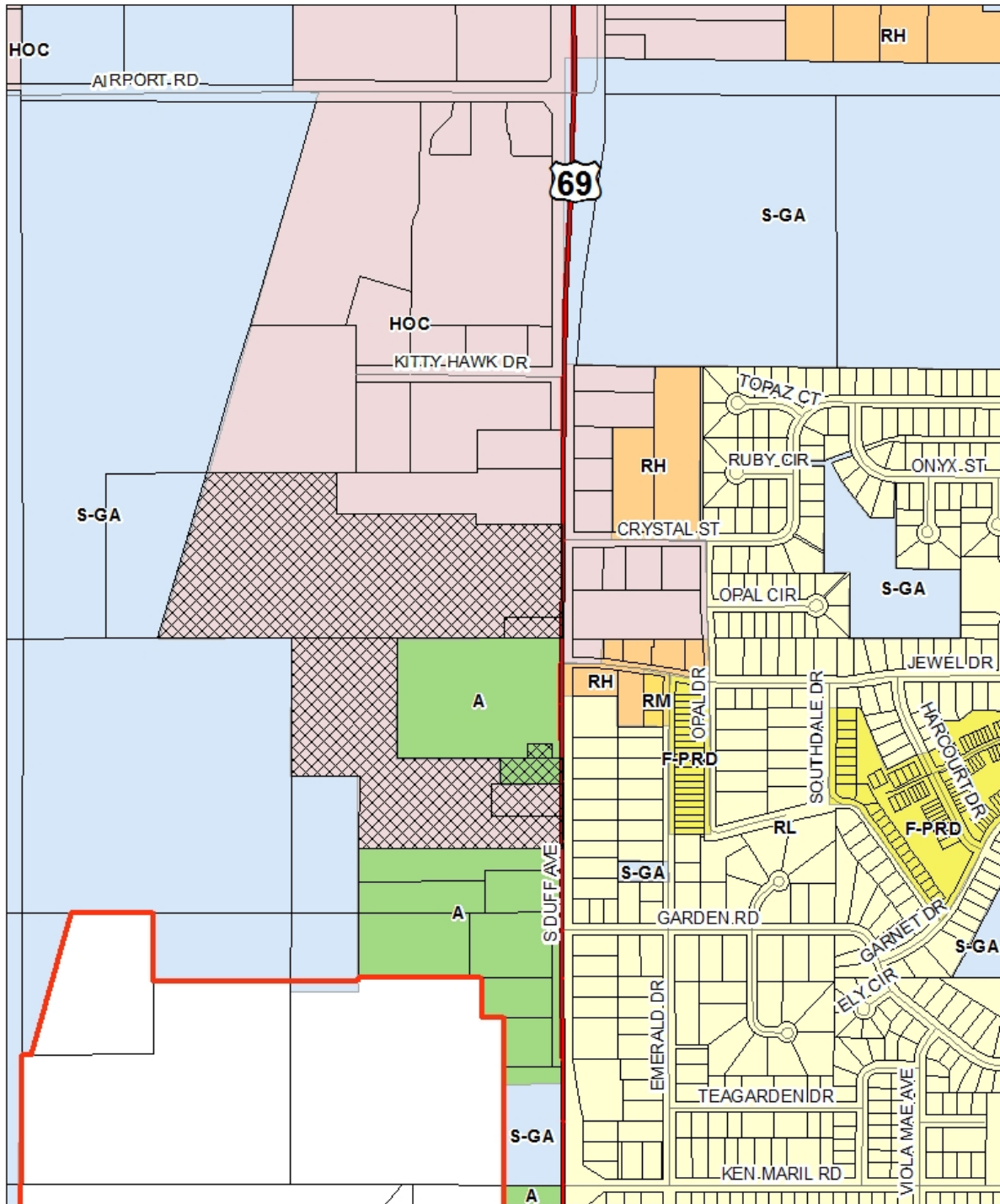
The fire station lies only 3,500 feet north of the southern-most extent of the development. Fire response time should be minimal.

Investment/Catalyst: The City never envisioned this as a potential residential neighborhood before this development was proposed. The size of it could allow it to be a self contained or private community, thus it may become an identifiable development even though its sits outside of an indefinable neighborhood.



Retaining a commercial area at the front will provide shopping and employment opportunities to serve this neighborhood as well as serving the 12,000 vehicles (2011 IDOT traffic count) per day on this section of US Highway 69.

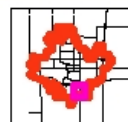
Public Notice. Notice was mailed to property owners within 200 feet of the subject site and a sign was posted on the subject property. As of this writing, no comments have been received.

ATTACHMENT A: CURRENT ZONING AND LOCATION

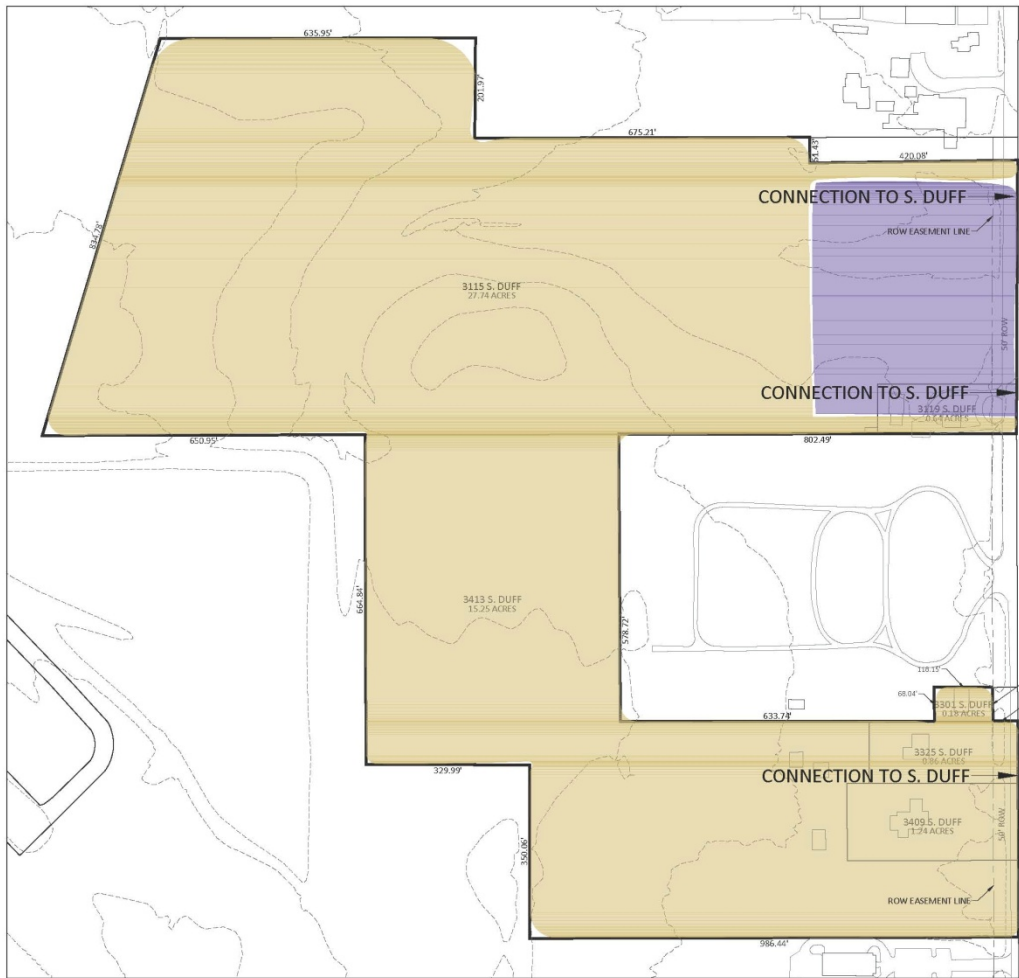


Legend

-  Subject Site
-  Ames City Limits



ATTACHMENT B: PROPOSED MASTER PLAN [NORTH TO THE LEFT]



MASTERPLAN BRICKTOWNE DEVELOPMENT Jensen Group

LEGAL DESCRIPTIONS

3301 S. DUFF
A PART OF THE NORTHEAST QUARTER (NE ¼) OF THE NORTHWEST QUARTER (NW ¼) OF SECTION 23, TOWNSHIP 83 NORTH, RANGE 24 WEST OF THE 5TH P.M., IN THE CITY OF AMES, STORY COUNTY, IOWA DESCRIBED AS FOLLOWS:
 BEGINNING AT A POINT ON THE WESTERLY RIGHT OF WAY OF DUFF AVENUE AND U.S. HIGHWAY 469 WHICH IS 50 FEET WEST AND 511.1 FEET SOUTH OF THE NE CORNER OF THE NORTHEAST QUARTER (NE ¼) OF THE NORTHWEST QUARTER (NW ¼), THENCE CONTINUING SOUTH ALONG SAID RIGHT OF WAY 68 FEET, THENCE NORTH 89°50' W 118 FEET, THENCE NORTH 68 FEET, THENCE S 89°50' E 138 FEET TO THE POINT OF BEGINNING.

3325 S. DUFF
BEGINNING AT A POINT 579.1 FEET SOUTH OF THE NE CORNER OF THE NORTHWEST QUARTER (NW ¼) OF SECTION TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-FOUR (24) WEST OF THE 5TH P.M., AMES, STORY COUNTY, IOWA, THENCE SOUTH 125 FEET, THENCE WEST 300 FEET, THENCE NORTH 125 FEET, THENCE EAST 300 FEET TO THE POINT OF BEGINNING, LOCALLY KNOWN AS 3325 SOUTH DUFF AVENUE, AMES, IOWA.

3115 S. DUFF
PARCEL "P" A PART OF THE SOUTHWEST ¼ OF SECTION 34, TOWNSHIP 83 NORTH, RANGE 24 WEST OF THE 5TH P.M., AMES, STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON OCTOBER 6, 1998, AS INST. NO. 98-03885, SLIDE 2, PAGE 4, SAID PARCEL "P" INCLUDES REAL ESTATE DESCRIBED AS PARCEL "P" A PART OF THE SW ¼ OF SEC. 14-T83N-R24W OF THE 5TH P.M., STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, MARCH 28, 2000, AS INST. NO. 00-04330, SLIDE 62, PAGE 4, EXCEPT THE FOLLOWING DESCRIBED REAL ESTATE: PARCEL "M" A PART OF THE SW ¼ OF SEC. 14-T83N-R24W OF THE 5TH P.M., STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, APRIL 27, 2000, AS INST. NO. 00-04315, SLIDE 66, PAGE 1.

3409 & 3413 S. DUFF
PARCELS "A" AND "B" A PART OF THE NORTHEAST QUARTER (NE ¼) OF THE NORTHWEST QUARTER (NW ¼) OF SECTION TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-FOUR (24) WEST OF THE 5TH P.M., AMES, STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON JUNE 7, 1995, AS INST. NO. 95-04494, BOOK 13, PAGE 70.

3119 S. DUFF
COMMENCING AT THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER (SW ¼) OF SECTION FOURTEEN (14), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-FOUR (24) WEST OF THE 5TH P.M., IOWA, THENCE WEST ALONG THE SOUTH LINE OF SAID SECTION FOURTEEN (14) TWO HUNDRED EIGHTY (280) FEET TO A POINT, THENCE NORTH PARALLEL WITH THE CENTER LINE OF SAID SECTION FOURTEEN (14), ONE HUNDRED (100) FEET, THENCE EAST PARALLEL TO THE SOUTH LINE OF SAID SECTION FOURTEEN (14), TWO HUNDRED EIGHTY (280) FEET, THENCE SOUTH ALONG THE CENTER LINE OF SAID SECTION FOURTEEN (14) TO THE PLACE OF BEGINNING, SUBJECT TO HIGHWAY EASEMENTS OF RECORD.

OWNER:
 DEKSON & LUNN JENSEN
 4611 MORTENSEN ROAD
 AMES, IA 50010

APPLICANT:
 JENSEN GROUP
 4611 MORTENSEN ROAD
 AMES, IA 50010

PREPARED BY:
 BOLTON & MENK
 2730 FORD ST.
 AMES, IA 50010

AREA:
 RFE: 41.30 ACRES
 HOC: 8.62 ACRES
 TOTAL: 49.92 ACRES

ZONING:
 EXISTING: HIGHWAY ORIENTED COMMERCIAL
 PROPOSED: RESIDENTIAL HIGH DENSITY (RH) & HIGHWAY ORIENTED COMMERCIAL (HOC)

DENSITY/USE TABLE

AREA	ZONING	USE	AREA (ACRES)	UNITS	DENSITY
[Yellow Box]	RESIDENTIAL HIGH DENSITY (RH)	APARTMENT	39.99 - 42.99	500-700	11.63 - 17.50
[Purple Box]	HIGHWAY ORIENTED COMMERCIAL (HOC)	COMMERCIAL SPACE	3.00 - 6.00	N/A	N/A

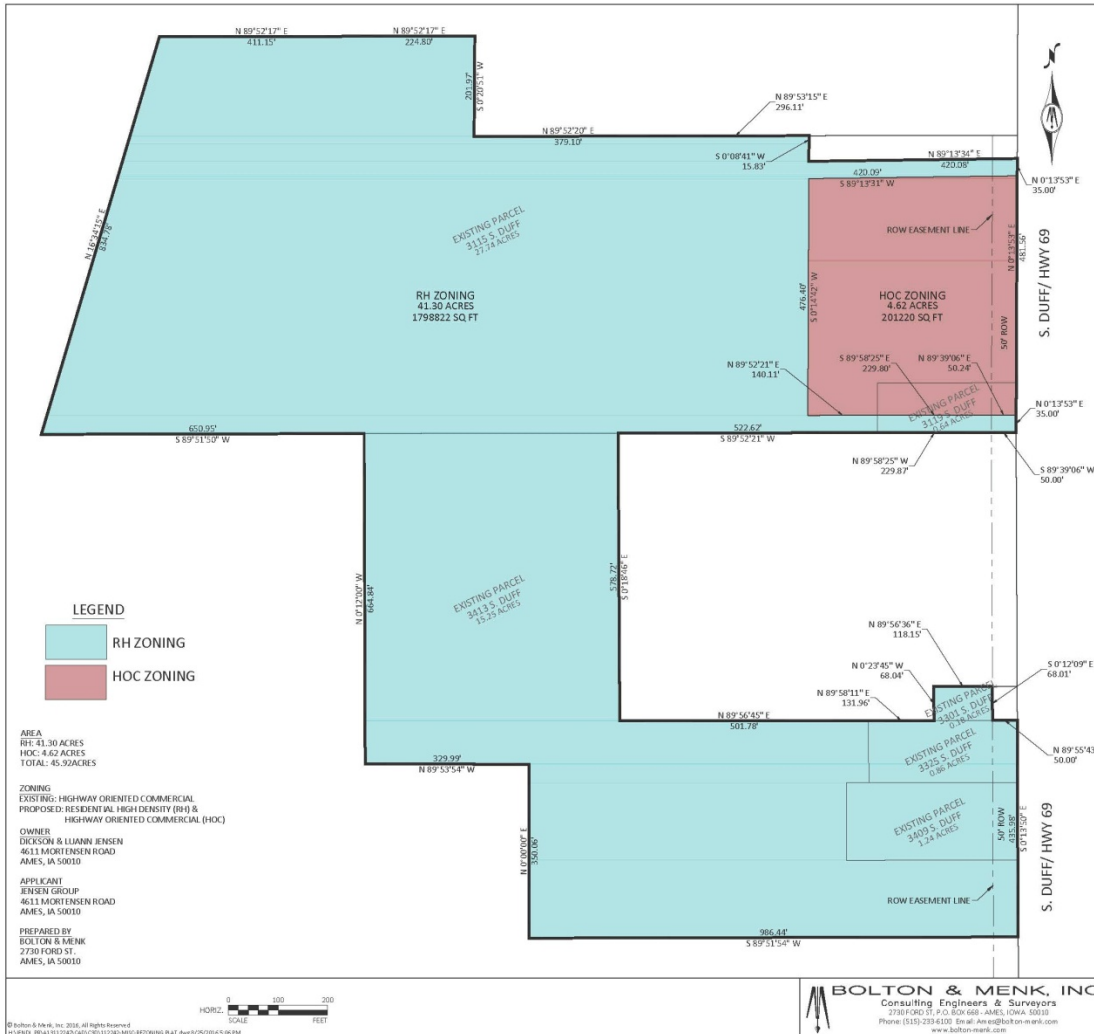


BOLTON & MENK, INC.
 Consulting Engineers & Surveyors
 2730 FORD ST. P.O. BOX 668 - AMES, IOWA 50010
 Phone: (515) 231-6200 Email: Ames@bolton-menk.com
 www.bolton-menk.com

REV	DATE	BY	DESCRIPTION

JENSEN GROUP
 BRICKTOWNE DEVELOPMENT
 MASTERPLAN

ATTACHMENT C: PROPOSED ZONING [NORTH TO THE LEFT]



REZONING PLAN FOR BRICKTOWNE DEVELOPMENT Jensen Group

LEGAL DESCRIPTIONS

3301 S. DUFF
A PART OF THE NORTHEAST QUARTER (NE ¼) OF THE NORTHWEST ¼ OF SECTION 14, TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), STORY COUNTY, IOWA, THENCE SOUTH 125 FEET, THENCE WEST 30 FEET TO THE POINT OF BEGINNING, LOCALLY KNOWN AS 3325 SOUTH DUFF.

3325 S. DUFF
BEGINNING AT A POINT 579.3 FEET SOUTH OF THE NE CORNER OF SECTION 14, TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), STORY COUNTY, IOWA, THENCE SOUTH 125 FEET, THENCE WEST 30 FEET TO THE POINT OF BEGINNING, LOCALLY KNOWN AS 3325 SOUTH DUFF.

3115 S. DUFF
PARCEL "A" A PART OF THE SOUTHWEST ¼ OF SECTION 14, TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY DESCRIBED AS PARCEL "A" A PART OF THE SW ¼ OF SEC. 14-T83N-R23W" SHOWING ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON OCTOBER 6, 1998, AS INST. NO. 98-13885, SLIDE 62, PAGE 4, EXCEPT THE FOLLOWING SW ¼ OF SEC. 14-T83N-R23W OF THE 5TH P.M., STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, APRIL 27, 1995, AS INST. NO. 95-04494, BOOK 13, PAGE 70.

3409 & 3413 S. DUFF
PARCELS "A" AND "B" A PART OF THE NORTHEAST QUARTER (NE ¼) OF SECTION 14, TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON JUNE 7, 1995, AS INST. NO. 95-04494, BOOK 13, PAGE 70.

3119 S. DUFF
COMMENCING AT THE SOUTHWEST CORNER OF THE SOUTHWEST ¼ OF SECTION 14, TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), TOWNSHIP EIGHTY-THREE (83) NORTH, RANGE TWENTY-THREE (23), STORY COUNTY, IOWA, AS SHOWN ON THE "PLAT OF SURVEY" FILED IN THE OFFICE OF THE RECORDER OF STORY COUNTY, IOWA, ON JUNE 7, 1995, AS INST. NO. 95-04494, BOOK 13, PAGE 70, PARALLEL WITH THE CENTER LINE OF SAID SECTION FOURTEEN (14) TO THE SOUTH LINE OF SAID SECTION FOURTEEN (14), TWO HUNDRED AND SIXTY-FOUR (264) FEET, THENCE SOUTH 89°50'00" WEST, THENCE WEST 118.15 FEET TO THE POINT OF BEGINNING, LOCALLY KNOWN AS 3119 SOUTH DUFF.

LEGEND

RH ZONING

HOC ZONING

AREA
RH: 41.30 ACRES
HOC: 4.62 ACRES
TOTAL: 45.92 ACRES

ZONING
EXISTING: HIGHWAY ORIENTED COMMERCIAL
PROPOSED: RESIDENTIAL HIGH DENSITY (RH) & HIGHWAY ORIENTED COMMERCIAL (HOC)

OWNER
DREXSON & LIANN JENSEN
4611 MORTENSEN ROAD
AMES, IA 50010

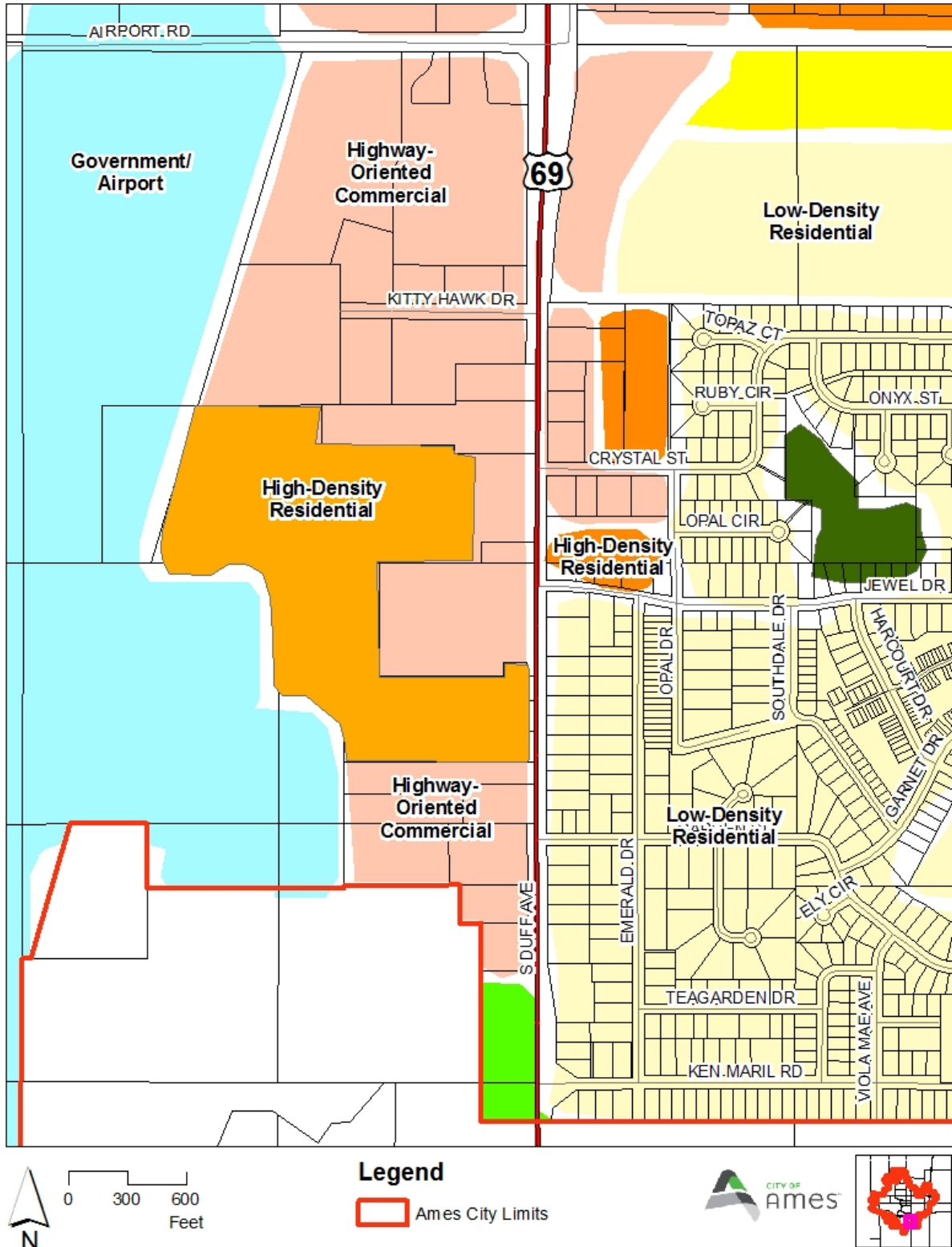
APPLICANT
JENSEN GROUP
4611 MORTENSEN ROAD
AMES, IA 50010

PREPARED BY
BOLTON & MENK
2710 FORD ST.
AMES, IA 50010

BOLTON & MENK, INC.
Consulting Engineers & Surveyors
2710 FORD ST., P.O. BOX 668, AMES, IOWA 50010
Phone: (515) 231-6100 Email: Admin@bolton-menk.com
www.boltonmenk.com

REV	REVISION	DATE	DESIGNED	CHECKED	DATE

ATTACHMENT D: LUPP MAP [EXCERPT]



ATTACHMENT E: APPLICABLE REGULATIONS

Per Section 29.1507(4): Master Plan Submittal Requirements:

- a. Name of the applicant and the name of the owner of record.
- b. Legal description of the property.
- c. North arrow, graphic scale, and date.
- d. Existing conditions within the proposed zoning boundary and within 200 feet of the proposed zoning boundary: Project boundary; all internal property boundaries; public rights-of-way on and adjacent to the site, utilities; easements; existing structures; topography (contours at two-foot intervals); areas of different vegetation types; designated wetlands; flood plain and floodway boundaries; areas designated by the Ames Land Use Policy Plan as Greenways and Environmentally Sensitive Areas
- e. Proposed zoning boundary lines.
- f. Outline and size in acres of areas to be protected from impacts of development
- g. Outline and size in acres of areas proposed of each separate land use and for each residential unit type
- h. Pattern of arterial streets and trails and off-site transportation connections
- i. For proposed residential development provide the number of unit type for each area, expressed in a range of the minimum to maximum number to be developed in each area
- j. For proposed residential development provide a summary table describing all uses of the total site area, including the number of units per net acre for each unit type and each zoning area.

ATTACHMENT F: RH SITE EVALUATION MATRIX

RH Site Evaluation Matrix	Project Consistency		
	High	Average	Low
Location/Surroundings			
Integrates into an existing neighborhood with appropriate interfaces and transitions High=part of a neighborhood, no significant physical barriers, includes transitions; Average=adjacent to neighborhood, some physical barriers, minor transitions; Low=separated from an residential existing area, physical barriers, no transitions available			X
Located near daily services and amenities (school, park ,variety of commercial) High=Walk 10 minutes to range of service; Average=10 to 20 minutes to range of service; Low= Walk in excess of 20 minutes to range of service. *Parks and Recreation has specific service objectives for park proximity to residential		X	
Creates new neighborhood, not an isolated project (If not part of neighborhood, Does it create a critical mass or identifiable place, support to provide more services?)		X	
Located near employment centers or ISU Campus (High=10 minute bike/walk or 5 minute drive; Average is 20 minute walk or 15 minute drive; Low= exceeds 15 minute drive or no walkability)	X		
Site			
Contains no substantial natural features on the site (woodlands, wetlands, waterways)			X
Located outside of the Floodway Fringe	X		
Separated adequately from adjacent noise, business operations, air quality (trains, highways, industrial uses, airport approach)			X
Ability to preserve or sustain natural features		X	
Housing Types and Design			
Needed housing or building type or variety of housing types	X		
Architectural interest and character		X	
Site design for landscape buffering			X
Includes affordable housing (Low and Moderate Income)			X

Transportation			
Adjacent to CyRide line to employment/campus High=majority of site is 1/8 miles walk from bus stop; Average= majority of site 1/4 mile walk from bus stop; Low= majority of site exceeds 1/4 miles walk from bus stop.		X	
CyRide service has adequate schedule and capacity High=seating capacity at peak times with schedule for full service Average=seating capacity at peak times with limited schedule Low=either no capacity for peak trips or schedule does not provide reliable service		X	
Pedestrian and Bike path or lanes with connectivity to neighborhood or commute			X
Roadway capacity and intersection operations (existing and planned at LOS C)			X
Site access and safety		X	
Public Utilities/Services			
Adequate storm, water, sewer capacity for intensification High=infrastructure in place with high capacity Average=infrastructure located nearby, developer obligation to extend and serve Low=system capacity is low, major extension needed or requires unplanned city participation in cost.		X	
Consistent with emergency response goals High=Fire average response time less than 3 minutes Average=Fire average response time within 3-5 minutes Low=Fire average response time exceeds 5 minutes, or projected substantial increase in service calls	X		
Investment/Catalyst			
Support prior City sponsored neighborhood/district investments or sub-area planning			X
Creates character/identity/sense of place			X
Encourages economic development or diversification of retail commercial (Mixed Use Development)		X	

DO NOT WRITE IN THE SPACE ABOVE THIS LINE; RESERVED FOR RECORDER

Prepared by: Judy K. Parks, Ames City Attorney, 515 Clark Ave., Ames, IA 50010; 515-239-5146

Return to: Ames City Clerk, Ames City Hall, P.O. Box 811, Ames, IA 50010

**MASTER PLAN/CONTRACT ZONING AGREEMENT BETWEEN DICKSON JENSEN
AND LUANN JENSEN AND THE CITY OF AMES
FOR THE BRICK TOWNE DEVELOPMENT AT 3115-3413 SOUTH DUFF AVENUE**

THIS AGREEMENT, (this “Agreement”) made and entered into this ____ day of _____, 2016, by and between the City of Ames, Iowa (hereinafter called “City”) and Dickson D. Jensen and Luann C. Jensen (hereinafter called “ the Developer”), their successors, heirs, and assigns.

WITNESSETH THAT:

WHEREAS, the Developer is seeking to improve and develop an area located at 3115, 3119, 3301, 3325, 3409 and 3413 South Duff Avenue and legally described as set out on Attachment A and depicted in Attachment B (collectively, the “Site”), which will be called the Brick Towne Development; and

WHEREAS, the Developer has applied to the City for rezoning of the Site from the present designations as HOC (Highway Oriented Commercial) and A (Agricultural) to HOC (Highway Oriented Commercial) and RH (Residential High Density), consistent with the Land Use Policy Plan; and

WHEREAS, the Developer has proposed a Master Plan to guide the general layout, design, and intensity of future development; and

WHEREAS, Developer and the City desire to enter into an agreement related to additional conditions for development of the Site which addresses storm water and off-site traffic impacts of the Site, and cost allocation for improvements related to those, in conjunction with granting the base zoning for the Site, as provided for under Iowa Code section 414.5.

NOW, THEREFORE, the parties hereto have agreed and do agree as follows:

I.
INTENT AND PURPOSE

- A. It is the intent of this Agreement to:
1. Recognize that Developer is owner of the Site which is being rezoned and that Developer expressly agrees to the imposition of additional conditions as authorized pursuant to Iowa Code section 414.5.
 2. Meet the Master Plan requirements pursuant to Section 29.1507(5) of the City of Ames Municipal Zoning Code.
 3. Provide for completion of storm water improvements for the land being rezoned as well as for additional land in the vicinity in a manner consistent with the Teagarden Drainage Study.
 4. Provide for off-site street and bicycle path improvements required for the development which is contemplated to occur on the Site, as well as provide for rehabilitation and widening of South Duff Avenue and signalization of the intersection of South Duff Avenue with Crystal Street.
 5. Ensure that street frontage improvements are completed along the Site frontage of South Duff Avenue and that those are connected across the cemetery frontage; and
 6. Allow for cost allocation of the storm water and off-site street traffic improvements between the parties.

II.
STORM WATER MANAGEMENT IMPROVEMENTS AND COST

The Developer is responsible for all storm water management for the Site.

It is also known that storm water flows from west of the Site into the Teagarden residential area east of South Duff Avenue. It would be desirable to the City to incorporate enhanced storm water management in this area.

In conjunction with the construction of its storm water improvements for the Site, the Developer agrees to build and provide additional storm water improvements in a capacity sufficient to provide adequate storm water management for the Teagarden area and in accordance with the Teagarden Drainage Report dated August, 2015. The Developer will bear the cost for all of these improvements. The Developer will design the improvements subject to the City's specifications and will install them only after review and approval has been given by the City of Ames Public Works Department.

The storm water detention facilities and other improvements shall be completed prior to occupancy of any structure on the Site, or by October of 2018, whichever occurs first.

III. OFF-SITE TRANSPORTATION IMPROVEMENTS AND COST

South Duff Avenue (also known as U.S. Highway 69) is in need of widening and signalization as off-site traffic improvements to meet the development needs of the Site. South Duff Avenue will be widened to three lanes in the vicinity of the Site.

Plans for the South Duff improvements are incomplete at this point, but all South Duff Avenue improvements are subject to approval by the Department of Transportation. Preliminary plans identify the widening principally along the west side of South Duff Avenue. However, in the event there is no reasonable alternative to widening to the east, and any of the improvements cause the need for relocation of the Ames Electric transmission lines or poles, the City and the Developer shall share equally in the cost of their relocation, with a cost not to exceed \$375,000 for the Developer.

The City, at its sole cost, will install a traffic signal at the intersection of Crystal Street and South Duff Avenue.

The City, at its sole cost, will install a shared use path south from its current terminus at the State Nursery property.

The City will complete off-site transportation improvements as described above. However, the Developer, at its sole cost, is responsible for dedication of required right of way and for street frontage improvements that are not related to South Duff Avenue street widening, including, but not limited to, the construction of a five-foot wide sidewalk. In addition, the Developer must install a five-foot wide sidewalk along the frontage of Story Memorial Gardens cemetery to connect the north and south frontages of the site.

Frontage improvements shall be completed commensurate with the phasing of residential development, or according to a timeframe directed by the City Council.

IV. MASTER PLAN AND DESIGN

The request for rezoning was accompanied by a Master Plan in accordance with Section 29.1507 of the Ames Municipal Code. The Master Plan is set out in Attachment C and is made a part of this contract. The development intensity of the Site is limited to a maximum of 700 dwelling units configured as one and two apartment dwellings.

The City and the Developer agree that the Master Plan set forth in Attachment C is the Master Plan for the proposed development of the Site and that any development shall be in compliance with said Master Plan as required by Section 29.1507(5) of the Ames Municipal Code. The Developer understands that site access from South Duff Avenue may be restricted by the City or

Department of Transportation and that shared access drives with the commercial and residential development will be required for development of the Site.

The Developer has stated an intent to make a desirable living environment with on-site amenities and features with development of the Site. The Developer agrees to incorporate recreational and lifestyle amenities commensurate with the phased development of the residential buildings of the Site. Additionally, the Developer shall incorporate architectural features into each building on the Site that helps to create identity as a residential community, enhance the building design, and provide architectural interest and relief elements to building massing. Additionally, the Developer shall incorporate a substantial amount of brick material with each building façade. Final approval of building design consistent with the design intent of this Agreement is subject to the approval by City Staff as part of the Site Development Plan approval process of Section 29.1502 of the Ames Municipal Code.

V.

NON-INCLUSION OF OTHER IMPROVEMENT OBLIGATIONS

The parties acknowledge and agree that this Agreement is being executed in contemplation of a conceptual plan for development, without further review or approval of subsequent specific plans for development of the Site. The parties acknowledge and agree that it is not possible to anticipate all the infrastructure requirements that the Developer may be required to complete to properly develop the Site. Therefore, the parties agree that all work done by and on behalf of the Developer with respect to, but not limited to, landscaping, sidewalks, building design, building construction and utilities, both on-site and off-site, shall be made in compliance with Iowa Code, SUDAS and all other federal, state and local laws and policies of general application, whether or not such requirements are specifically stated in this Agreement.

VI.

GENERAL PROVISIONS

A. Modification. The parties agree that this Agreement may be modified, amended or supplemented only by written agreement of the parties.

B. Incorporation of Recitals and Exhibits. The recitals, together with any and all exhibits attached hereto, are confirmed by the parties as true and incorporated herein by reference as if fully set forth verbatim. The recitals and exhibits are a substantive contractual part of this Agreement.

VII.

COVENANTS RUN WITH THE LAND

This Agreement shall run with the land and shall be binding upon the Developer, its successors, subsequent purchasers and assigns. Each party hereto agrees to cooperate with the other in executing a Memorandum of Agreement that may be recorded in place of this document.

IN WITNESS WHEREOF, the parties hereto have caused this instrument to be executed effective as of the date first above written.

<p>CITY OF AMES, IOWA</p> <p>By _____ Ann H. Campbell, Mayor</p> <p>Attest _____ Diane R. Voss, City Clerk</p> <p>STATE OF IOWA, COUNTY OF STORY, ss:</p> <p>On this _____ day of _____, 2016, before me, a Notary Public in and for the State of Iowa, personally appeared Ann H. Campbell and Diane R. Voss, to me personally known, who, being by me duly sworn, did say that they are the Mayor and City Clerk, respectively, of the City of Ames, Iowa; that the seal affixed to the foregoing instrument is the corporate seal of the corporation, and that the instrument was signed and sealed on behalf of the corporation by authority of its City Council, as contained in Resolution No. _____ adopted by the City Council on the _____ day of _____, 2016, and that Ann H. Campbell and Diane R Voss acknowledged the execution of the instrument to be their voluntary act and deed and the voluntary act and deed of the corporation, by it voluntarily executed.</p> <p>_____ Notary Public in and for the State of Iowa</p>	<p>_____ DICKSON D. JENSEN</p> <p>STATE OF IOWA, COUNTY OF _____, ss:</p> <p>This instrument was acknowledged before me on _____, 2016, by _____, Dickson D. Jensen.</p> <p>_____ Notary Public in and for the State of Iowa</p> <p>_____ LUANN C. JENSEN</p> <p>STATE OF IOWA, COUNTY OF _____, ss:</p> <p>This instrument was acknowledged before me on _____, 2016, by _____, Luann C. Jensen.</p> <p>_____ Notary Public in and for the State of Iowa</p>
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Attachment A: Legal Description of Site

COMMENCING AT THE NORTHEAST CORNER OF PARCEL 'M' IN SECTION 14, TOWNSHIP 83, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, THENCE S00°13'53"W 35.0 FEET ALONG THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14 TO THE POINT OF BEGINNING; THENCE S89°13'31"W 420.09 FEET; THENCE S00°14'42"W 476.40 FEET; THENCE N89°52'21"E 140.11 FEET; THENCE S89°58'25"E 229.80 FEET TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°39'06"E 50.24 FEET TO THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14; THENCE N00°13'53"E 481.56 FEET ALONG THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14 TO THE POINT OF BEGINNING.

AND

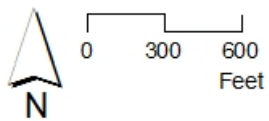
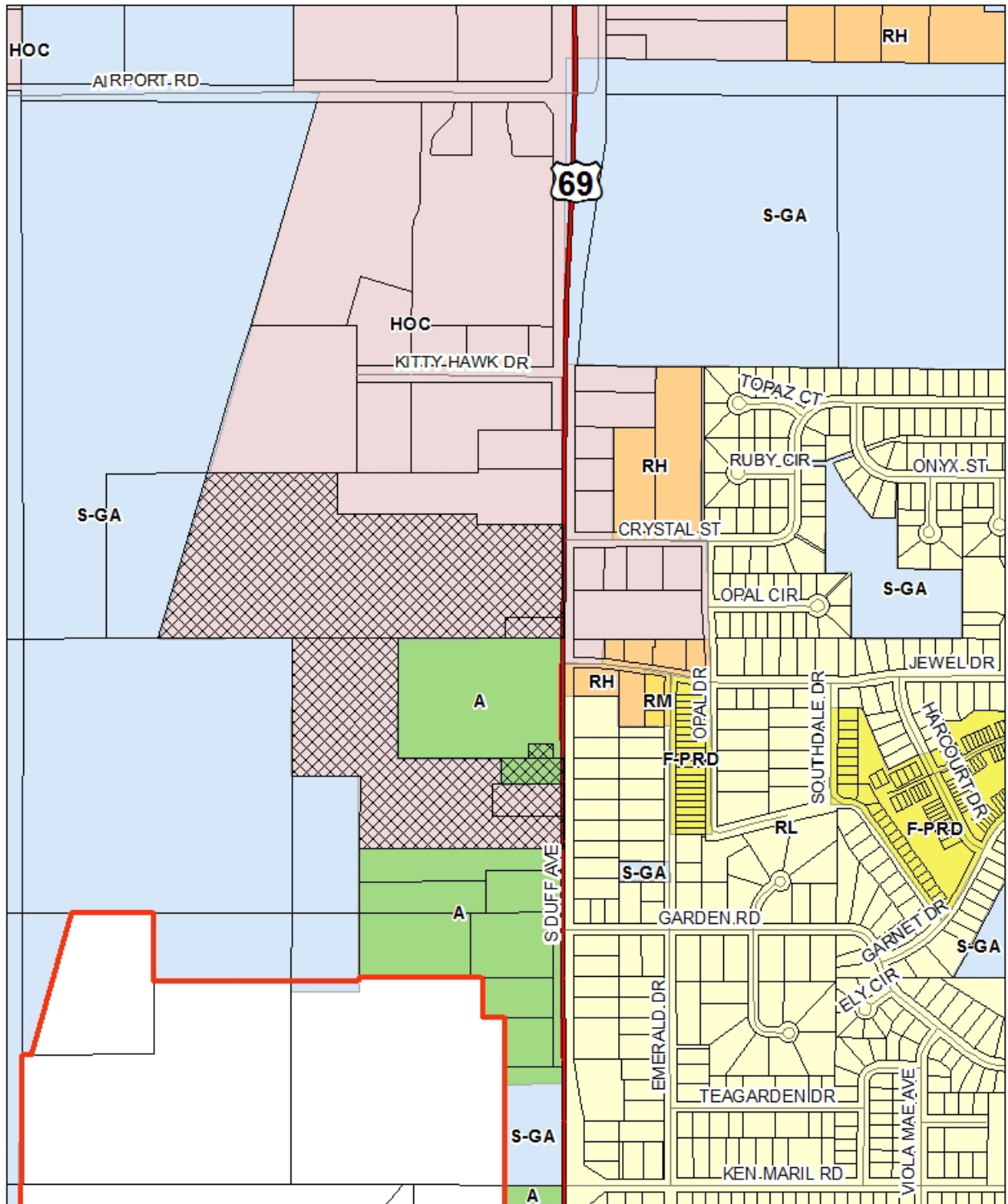
BEGINNING AT THE SOUTHEAST CORNER OF PARCEL 'A' IN SECTION 23, TOWNSHIP 83, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, THENCE S89°51'54"W 986.44 FEET ALONG THE SOUTH LINE OF PARCEL 'A' TO THE SOUTHWESTERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N00°00'00"E 350.06 FEET ALONG THE WEST LINE OF PARCEL 'A' TO THE SOUTHWESTERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N89°53'54"W 329.99 FEET ALONG THE SOUTH LINE OF PARCEL 'A' TO THE SOUTHWESTERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N00°12'00"W 664.84 FEET ALONG THE WEST LINE OF PARCEL 'A' TO THE NW CORNER OF PARCEL 'A' AND THE NW CORNER OF NE1/4-NW1/4 IN SAID SECTION 23; THENCE S89°51'50"W 650.95 FEET ALONG THE SOUTH LINE OF PARCEL 'J' AND THE SOUTH LINE OF THE SW1/4-SW1/4 TO THE SOUTHWEST CORNER OF PARCEL 'J' IN SAID SECTION 14, TOWNSHIP 83, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY; THENCE N16°34'15"E 834.78 FEET ALONG THE WEST LINE OF PARCEL 'J' TO THE NORTHWEST CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°52'17"E 411.15 FEET ALONG THE NORTH LINE OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°52'17"E 224.80 FEET ALONG THE NORTH LINE OF PARCEL 'J' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE S00°20'51"W 201.97 FEET ALONG THE EAST LINE OF PARCEL 'J' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°52'20"E 379.10 FEET ALONG THE NORTH LINE OF PARCEL 'J' TO THE WEST CORNER OF PARCEL 'L' IN SAID SECTION 14; THENCE N89°53'15"E 296.11 FEET ALONG THE NORTH LINE OF PARCEL 'L' TO THE NORTHWEST CORNER OF PARCEL 'L' IN SAID SECTION 14; THENCE S00°08'41"W 15.83 FEET ALONG THE EAST LINE OF PARCEL 'L' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE S00°17'23"W 35.60 FEET ALONG THE EAST LINE OF PARCEL 'J' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°13'34"E 420.08 FEET ALONG THE NORTH LINE OF PARCEL 'J' TO THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 IN SAID SECTION 14; THENCE S00°13'53" W 35.00 FEET ALONG THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14; THENCE S89°13'31"W 420.09 FEET; THENCE S00°14'42"W 476.40 FEET; THENCE N89°52'21"E 140.11 FEET; THENCE S89°58'25"E 229.80 FEET TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°39'06"E 50.24 FEET TO THE EAST LINE OF THE SW1/4 OF SAID SECTION 14; THENCE S00°13'53"W 35.00 FEET ALONG THE EAST LINE OF THE SW1/4 TO THE SOUTH 1/4 CORNER OF SAID SECTION 14; THENCE S89°39'06" W 50.00 FEET ALONG THE NORTH LINE OF THE NE1/4-NW1/4 OF SAID SECTION 23 TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°58'25" W 229.87 FEET ALONG THE NORTH LINE OF THE

NE1/4-NW1/4 OF SAID SECTION 23; THENCE S89°52'21"W 522.62 FEET ALONG THE NE1/4-NW1/4 TO THE NORTHEAST CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE S00°18'46"E 578.72 FEET ALONG THE EAST LINE OF PARCEL 'A' TO THE NORTHERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N89°56'45"E 501.78 FEET ALONG THE NORTH LINE OF PARCEL 'A' TO THE NORTHEAST CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N89°58'11"E 131.96 FEET; THENCE N00°23'45"W 68.04 FEET; THENCE N89°56'36"E 118.15 FEET TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE S00°12'09"E 68.01 FEET ALONG THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°55'43"E 50.00 FEET TO THE EAST LINE OF THE NW1/4 OF SAID SECTION 23; THENCE S00°13'50"E 435.98 FEET ALONG THE EAST LINE OF THE NW1/4 OF SAID SECTION 23 TO THE POINT OF BEGINNING.

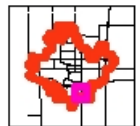
*THE EAST LINE OF PARCEL 'J', 'M' AND THE EAST LINE OF THE SW1/4 OF SECTION 14-83-24 IS ALSO THE CENTER LINE OF US HIGHWAY #69.

*THE EAST LINE OF PARCEL 'A' AND THE EAST LINE OF THE NW1/4 OF SECTION 23-83-24 IS ALSO THE CENTERLINE OF US HIGHWAY #69.

Attachment B: Location of Site



- Legend**
-  Subject Site
 -  Ames City Limits



DO NOT WRITE IN THE SPACE ABOVE THIS LINE, RESERVED FOR RECORDER
Prepared by: Judy K. Parks, Ames City Attorney, 515 Clark Avenue, Ames, IA 50010 Phone: 515-239-5146
Return to: Ames City Clerk, P.O. Box 811, Ames, IA 50010 Phone: 515-239-5105

ORDINANCE NO. _____

AN ORDINANCE AMENDING THE OFFICIAL ZONING MAP OF THE CITY OF AMES, IOWA, AS PROVIDED FOR IN SECTION 29.301 OF THE *MUNICIPAL CODE* OF THE CITY OF AMES, IOWA, BY CHANGING THE BOUNDARIES OF THE DISTRICTS ESTABLISHED AND SHOWN ON SAID MAP AS PROVIDED IN SECTION 29.1507 OF THE *MUNICIPAL CODE* OF THE CITY OF AMES, IOWA; REPEALING ALL ORDINANCES AND PARTS OF ORDINANCES IN CONFLICT HEREWITH AND ESTABLISHING AN EFFECTIVE DATE

BE IT HEREBY ORDAINED by the City Council of the City of Ames, Iowa;

Section 1: The Official Zoning Map of the City of Ames, Iowa, as provided for in Section 29.301 of the *Municipal Code* of the City of Ames, Iowa, is amended by changing the boundaries of the districts established and shown on said Map in the manner authorized by Section 29.1507 of the *Municipal Code* of the City of Ames, Iowa, as follows: That the real estate, generally located at 3115, 3119, 3301, 3325, 3409, and 3413 South Duff Avenue, is rezoned, with Master Plan, from Highway-Oriented Commercial (HOC) and Agricultural (A) to Residential High Density (RH) and Highway-Oriented Commercial (HOC).

Real Estate Description:

HOC PARCEL

COMMENCING AT THE NORTHEAST CORNER OF PARCEL 'M' IN SECTION 14, TOWNSHIP 83, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, THENCE S00°13'53"W 35.0 FEET ALONG THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14 TO THE POINT OF BEGINNING; THENCE S89°13'31"W 420.09 FEET; THENCE S00°14'42"W 476.40 FEET; THENCE N89°52'21"E 140.11 FEET; THENCE S89°58'25"E 229.80 FEET TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°39'06"E 50.24 FEET TO THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14; THENCE N00°13'53"E 481.56 FEET ALONG THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14 TO THE POINT OF BEGINNING.

RH PARCEL

BEGINNING AT THE SOUTHEAST CORNER OF PARCEL 'A' IN SECTION 23, TOWNSHIP 83, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY, THENCE S89°51'54"W 986.44 FEET ALONG THE SOUTH LINE OF PARCEL 'A' TO THE SOUTHWESTERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N00°00'00"E 350.06 FEET ALONG THE WEST LINE OF PARCEL 'A' TO THE SOUTHWESTERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N89°53'54"W 329.99 FEET ALONG THE SOUTH LINE OF PARCEL 'A' TO THE SOUTHWESTERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N00°12'00"W 664.84 FEET ALONG THE WEST LINE OF PARCEL 'A' TO THE NW CORNER OF PARCEL 'A' AND THE NW CORNER OF NE1/4-NW1/4 IN SAID SECTION 23; THENCE S89°51'50"W 650.95 FEET ALONG THE SOUTH LINE OF PARCEL 'J' AND THE SOUTH LINE OF THE SW1/4-SW1/4 TO THE SOUTHWEST CORNER OF PARCEL 'J' IN SAID SECTION 14, TOWNSHIP 83, RANGE 24 WEST OF THE 5TH P.M. IN THE CITY OF AMES, STORY COUNTY; THENCE N16°34'15"E 834.78 FEET ALONG THE WEST LINE OF PARCEL 'J' TO THE NORTHWEST CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°52'17"E 411.15 FEET ALONG THE NORTH LINE OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°52'17"E 224.80 FEET ALONG THE NORTH LINE OF PARCEL 'J' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE S00°20'51"W 201.97 FEET ALONG THE EAST LINE OF PARCEL 'J' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°52'20"E 379.10 FEET ALONG THE NORTH LINE OF PARCEL 'J' TO THE WEST CORNER OF PARCEL 'L' IN SAID SECTION 14; THENCE N89°53'15"E 296.11 FEET ALONG THE NORTH LINE OF PARCEL 'L' TO THE NORTHWEST CORNER OF PARCEL 'L' IN SAID SECTION 14; THENCE S00°08'41"W 15.83 FEET ALONG THE EAST LINE OF PARCEL 'L' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE S00°17'23"W 35.60 FEET ALONG THE EAST LINE OF PARCEL 'J' TO THE NORTHERLY CORNER OF PARCEL 'J' IN SAID SECTION 14; THENCE N89°13'34"E 420.08 FEET ALONG THE NORTH LINE OF PARCEL 'J' TO THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 IN SAID SECTION 14; THENCE S00°13'53" W 35.00 FEET ALONG THE EAST LINE OF PARCEL 'J' AND THE EAST LINE OF THE SW1/4 OF SAID SECTION 14; THENCE S89°13'31"W 420.09 FEET; THENCE S00°14'42"W 476.40 FEET; THENCE N89°52'21"E 140.11 FEET; THENCE S89°58'25"E 229.80 FEET TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°39'06"E 50.24 FEET TO THE EAST LINE OF THE SW1/4 OF SAID SECTION 14; THENCE S00°13'53"W 35.00 FEET ALONG THE EAST LINE OF THE SW1/4 TO THE SOUTH 1/4 CORNER OF SAID SECTION 14; THENCE S89°39'06" W 50.00 FEET ALONG THE NORTH LINE OF THE NE1/4-NW1/4 OF SAID SECTION 23 TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°58'25" W 229.87 FEET ALONG THE NORTH LINE OF THE NE1/4-NW1/4 OF SAID SECTION 23; THENCE S89°52'21"W 522.62 FEET ALONG THE NE1/4-NW1/4 TO THE NORTHEAST CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE S00°18'46"E 578.72 FEET ALONG THE EAST LINE OF PARCEL 'A' TO THE NORTHERLY CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N89°56'45"E 501.78 FEET ALONG THE NORTH LINE OF PARCEL 'A' TO THE NORTHEAST CORNER OF PARCEL 'A' IN SAID SECTION 23; THENCE N89°58'11"E 131.96 FEET; THENCE N00°23'45"W 68.04 FEET; THENCE N89°56'36"E 118.15 FEET TO THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE S00°12'09"E 68.01 FEET ALONG THE WEST RIGHT-OF-WAY LINE OF US HIGHWAY #69; THENCE N89°55'43"E 50.00 FEET TO THE EAST LINE OF THE NW1/4 OF SAID SECTION 23; THENCE S00°13'50"E 435.98 FEET ALONG THE EAST LINE OF THE NW1/4 OF SAID SECTION 23 TO THE POINT OF BEGINNING.

*THE EAST LINE OF PARCEL 'J', 'M' AND THE EAST LINE OF THE SW1/4 OF SECTION 14-83-24 IS ALSO THE CENTER LINE OF US HIGHWAY #69.

*THE EAST LINE OF PARCEL 'A' AND THE EAST LINE OF THE NW1/4 OF SECTION 23-83-24 IS ALSO THE CENTERLINE OF US HIGHWAY #69.

Section 2: All other ordinances and parts of ordinances in conflict herewith are hereby repealed to the extent of such conflict.

Section 3: This ordinance is in full force and effect from and after its adoption and publication as provided by law.

ADOPTED THIS _____ day of _____, _____.

Diane R. Voss, City Clerk

Ann H. Campbell, Mayor

COUNCIL ACTION FORM

SUBJECT: MAJOR SITE DEVELOPMENT PLAN AMENDMENT FOR 3505 AND 3515 LINCOLN WAY (WALNUT RIDGE)

BACKGROUND:

In September 2015, the City Council approved a Major Site Development Plan for two parcels at 3505 and 3515 Lincoln Way to allow for the development of a commercial and residential mixed-use development. The subject site totals 2.23 acres and is located just west of the Franklin Avenue intersection. (See Attachment A Location and Existing Zoning Map) The property owners, Turn Key Investments, LLC and Chuck Winkleblack, are requesting approval of an amendment to the plan **to revise the overall Landscape Plan for the property to request removal of the west property line parking lot screening due to the installed retaining wall, revision to the north property line parking lot screening, and revision to the east parking lot screening due to the constructed slopes of the detention areas.** (See Attachment B).

The overall project includes two separate three-story buildings containing 10,912 square feet of commercial space and 18 dwelling units. The project has a single point of access from Lincoln Way and a second access point from a rear alley. The design incorporated commercial space at the front of the site perpendicular to Lincoln Way with outdoor seating and plaza areas. Parking is located in between buildings and to the rear of the site.

With the approval of a Major Site Development Plan, the City Council has the authority to approve a Landscape Plan which does not meet the strict landscape requirements of the zoning code, if the Council determines that the plan meets the purposes described in Section 29.403(4)(a), *“... to protect and promote the public health, safety and general welfare by requiring the landscaping of surface parking lots in such a manner as will serve to reduce the effects of wind and air turbulence, heat and noise, and the glare of automobile lights; to preserve ground water strata; to act as a natural drainage system and ameliorate stormwater drainage problems; to reduce the level of carbon dioxide and return oxygen to the atmosphere; to prevent soil erosion; to conserve and stabilize property values and to otherwise facilitate the creation of a convenient, attractive and harmonious community; to relieve the blight of the appearance of surface parking lots; and to generally preserve a healthful and pleasant environment.”* **The applicant is requesting approval of a revision to the Landscape Plan to allow for the intent of the Code to be achieved for screening by using an alternative planting plan compared to strict conformance to the L2 and L3 standards**

Under base zoning standards the project must meet a minimum L3 parking lot screen (6-foot high fence with interspersed shrubs and trees) along the north and east property line abutting the residential properties and L2 parking lot screening (3-foot high shrubs planted 4-foot on center and trees) along all other lot lines abutting commercially zoned

properties. The L3 screen is a high screen consisting of either a fence or high shrubs spaced at a maximum distance of 6 feet on center to form a screen 6 feet high. In addition, one landscape tree is required per 50 lineal feet of landscaped area or as appropriate to provide a tree canopy over the landscaped area. The L2 screen is a low screen consisting of low shrubs spaced at a maximum distance of 4 feet on center to form a continuous screen 3 feet high. In addition, one landscape tree is required per 50 lineal feet of landscaped area or as appropriate to provide a tree canopy over the landscaped area. Use of a wood fence is an allowed substitution to the planting requirement that reduces the overall number of shrubs to be planted.

The developer desires to change the L2 screen along the west property line due to the style of the retaining wall that was constructed that limited the depth of soil for plantings and the limited physical space along the property line for plants to grow. The parking lot is located on the low side of the 5-6 foot wall so all visibility of parked vehicles from the neighboring property is obstructed in this area due to the wall. **The developer proposes only ground cover in this area and to plant only one of two required trees due to the retaining wall location.**

The proposed plan includes a revision to the other landscape areas of the site along the north and east property lines to screen the parking lot with the existing 6-foot wood fence and using a mix of grasses and shrubs to meet the intent of the parking lot screen requirements and the use of understory trees. Grasses typically are not counted to meet the minimum screen requirement of an L2 or L3 screen. The east lot line is the most difficult area to address landscaping as the slopes of the detention area limits planting space for such screen requirements. **Lower height crabapple trees have been proposed along the north and east property lines to stay below the overhead power lines, but still meet tree requirement of the ordinance for trees within the screen areas.**

As proposed the amendment to the Landscape Plan does not materially impact the aesthetic of the project or function of the site and still meets the screen intent of the Zoning Ordinance.

At its meeting of September 7, 2016, the Planning and Zoning Commission reviewed the proposed amendment to the approved Major Site Development Plan amendment for this mixed-use project. There were no comments from the Commission, applicant or the public. By a vote of 6-0, the Commission recommended that the City Council approve the amendment to the Major Site Development Plan for Walnut Ridge in accordance with the plan submitted in Attachment B.

ALTERNATIVES:

1. The City Council can approve the Major Site Development Plan Amendment for 3505 and 3515 Lincoln Way to allow for the proposed revision to the Landscape Plan with the following condition:
 - A. The developer is responsible for the construction and maintenance of the 6-foot fence along all residential property lines.

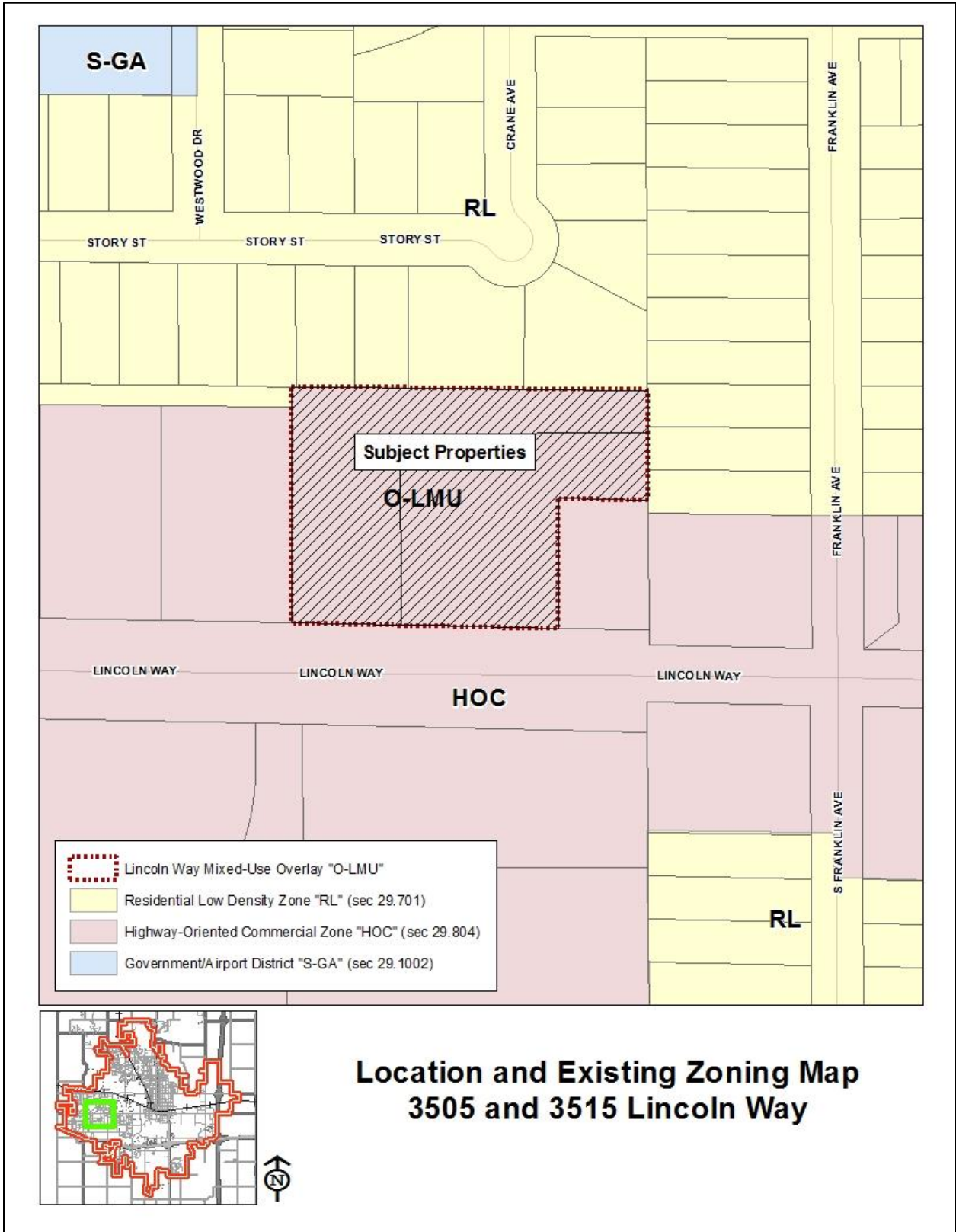
2. The City Council can deny the request for the Major Site Development Plan Amendment for 3505 and 3515 Lincoln Way.
3. The City Council can defer action on this request and refer it back to City staff and/or the applicant for additional information.

MANAGER'S RECOMMENDED ACTION:

When City Council approved the Major Site Development Plan for development of the properties addressed as 3505 and 3515 Lincoln Way, it determined that the Plan meets the minimum criteria and standards for approval listed in Ames *Municipal Code* Section 29.1502(4)(d). **The requested Major Site Development Plan amendment meets the applicable design standards and criteria of the Zoning Ordinance and is still in line with the original Major Site Development Plan and criteria.** The original approval also included a condition that the developer was responsible for the construction of or maintenance of any existing fence along the residential property line to meet the zoning requirements for landscape buffering.

Therefore, it is the City Manager's recommendation that the City Council adopt Alternative #1 as stated above.

Attachment A Location and Existing Zoning Map



Attachment C
Design Standards for Major Site Development Plans
(from Ames *Municipal Code* Section 29.1502(4)(d))

When acting upon an application for a Major Site Development Plan approval, the Planning and Zoning Commission and the City Council shall rely upon generally accepted site planning criteria and design standards. These criteria and standards are necessary to fulfill the intent of the Zoning Ordinance, the Land Use Policy Plan, and are the minimum necessary to safeguard the public health, safety, aesthetics, and general welfare.

1. The design of the proposed development shall make adequate provisions for surface and subsurface drainage to limit the rate of increased runoff of surface water to adjacent and downstream property.
2. The design of the proposed development shall make adequate provision for connection to water, sanitary sewer, electrical, and other utility lines within the capacity limits of those utility lines.
3. The design of the proposed development shall make adequate provision for fire protection through building placement, acceptable location of flammable materials, and other measures to ensure fire safety.
4. The design of the proposed development shall not increase the danger of erosion, flooding, landslide, or other endangerment to adjoining and surrounding property.
5. Natural topographic and landscape features of the site shall be incorporated into the development design.
6. The design of the interior vehicle and pedestrian circulation shall provide for convenient flow of vehicles and movement of pedestrians and shall prevent hazards to adjacent streets or property.
7. The design of outdoor parking areas, storage yards, trash and dumpster areas, and other exterior features shall be adequately landscaped or screened to minimize potential nuisance and impairment to the use of adjoining property.
8. The proposed development shall limit entrances and exits upon adjacent streets in order to prevent congestion on adjacent and surrounding streets and in order to provide for safe and orderly vehicle movement.
9. Exterior lighting shall relate to the scale and location of the development in order to maintain adequate security, while preventing a nuisance or hardship to adjacent property or streets.

10. The proposed development shall ensure that dust and other forms of air pollution, noise disturbances, odor, glare, and other nuisances will be limited to acceptable levels as prescribed in other applicable State and City regulations.
11. Site coverage, building scale, setbacks, and open spaces shall be in proportion with the development property and with existing and planned development and structures, in adjacent and surrounding property.

COUNCIL ACTION FORM

**SUBJECT: WATER POLLUTION CONTROL FACILITY TRICKLING FILTER
PUMPING STATION PIPE RECOATING PROJECT**

BACKGROUND:

The Water Pollution Control Facility’s (WPCF) Trickling Filter Pumping Station (TFPS) was constructed in 1989. The existing TFPS piping and pipe coatings are original to the initial construction. Piping in the TFPS is exposed to a harsh wastewater environment and the pipe coatings have failed, causing the piping to show signs of surface corrosion. Sand-blasting and repainting of the piping is necessary to protect the piping from further corrosion.

On August 23, 2016, City Council issued a notice to bidders for this project. Bids were opened on September 20, 2016, and the bid tabulation is shown below.

Bidder	Total Project Bid Price
Mongan Painting Co., Inc.	\$ 45,157
<i>Engineer’s Estimate</i>	\$ 51,000
TMI Coatings, Inc.	\$ 58,500
Pospisil Painting Inc.	\$ 60,405
L & P Painting	\$ 60,827

Mongan Painting Company did not submit required documents with their bid, nor did they conduct a required pre-bid tour of the project. Therefore, the bid is considered nonresponsive. TMI Coatings, Inc. is the lowest responsive, responsible bidder. TMI complied with all requirements of the notice to bidders.

The FY 2016/17 CIP includes \$59,000 for sand-blasting and recoating of the TFPS piping as a part of the Water Pollution Control Plant Facility Improvements Project. The lowest responsive bid is within the budget, but with a contingency of less than 1%. Savings from the WPCF Raw Water Pump Station Repainting Project are available for that purpose; and staff recommends that those funds be transferred to this project to provide a modest contingency as shown below:

Funding Sources:

FY 15/16 CIP – TFPS repainting	\$ 59,000
Savings from WPC Raw Water Pump Station Repainting Project	\$ 2,890
Total Funds Available	\$ 61,890

ALTERNATIVES:

1. a.) Award a contract for recoating of the trickling filter pump station at the Water Pollution Control Facility to TMI Coatings, Inc. of St. Paul, MN.

b.) Authorize the transfer of \$2,890 in savings from the WPC Raw Water Pump Station Repainting Project to this project budget.
2. Do not award a contract at this time.

MANAGER'S RECOMMENDED ACTION:

The original coating on the TFPS piping has failed and the piping has begun to show signs of corrosion. Sand-blasting and recoating of the piping is necessary to protect the piping from further corrosion and to ensure the long-term integrity of the Water Pollution Control Facility equipment.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative No. 1 as stated above.

COUNCIL ACTION FORM

SUBJECT: 2007/08 SHARED USE PATH SYSTEM EXPANSION (OAKWOOD ROAD)

BACKGROUND:

This program provides for construction of shared use paths on right-of-way adjacent to City streets and through greenbelts. **This specific project is for construction of a shared use path on the south side of Oakwood Road from State Avenue east to Christofferson Park, as well as on the west side of Cedar Lane from Suncrest Drive north to Oakwood Road.** A map of the proposed locations is shown in Attachment A.

As the adjacent property owner, Iowa State University has agreed to fund the portion of the shared use path from State Avenue east to the Ringgenberg Subdivision which is currently estimated to be \$120,000. The agreement also provides the City with access for construction and maintenance of the path, and requires that the ISU portion be constructed this year. Relocation of the ISU farm fence along Oakwood Road is included in the plans.

Staff met with area residents for a project information meeting at Oakwood Church to receive input and comments. Staff also met with individual project owners on several occasions to discuss impacts to their properties that required adjustment to the project alignment to address those concerns.

On September 21, 2016 bids on the project were received as follows:

<i>Bidder</i>	<i>Amount</i>
Engineer's Estimate	\$ 226,791.00
Con-Struct	\$ 203,988.90
TK Concrete	\$ 228,903.00
Synergy Contracting	\$ 246,616.02
Kingston Services	\$ 249,221.40

The following table summarizes the 2007/08 Shared Use Path System Expansion program funding sources, funding distribution and expense breakdown.

Program Funding Summary

2007/08 Shared Use Path System Expansion Program		
Local Option Sales Tax (LOST)	\$	61,998
Developer Contributions (Ringgenberg)	\$	38,150
Developer Contributions (Suncrest)	\$	23,261
2016/17 Storm Sewer Improvements	\$	12,650
Accessibility Enhancement Funds	\$	26,300
Iowa State University (Estimated)	\$	120,000
Total Funding		\$ 282,359

Program Expense Summary

Engineering & Contract Administration (estimated)	\$	30,600
Construction Costs (estimated)	\$	203,989
Total Expenses		\$ 234,589

ALTERNATIVES:

- 1a. Accept the report of bids for the 2007/2008 Shared Use Path System Expansion (Oakwood Road).
 - b. Approve the final plans and specifications.
 - c. Award the 2007/2008 Shared Use Path System Expansion (Oakwood Road) to Construct, Inc. of Ames, Iowa in the amount of \$203,988.90.
2. Do not approve this project.

MANAGER'S RECOMMENDED ACTION:

This project will provide an important link in the City's shared use path/trail system. The entire length of Oakwood Road will be served by a path that connects neighborhoods such as Ringgenberg and Suncrest to the path network on University Avenue/Airport Road and the ISU Research Park.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative No. 1 as described above.

Attachment A



Geographic Information System (GIS) Product Disclaimer: City of Ames GIS map data does not replace or modify land surveys, deeds, and/or other legal instruments defining land ownership & land use nor does it replace field surveys of utilities or other features contained in the data. All features represented in this product should be field verified. This Product is provided "as is" without warranty or any representation of accuracy, timeliness or completeness. The burden for determining accuracy, completeness, timeliness, merchantability and fitness for or the appropriateness for use rests solely on the User.



Oakwood Road Shared Use Path



Scale: 1 in = 583 ft
Date: 8/16/2016

COUNCIL ACTION FORM

SUBJECT: AMENDMENT TO 2014-18 FIVE-YEAR C.D.B.G. CONSOLIDATED PLAN TO CREATE A NEIGHBORHOOD REVITALIZATION STRATEGY AREA (NRSA) AND TO THE 2016-17 ANNUAL ACTION PLAN.

BACKGROUND:

City Council first discussed redevelopment of 321 State Avenue (Old Middle School Site) as part of the draft 2016-17 Annual Action Plan on March 1, 2016. Staff described that in order to develop the site for mixed income households and not exclusively as low and moderate income housing, the City must seek a Neighborhood Revitalization Strategy Area (NRSA) designation for the area from HUD. Under NRSA regulations, entitlement cities are given greater flexibility to develop comprehensive approaches to address economic development and housing needs in a designated neighborhood within their community.

The NRSA guidelines outline that following Neighborhood and Demographic Criteria must be met to be eligible for the designation:

1. The areas covered must be **contiguous**; with no checkerboard areas across the community.
2. The selected area **must be primarily residential**.
3. The area must **contain a high percentage of LMI households**.
4. The percentage of LMI residents within the neighborhood must satisfy **one of the three** following criteria:
 - a. 70 percent of the total population in the selected area (if the grantee's upper quartile* is greater than 70 percent LMI);
 - b. The upper quartile* percentage (if the grantee's upper quartile is greater than 51 percent, but less than 70 percent LMI in the total population); or
 - c. 51 percent of the total population (if the grantee's upper quartile* percentage is less than percent).

*Upper quartile determinations are based on census block groups within the census tract and not just the census track itself.

Staff research revealed that the site falls within Block Group 2 in 13.01 Census Tract. With assistance from HUD staff, it has been determined that this area meets criteria 4b above. The actual LMI percentage and the quartile percent happened to both be 69.25 percent (see attached map). Therefore, no other census tracts or block groups would need to be added for the Block Group 2 of Census Tract 13.01 to qualify under the NRSA guidelines

With support from the Old Ames Middle School/College Creek Neighborhood Association and other interested citizens, The City Council authorized in March for staff to proceed with amending the 2014-2018 Five-Year Consolidated Plan to create a Neighborhood Revitalization Strategy Area (NRSA) specifically for redevelopment of the 321 State Avenue (former Old Ames Middle School). **The proposed NRSA is included as Attachment C.**

A major component for an NRSA designation is outreach and it requires a “Community Consultation” with the following members of the community:

- Residents of the area
- Owners/operators of businesses in the area
- Local financial institutions in the area
- Non-profit organizations in the area
- Community groups in the area

For the Community Consultation process, staff hosted two separate open houses to seek input from the business owner/operators, financial institutions, and/or non-profit organizations operating in the area, and with the residents and property owners in the area. There were 22 business owner/operators, financial institutions, and/or non-profit organizations that were invited, of which four (4) attended. There were 256 area residents and property owners/managers invited, of which three (2 property owners and one property owner/manager) attended.

A second component for an NRSA designation, requires preparing an historical overview of the area, and an assessment of the demographic conditions of the area and an examination of the strengths and weakness and the opportunities for housing development and other housing and neighborhood enhancements and improvements. In summary the research revealed the following:

- The area is comprised of a variety of land used districts that include: Residential, Commercial, Special Purpose and Government zones. Approximately, 73% of the area is zoned residential.
- The area contains approximately 1,115 rental units and approximately 238 detached owner-occupied single-family dwellings.
- The racial/ethnic make-up of the area is 94% White; the low and moderate percentage of the area is 69.25 %; the average house size by owner is 2.43 and for renter it is 2.07.

The research revealed the following **Strength/Opportunities**:

- Availability of vacant land owned by the City of Ames that can be re-developed into low and moderate cost housing for either owner and/or renter households;
- Strong commercial and retail businesses;
- Access to citywide Cy-Ride Bus Service;
- A number of medical facilities, financial institutions, retail shops, & restaurants;
- An active Neighborhood Association;
- Updated infrastructure improvements have occurred within the last 3-5 years.

The research also revealed the following **Weaknesses/Areas for Improvement**

- Slightly higher unemployment rate compared to the city overall
- Higher poverty rate compared to the city overall
- Higher percent of rental housing units compared to the city overall
- Higher percent of families earning less than \$30,000 compared to the city overall
- Neighborhood parks are underutilized, Franklin Park is not owned by the City

A third component for an NRSA designation, requires creating Performance Measures Funding and benchmarks the NRSA area. In that there are three (3) years remaining in the 2014-18

Consolidated Plan staff has identified the following Performance Measures and programs:

1. Housing: Homebuyer Assistance, Owner-occupied Housing Rehabilitation and Renter Affordability Assistance (Deposit, First Month's Rent, Transportation)
2. Public Infrastructure Improvements: streets, water, sewer, sidewalks, etc. to the production of housing units.
3. Acquisition of Land for Public Facilities Improvements: acquire land to maintain or relocate a park within the NRSA.

A fourth component for a NRSA designation, requires identifying how the funding process will occur.

- The City of Ames will not need to commit specific future funds for use in the NRSA at the time that the NRSA is submitted.
- After approval of the NRSA, the City's subsequent Annual Action Plans must describe how the City's CDBG and other resources will be used to achieve the NRSA goals.
- Each year after the initial NRSA submission, the City must identify in its Annual Action Plan measurable outcomes it expects to achieve during the year for the NRSA.

Attachment A includes a list of the proposed amendments to the 2016-17 CDBG Annual Action Plan. The program activities reflect activities related to the NRSA as well as citywide activities that were already part of the Annual Action Plan.

Reflected below is the summary of the revenues and expenditures of the program activities comparing the currently adopted budget and expected revenues to the proposed amendments with the NRSA. All of the prior programs will remain in the Amended Action Plan with the exception of the Homebuyer Assistance Program. This program will have no budgeted funding for the remainder of this year due priorities to complete other programs. The Homebuyer Program will likely be a key component of the next Annual Action Plan to help with development of the 321 State Avenue site.

Current Program Revenue Sources	Budget
2016-17 CDBG Program Allocation	\$490,986
2015-16 Program Rollover Balance	\$337,000
2016-17 Anticipated Program Income	\$160,506
Total Revenue Sources	\$988,492

Amended Program Revenue Sources	Budget
2016-17 CDBG Program Allocation	\$490,986
2015-16 Program Rollover Balance	\$386,067
2016-17 Anticipated Program Income	\$111,439
Total Revenue Sources	\$988,492

2016-17 Current Program Activities	Program Budget
Homebuyer Assistance Program	\$ 50,000
Single-Family Housing Improvements Program	\$132,506
Acquisition/Reuse Program for Affordable Housing	\$155,000
Public Infrastructure Improvements Program for State Ave.	\$392,789
Public Facilities Improvement Program for Non-Profits	\$100,000
Renter Affordability (Deposits, Rent & Transportation, Childcare Assistance) Programs	\$40,000
2016-17 Program Administration	\$ 118,197
Total	\$988,492

Amended 2016-17 Program Activities	Program Budget
Homebuyer Assistance Program	\$ 0.00
Single-Family Housing Improvements Program	\$82,506
Acquisition/Reuse Program for Affordable Housing	\$105,000
Public Infrastructure Improvements Program for State Ave.	\$392,789
Public Facilities Improvement Program for Non-Profits	\$134,000
Renter Affordability (Deposits, Rent & Transportation, Childcare Assistance) Programs	\$40,000
Acquisition of Public Facilities Land for Recreational Purposes(new)	\$116,000
2016-17 Program Administration	\$ 118,197
Total	\$988,492

The fifth component for a NRSA designation is HUD's review of the request, their approval, and how they will conduct on-going monitoring of activities in the area. Once the plan is submitted to HUD, it will take approximately 30-45 days for a response. If approved we can proceed with implementing the planned activities for the area. HUD will review our process through the submittal of our annual Consolidated Annual Performance Evaluation Report (CAPER), they will also conduct on-site monitoring visits periodically as well.

The sixth and final component for a NRSA designation is that the proposed request for an Amendment to the 2014-18 5-Year Consolidated Plan and 2016-17 Annual Action Plan requires that it is available for a 30-day comment period and public hearing. The 30-day comment period began on August 23, 2016 and ended on September 22, 2016, with the public hearing

scheduled with City Council on September 27, 2016. The comment period notification was sent to those who attended the open houses, the neighborhood association, and notice was placed in the area newspapers, on the City's website and social media sites. **Attachment B** is the public comments received during the comment period.

ALTERNATIVES:

1. The City Council can adopt a resolution to approve the proposed Amendment to the 2014/18 Five-Year Consolidated and the 2016-17 Annual Action Plans to create a Neighborhood Revitalization Strategy Area (NRSA) for Census Tract 13.01, Block Group 2, that borders along the south side of West Lincoln Way Corridor, the east side of South Dakota, north side of College Creek and the west side of State Avenue.
2. The City Council can decide not to approve the proposed Amendment to the 2014/18 Five-Year Consolidated and the 2016-17 Annual Action Plans to create a Neighborhood Revitalization Strategy Area (NRSA) for Census Tract 13.01, Block Group 2, that borders along the south side of West Lincoln Way Corridor, the east side of South Dakota, north side of College Creek and the west side of State Avenue.
3. The City Council can refer this item back to staff for more information.

MANAGER'S RECOMMENDED ACTION:

Amending the City's 2014-18 5-Year Consolidated and 2016-17 Annual Action Plans to create a designated NRSA, will meet both objectives to serve low and moderate income and non-low and moderate income households in this area. The 321 State Avenue (former Old Ames Middle School) site would be able to be developed as a mixed income residential subdivision under this designation. In addition to this development other specific housing related program activities (i.e. housing rehabilitation, down payment assistance, and renter affordability assistance) can be implemented for the area as well.

Therefore, it is the recommendation of the City Manager that the City Council adopt Alternative #1. This alternative will approve a resolution to submit an Amendment to the 2014-2018 Five-Year Consolidated and 2016-17 Annual Action Plans to create a Neighborhood Revitalization Strategy Area (NRSA) for Census Tract 13.01, Block Group 2, that borders along the south side of West Lincoln Way Corridor, the east side of South Dakota, north side of College Creek and the west side of State Avenue to allow flexibility in undertaking various housing and public service activities utilizing the City's CDBG funds.

ATTACHEMENT A

1	Project Name	Renter Affordability Program/Deposit First Month's Rent
	Target Area	CITY-WIDE
	Goals Supported	Create & expand Affordable Housing for LMI Persons Maintain Development Services in the Community
	Needs Addressed	Affordability & Accessibility
	Funding	CDBG: \$25,000
	Description	Funds under this project will be used to provide Deposit and/or First month rent assistance to households with annual incomes at 50% or less of the area median income limits, this project is being carried over for 2016-17.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	Under the Renter Affordability Program/Deposit and 1st Month's Rent Assistance it is anticipated that approximately 25-30 extremely low and low income households will benefit.
	Location Description	N/A
	Planned Activities	The activities under this program is provide one time funding to households and/or families with incomes at or below 50% of the Story County Median income limits with to assist them with Security Deposits and/or First Month's rent. The assistance may be expanded to include up to three months of rent assistance.
2	Project Name	Renter Affordability Programs/Transportation
	Target Area	CITY-WIDE
	Goals Supported	Maintain Development Services in the Community
	Needs Addressed	Affordability & Accessibility
	Funding	CDBG: \$10,000
	Description	Under this activity funds will be used to assist approximately households at 50% or less of the AMI with their interim transportation needs (fuel vouchers, or bus passes). This project is being carried over into 2016-17.

	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	Under the Renter Affordability Program/Transportation Assistance it is anticipated that approximately 15-25 extremely low and low income households will benefit from assistance with gas voucher and/or bus passes.
	Location Description	N/A
	Planned Activities	The activities under this program is to provide financial assistance to households and/or families with incomes at or below 50% of the Story County Median Income limits with assistance with their transportation needs through either fuel vouchers or bus passes.
3	Project Name	Renter Affordability Program/Childcare Assistance
	Target Area	CITY WIDE
	Goals Supported	Maintain Development Services in the Community
	Needs Addressed	Affordability & Accessibility
	Funding	CDBG: \$5,000
	Description	Under this activity child care assistance will be available to assist households with income at 50% or less of the Story County Median Income limits.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	Under this activity it is anticipated by 5-10 households' low income households could benefit under this project.
	Location Description	N/A
	Planned Activities	The activities under this program is provide one time funding to households and/or families with incomes at or below 50% of the Story County Median income limits with to assist them with Childcare Assistance to enable them to work or go to school. The assistance may be include 2-3 months of childcare assistance.

4	Project Name	Acquisition/ Reuse Program for Affordable Housing
	Target Area	CITY-WIDE
	Goals Supported	Create & expand Affordable Housing for LMI Persons
	Needs Addressed	Affordability & Accessibility
	Funding	CDBG: \$105,000
	Description	Under this activity funds will be used to: a. Purchase vacant in-fill lots for redevelopment into affordable housing, which may include demolition and clearance; b. Purchase of properties for rehabilitation into affordable housing. The goal is to create, expand and maintain Affordable Housing for homeless and low income households.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	For the 2016-17 program year, it is anticipated that one (1) low and moderate income household will benefit from this activity.
	Location Description	N/A
Planned Activities	Under the implementation of the Acquisition/Reuse for Affordable Housing, which will consist of the purchase of infill lots (vacant or with properties needing to be demolished and cleared); the purchase of foreclosed or blighted properties for rehabilitation, or the purchase of single-family or multi-family units that can be rehabilitated; it is anticipated that one (1) property will be acquired for reuse into either an affordable rental or owner-occupied unit for a household at 80% or less of the Story County median income limits. The activity may include demolition and clearance and/or Acquisition/Rehab.	
Planned Activities	See project description above	
5	Project Name	Homebuyer Assistance for First-Time Homebuyers
	Target Area	CITY-WIDE
	Goals Supported	Affordability and Accessibility
	Needs Addressed	Create and Expand Affordable Housing for LMI Households in the Community
	Funding	CDBG: \$0.00

	Description	The objective under this program is to provide financial assistance to qualified low- and moderate-income first-time homebuyers, with incomes at or below 80% of the AMI limits, to purchase existing and/or newly constructed single-family housing in residentially-zoned areas. The overall goal of the Homebuyer Assistance Program is to allow low- and moderate-income households to gain access to housing and/or improve their housing status. This is a roll over program from 2014-15.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	It is anticipated that 1-5 low and moderate income first-time homebuyers with incomes at 80% of the Story County Median Income Limits, could receive down payment and closing cost assistance to purchase an existing or new home.
	Location Description	N/A
	Planned Activities	see above
6	Project Name	Single- Family Housing Improvement Program
	Target Area	CITY-WIDE
	Goals Supported	Affordability & Sustainability
	Needs Addressed	Maintaining of Existing Housing Affordable Housing for LMI Persons
	Funding	CDBG: \$82,506
	Description	The Housing Improvement Program objective will be to provide financial assistance to qualified low- and moderate-income single-family homeowners at or below 80% of the area median income limits to improve the physical condition of their single family homes in residentially zoned areas. The overall goal of the Housing Improvement Program is to allow qualified low- and moderate-income households to reside in decent, safe, and sanitary housing that will enhance neighborhood sustainability.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	Under this activity it is anticipated that up to 10 single-family homeowners with incomes at 80% or less of the Story County Median Income limits would be provided assistance to make health and safety repairs to their properties.
	Location Description	N/A
	Planned Activities	See project description.

7	Project Name	Public Facilities Improvements Program for Non-Profit Agencies
	Target Area	CITY-WIDE
	Goals Supported	Sustainability
	Needs Addressed	Maintaining Community Development Services in the Community
	Funding	CDBG: \$134,000
	Description	Under this project activity financial will be provided to assist non-profit organizations that provide support services or transitional housing for low to moderate-income persons/families with funds to complete repairs and/or expansion to their facilities. The overall goal of the Public Facilities Improvement Program is to preserve and enhance facilities of non-profit agencies that house and/or provide services to homeless, special needs, very-low, and low-income residents.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	Under this activity it is anticipated that 1-5 nonprofit agencies, who serve approximately 25-100 homeless or special needs populations, can receive assistance to make repairs and/or improvements to their shelters or facilities they own to provide housing and/or basic needs services.
	Location Description	N/A
Planned Activities	See project description.	
8	Project Name	Public Infrastructure Improvements Program for State Avenue
	Target Area	State Avenue NRSA
	Goals Supported	Create and expand Affordable Housing for LMI Persons
	Needs Addressed	Affordability and Sustainability
	Funding	CDBG: \$392,789
	Description	See Planned Activities.
	Target Date	6/30/2017

	Estimate the number and type of families that will benefit from the proposed activities	It is anticipated that approximately 40-50 units could be constructed, of which at least 51% (20-26) would be made available to households with incomes at 80% or less of the Story County median income limits.
	Location Description	See Target Area
	Planned Activities	Provide infrastructure improvements (streets, water, sewer, sidewalks, etc.) for the production of affordable and market rate housing units.
9	Project Name	Acquisition of Land for Public Facilities and Improvements
	Target Area	State Avenue NRSA
	Goals Supported	Maintaining Community Development Services in the Community
	Needs Addressed	Sustainability
	Funding	CDBG: \$116,000
	Description	See Planned Activities.
	Target Date	6/30/2017
	Estimate the number and type of families that will benefit from the proposed activities	1,980 households of which 69.25 are considered LMI populations
	Location Description	See Target Area
	Planned Activities	Acquire available land in the area to maintain recreational opportunities specifically a neighborhood park and/or open green space areas.

ATTACHMENT B
2016 Public Comments/Feedback
September 8 to 22, 2016

1. We have both questions and concerns about the Amendment 2014-18 Consolidated & 2016-17 Action Plans.

Our concerns include about living and owning our property in the proposed area (Cochrane Parkway) and is this area now going to become the low income area of Ames for multiple projects?

Also with the middle school in the area, does it make since to bring low income property and many times other issues affecting the lower income population to this area? These issues include drug dealers targeting lower income property as their primary means of establishing drug addictions that predominately are targeted towards the lower income population.

Please let us know that our concerns are brought to the attention of the key decision makers and let us know where we can find out some answers to our questions.

What other areas in Ames are you looking at for the affordable housing groups? Has Sommerset been looked at? It seems like the Sommerset area may have a lot of potential. Also putting several different options in the city of Ames may make the most sense and not just put all the housing in the southwest corner.

Also the northeast pet of Ames might be nice with the new water facility being put in and the infrastructure being in place.

In the southwest part Ames, where we live, there is already a huge amount of apartments and it seems like more affordable housing in this southwest area would create a larger strain on the current homeowners.

What are the other proposals for the use of the old middle school location? It seems like it may make a good wildlife refuge or a nature preserve. It seems like if they are big proponents of the affordable housing, maybe the funds can be used to purchase some land near their residences?

I guess I am a little unsure how funds could be already used to purchase the land and then after the land is purchased, then the comments and review meetings are held. It seems a little backwards, but maybe if there is a big problem with the plan, then it can be changed at that point or the land sold off to some other entity for a different purpose?

Why is the area being made so large? Why not just limit the area to the old Ames middle school and not include the entire area? What would be the benefit of doing the entire area?

Also what other parts of the city have these Neighborhood Revitalization Strategy Area (NRSA) been created? Is this the only one? If it is the only area, why is it needed to be in the same area as the old middle school project? Wouldn't it be better to have this new area be in another location such as Sommerset area, etc?

It appears that creating this area will lower the property values within the area. Also it seems like the current are that is being talked about is already by a large apartment complex area and not sure why the market values of the southwest side of Ames seems to be of less concern than other areas of town.

ATTACHMENT A



Community Development Block Grant (CDBG)

**Amendment to the
2014-2018 Consolidated and
2016-17 Action Plans**

PROPOSED

**Application for a
Neighborhood Revitalization Strategy Area (NRSA)
Plan Designation for
State Avenue**

August 2016



Neighborhood Revitalization Strategy Area (NRSA) Plan Designation for State Avenue

Table of Contents

I. Introduction	2
II. Benefits of a NRSA	2
III. Background of a NRSA	3
IV. Boundaries	4
Figure 1-NRSA Boundary Map.....	4
V. History of the Area	5
Figure 2-Land Use Breakdown	5
VI. Demographics Indicators	6
Figure 3-Percent of LMI Persons	6
Figure 3.1-Race/Ethnic Breakdowns	6
Figure 3.2-Household & Economic Breakdowns	7
Figure 3.3-Housing & Condition of Housing Stock Breakdowns	8
Figure 3.4-Overall Owner-occupied vs Rental Occupancy Breakdowns	8
Figure 3.5- HUD HOME Income Limits for Ames MSA	9
Figure 3.6- Area Facilities, Entities or Amenities Breakdowns	9
VII. Process and Community Consultations	10
VIII. Assessment of Economic Conditions	11
a.Strengths/Opportunities	11
b.Weaknesses/Areas for Improvement.....	12
IX. Opportunities for Economic Development & Empowerment	13
X. Performance Measurements	14
Figure 4-Housing Performance Measurements	14
Figure 5-Infrastructure Performance Measurements.....	14
Figure 6-Acquisition of Land for Public Facilitates and Improvement Performance Measurements.....	14
XI. Vision	15
XII. Attachments	
Appendix A- City Council Minutes of March 1, 2016.....	16
Appendix B- City Council Special Minutes of June 21, 2016.....	23
Appendix C- Open House Invitation Letters	26
Appendix D- State Avenue Zoning Map	31
Appendix E- Census Tract 13.01 Map.....	32
Appendix F- Census Tract 13.01 Block Group2 Overall Conditions Map	33
Appendix G- Ames City Assessor Overall Condition Ratings.....	34
Appendix H- Census Tract 13.01 Block Group 2 Renter vs Owner Map.....	35

State Avenue Neighborhood Revitalization Strategy Area (NRSA)
Ames, Iowa
2014 to 2018

I. Introduction

The U.S. Department of Housing and Urban Development (HUD) encourages the establishment of Neighborhood Revitalization Strategy Areas (NRSA) as a means to create communities of opportunity in neighborhoods where at least 70% of the residents are of low to moderate incomes and/or the area falls within the upper quartile threshold of low to moderate income residents as determined by HUD. Within the Community Development Block Grant (CDBG) program, an entitlement community can designate specific areas or neighborhoods as a Neighborhood Revitalization Strategy Area (NRSA) that meet one of the above criteria. The establishment of a NRSA not only allows greater flexibility in the use of Community Development Block Grant (CDBG) funding that would promote the revitalization of those specified areas, but also serves as a policy framework for spending Community Development Block Grant (CDBG) and other HUD dollars within these established areas.

II. Benefits of a Neighborhood Revitalization Strategy Area

NRSA benefits are described in amendments to the Community Development Block Grant (CDBG) regulations at 24 CFR 570. They are as follows:

1. **Aggregation of Housing Units:** Housing units assisted pursuant to the strategy may be considered to be part of a single structure for purposes of applying for low and moderate-income national objective criteria, thus providing greater flexibility to City of Ames Neighborhood Revitalization Strategy Areas Plan 3 to carry out housing programs that revitalize a neighborhood (24 CFR 570.208(a)(3) and (d)(5)(ii)); and
2. **Aggregate Public Benefit Standard Exemption:** Economic development activities carried out under the strategy will be exempt from the aggregate public benefit standards, thus increasing a grantee's flexibility for program design as well as reducing its record-keeping requirements (24 CFR 570.209 (b)(2)(v)(L) and (M)); and
3. **Public Service Cap Exemption:** Public Services carried out pursuant to the strategy by a Community-Based Development Organization (CBDO) will be exempt from the public service cap (24 CFR 570.204(b)(2)(ii)); and
4. **Job Creation/Retention as Low/Moderate Income Area Benefits:** Job creation and retention activities undertaken pursuant to the strategy will be qualified as meeting area benefit requirements, thus eliminating the need for a business to track the income of persons that take, or are considered for, such jobs (24 CFR 570.208(a)(1)(vii) and (d)(5)(i));

III. Background of a Neighborhood Revitalization Strategy Area Designation

One of the major goals and priorities of the City's Adopted 2014-18 5-year Consolidated Strategic Plan was **"To create, expand, and maintain Affordable Housing for Homeless and Low-income persons"**. This goal and priority was to address the following two barriers that were outlined in the 2013 Impediments to Fair Housing Analysis Study:

- 1) the "lack of available, decent rental units in affordable price ranges", and
- 2) the "cost of housing" for both renters and home buyers.

The City of Ames has a long standing history of having as one of its primary missions to identify, address, and implement solutions and programs that serve the needs of the elderly, disabled, homeless, extremely low-income, low-income, and moderate-income, and families in its community. In identifying the needs, the City of Ames has continued to conduct and/or partner in commissioning reports and studies to collect data to assist in determining the needs and the actions that should be taken to address those needs.

The documentation that follows will serve as an amendment to the City of Ames current 2014-2018 Consolidated Plan and 2016-17 Annual Action Plan and contains **"estimated/approximated"** data gathered on an area in Census Tract 13.01, Block Group 2 in West Ames as it pertains to the NRSA requirements. For the purposes of this designation the area will be called the **"State Avenue" NRSA**.

The City of Ames strategy for the State Avenue NRSA describes how it meets the following criteria:

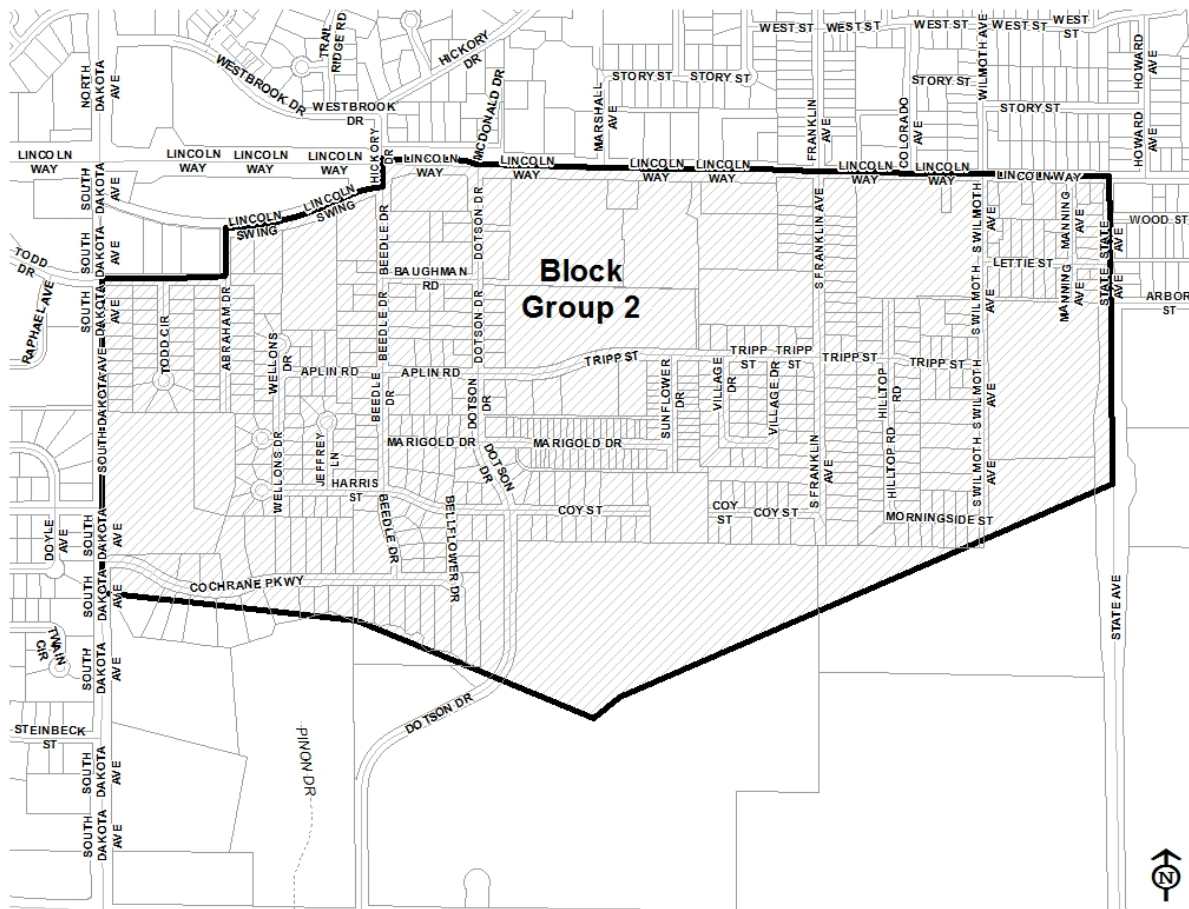
- **Boundaries:** The City has identified one strategy area and the neighborhood boundaries for which the strategy applies. All areas within those boundaries are contiguous. They are not checker boarded areas across the community.
- **Demographic Criteria:** The designated area is primarily residential and contains a percentage of low-and moderate-income residents that is equal to the "upper quartile percentage" (as computed by HUD pursuant to 24 CFR 570.208(a)(1)(ii) or 70 percent, whichever is less but, in any event, not less than 51 percent.
- **Consultation:** The City will describe how the strategy is being developed in consultation with the area's stakeholders, including residents, owners/operators of businesses and financial institutions, non-profit organizations, and community groups that are in or serve the areas.
- **Assessment & Economic Empowerment:** The City's strategy includes an assessment of the demographic conditions of the area and an examination of the strengths and weakness and the opportunities for housing development and other housing and neighborhood enhancements and improvements.
- **Performance Measurements:** The strategy must identify the results (i.e., physical improvements, social initiatives and economic empowerment) expected to be achieved, expressing them in terms that are readily measurable. This will be in the form of "benchmarks."
- **Performance Reporting:** The City will report on the progress of the NRSA at the end of each Fiscal Year as part of the Consolidated Annual Performance and Evaluation Report (CAPER).

This strategy has a **minimum three-year duration** and is being integrated into the FY 2016 Action Plan as well as the subsequent Five Year 2014 - 2018 Strategic Consolidated Plan.

IV. Boundaries of the Area

The City of Ames is proposing one (1) area for consideration as a Neighborhood Revitalization Strategy Area (NRSA.) The area is located in Census Tract 13.01, Block Group 2 in West Ames. The State Avenue NRSA includes the neighborhoods around the south side of West Lincoln Way, the East side of South Dakota Avenue, the West side of State Avenue and North of College Creek. The area encompasses a .43 square mile geographic radius.

Figure 1-Proposed NRSA Boundaries



The Housing and Community Development Division of the City of Ames Planning & Housing Department is taking the lead in the drafting of the Neighborhood Revitalization Strategy Area designation. Participants in the State Avenue NRSA include: general stakeholders—those who live, work or own a business in the area along with key development partners in the area such as banks, developers, neighborhood and community organizations.

The selection of this neighborhood as a NRSA has been determined by a combination of factors. First, the neighborhood is eligible based upon criteria established by HUD to assure programs serve communities experiencing a high concentration of low and moderate-income residents. Second, the City has determined that a specific area within the State Avenue NRSA would optimize the kind of flexibility and economic development incentive that a designation as an NRSA permits. The lack of affordable owner-occupied and/or rental housing for low income households has been identified as a priority need in the City’s 2014-2018 5-Year Consolidated Plan. With housing a major component of this economic development effort, a significant portion of the NRSA in which housing related activities will take place are necessary to have the desired economic impact.

V. History of State Avenue NRSA

The history of the West Ames indicates that urban development of the area began around 1902 and continues through the present day. The area is located in the Northern part of Washington Township in sections 1 and 4. The area for the most part was farmland until the 1950's. The areas along the Lincoln Way began to develop around the early 1910's and 1920's.

Today, the designated area contains a variety of land use districts that include, but not limited to: Residential, Commercial, Special Purpose, and Government zones, however, residentially zoned land makes up the primary use at 73.77%.

The area includes "approximately" of 491 parcels, of which 470 are residential and 21 are either commercial or governmental/public spaces. The commercial/office uses comprise approximately 8 acres and are primarily located along Lincoln Way. The commercial/office uses include: medical facilities, grocery stores, restaurants, financial institutions, gas stations/convenient stores, retail shops, and office spaces. The governmental/public spaces use comprises approximately 49 acres, which includes the vacant site of the former Ames Middle School, the current Ames Middle School Athletic Field and contains 2 neighborhood parks (Franklin and Christopher Gartner). Franklin Park the larger of the two parks (approximately 4 acres), is not owned by the City of Ames, and is currently being operating under an expired leased with Iowa State University.

The residential uses comprise approximately 164 acres and 470 residential parcels. Of the 470 residential parcels, 242 (58%) parcels contain rentals and 228 parcels contain non-rental units. The 242 rental parcels are comprised of approximately 818 apartments/condos/co-ops dwelling units, approximately 158 duplexes/two & three family conversion dwelling units, 55 townhome units, and approximately 84 single-family detached units for a total of approximately 1,115 rental units. The remaining 228 parcels are comprised of approximately 238 detached owner-occupied single-family dwellings.

Figure 2-Land Use Breakdown

	Land Use	# of Acres*	# of Parcels*	# of Units*	Notes
Residential Categories	Single-Family Detached	103	312	322	Some Parcels contain multiple Single-Family Houses
	Duplexes; two/ three-family	17	79	158	Some Parcels contain multiple Duplexes, Two/three
	Multi-Family (Apts.; Condos/Co-ops)	37	24	818	Some Parcels contain multiple Multi-family units
	Townhomes	5	55	55	Some Parcels contain multiple Townhomes
	Totals	162	470	1,353	
Non-Residential Categories	Neighborhood Parks	7	2	2	1 City-owned; 1 ISU Owned
	Commercial/Office	26	16	27	Existing Commercial Businesses
	Government/ Institutional	42	3	4	City, Schools, University, Medical Facilities, & Private
	Sub Total	75	21	33	
Grand Totals		231	489	1,386	

*Data is estimates from the City GIS, rental housing licensing & City Assessor Records databases.

VI. Demographic Background

Outlined below is the demographic background data on the proposed State Avenue NRSA. The primary sources for the statistical data for the proposed NRSA include: the Department of Housing and Urban Development (HUD), the 2006-2010 and 2010-2014 5-year American Community Survey (ACS) Estimates, the City of Ames GIS Data, the Ames City Assessor's Office and a site area survey. Relevant, tables, and maps are included in the proposed NRSA to illustrate pertinent information about the State Avenue NRSA.

The **primary** demographic criteria for qualifying as an NRSA is as follows:

The designated area must be primarily residential and contain a percentage of low- and moderate-income residents that is equal to the "upper quartile percentage" (as computed by HUD pursuant to 24 CFR 570.208(a)(1)(ii)) or 70 percent, whichever is less but, in any event, not less than 51 percent.

Using block group data based on the 2006-2010 American Community Survey and the FY-2016 Low Moderate Income Individuals (LMISD), the upper quartile of residents that are low- and moderate-income is determined to be 69.25%. Block Group 2 in Census Tract 13.01 in West Ames, which falls exactly within this upper quartile requirements and therefore can be a stand-alone NRSA designation.

Figure 3-Percent of LMI Persons

Percentage Low-Moderate Income Persons	West Ames NRSA	Ames
City of Ames Upper Quartile LMI % - 69.25%*		
Total LMI Universe	1,870	50,540
Total LMI Persons	1,295	27,195
LMI Percent	69.25	20.48
Designated Area Eligible for NRSA based on LMI %	YES	NA

*American Community Survey 5-Year 2006-2010 Low and Moderate Income Summary Data from Department of Housing and Urban Development (HUD).

Figure 3.1 – Race/ Ethnic Breakdown

	West Ames NRSA Ethnic Breakdown*		Overall City of Ames Ethnic Breakdown*	
Total Population	1,980	3%	61,276	100%
White Alone	1,860	94%	51,722	84.40%
Black Alone	4	<1%	2,105	3.40%
American Indian/Alaska Native Alone	1	<1%	123	.20%
Asian Alone	0	0.00%	5,441	8.90%
Native Hawaiian/ Other Pacific Islander Alone	0	0.00%	30	0.00%
Some Other Race Alone	38	<1%	363	0.60%
Two or More Races	77	<1%	1,492	2.40%
Hispanic Origin (Any Race)	130	<1%	1,963	3.20%

*2010-2014 American Community Survey (ASC) 5-year estimates

Note: Based on other data sources, for 2016, it is likely that the number of other races and ethnic backgrounds living in the area is much higher than the ACS indicates, however approximate numbers could not be ascertained for this report.

Figure 3.2- Household & Economic Breakdowns

Household Breakdowns*	STATE AVENUE NRSA	Overall City of Ames
Average Household size by owner	2.43	2.41
Average Household by renter	2.07	2.14
Median age (years)	22.8	23.5
Economic Breakdowns*	STATE AVENUE NRSA	Overall City of Ames
Median Household Income	\$32,222	\$42,373
Median Family Income	\$73,182	\$80,977
Percent of Families earning <\$24,999	10%	12%
Percent of Families earning \$25,000 to \$49,999	32%	15%
Percent of Families earning \$50,000 to \$74,999	16%	19%
Unemployment Rate	6.62%	4.10%
Poverty Status of Individuals	44.5%	30%
Percent of people (25 years and older) graduated from High school	12%	12%
Percent of people (25 years and older) graduated with Bachelor Degrees	36%	33%
Percent of people (25 years and older) graduated with Masters + Degrees	26.34%	29.41%

*2010-2014 American Community Survey (ASC) 5-year estimates

Figure 3.3- Housing & Condition of Housing Stock Breakdowns

Housing Breakdowns *, **, ***	STATE AVENUE NRSA	Overall City of Ames
# of Occupied Housing Units	1,189**	23,566**
Owner-occupied to Renter-occupied units	238 (18%)*	9,787 (41%)**
Renter-occupied to Owner-occupied units	1,115 (82%)*	13,779 (58%)**
Median owner-occupied housing values	\$120,900***	\$172,600***
Gross Rent	\$731***	\$774***
Housing units built: Before 1980	454 (47%)**	12,080 (49%)**
Housing units built: After 1980	518(53%)**	12,401(51%)**
Percent of vacant housing	5%***	3.70%***
Tax Credit/Low-Income Subsidized Housing Units	145 (27%)*	545*
Condition of the Housing Stock: **		
Poor 1.0	1	29
Poor 1.5	2	33
Below Average 2.0	14	198
Below Average 2.5	31	472
Average 3.0	511	12,137
Above Average (3.5)	80	2,218
Good (4.0)	38	1,302
Very Good (4.5)	4	452
Excellent (5.0)	16	104

*Data is estimates from the City GIS, rental housing licensing & City staff

** City Assessor Records databases, does not include all condos that have been added to the market.

***2010-2014 American Community Survey (ASC) 5-year estimates

Figure 3.4- Overall Owner- Occupied vs. Rental Occupancy Breakdowns

Occupancy Status*	# Parcels	# of Units	% based on # of Units
Owner Occupied	228	238	18%
Rental	242	1,115	82%
Totals	502	1,353	100%

*Data is estimates from the City GIS, rental housing licensing & City Assessor Records databases.

Figure 3.5 HUD HOME Income Limits for Ames Metropolitan Statistical Area (MSA)

*Average family size for the City of Ames

Family Size									
Median	Income Category	1	2	3*	4	5	6	7	8
\$80,977	Extremely Low (30%)	\$16,700	\$19,100	\$21,500	\$23,850	\$25,800	\$27,700	\$29,600	\$31,500
	Very Low (50%)	\$27,850	\$31,800	\$35,800	\$39,750	\$42,950	\$46,150	\$49,300	\$52,500
	Low (80%)	\$44,550	\$50,900	\$57,250	\$63,600	\$68,700	\$73,800	\$78,900	\$84,000

Figure 3.6- Area Facilities, Entities or Amenities Breakdowns

Type of Facilities/Entities/Amenities	# of Facilities/Groups
Grocery/Ethnic Markets	2
Restaurants	6
Financial Institutions	3
Medical Facilities (4) /Pharmacies (1)	5
Non-Profit Organizations/Community Organizations	3
Gas Stations/Convenience Stores	2
Neighborhood Associations	1
Offices/Other Business	3
Transportation Access (Cy-Ride, Uber, Taxi Services)	3

VII. Process and Community Consultation

In order to create a NRSA designation, the City is required to amend its Community Development Block Grant 2014-18 5-year Consolidated Plan and 2016 Annual Action Plans. Thereby, the City will follow the Citizen Participation it uses in developing these Plans in order to create a State Avenue NRSA designation. This involves advertising in the area newspapers, on various social media outlets and directs mailings of public forums being held to gather input from non-profit organizations, human service agencies, community groups, citizens, etc. It will involve meeting directly with impacted neighborhood associations, area businesses, area residents, area property owners, property managers, and area institutional organizations.

The City of Ames welcomes input regarding the concept of creating a NRSA in this area. Beginning in February 2016, City staff, as part of preparing its 2016-17 Annual Action Plan, hosted public forums, and meetings with the impacted neighborhood association to educate and to gain input regarding creating a NRSA. In March 2016, staff conducted a public meeting with City Council as a follow-up to the meetings with the public and neighborhood association regarding the NRSA concept. With consensus from the community and the neighborhood association, City Council directed staff to proceed with the process of amending the City's 2014-18 Adopted Five-Year Consolidated Plan to create a Neighborhood Revitalization Strategy Area in the West Ames, Census Tract 13.01; Block Group 2 area. (see Attachments A & B- City Council Minutes)

As outlined in the NRSA guidelines established by HUD, staff through directed mailings, invited and hosted specific individual community consultations with the following groups: neighborhood association, area businesses/operators, (financial institutions, non-profit organizations/community organizations, government institutions), area residents, property owners and managers, developers). During these community consultations, staff discussed a conceptual framework for a State Avenue West NRSA, specifically regarding the redevelopment of a 10+-acre parcel of land owned by the City in the area, along with various types of programs and services that could be provided to the area. The impacted neighborhood association has been very engaged in the process and provided feedback as noted in the above paragraph. Additionally, they have been very helpful with outreach to the residents of the area. The participation from the business/operators centered on questions about the overall concept of NRSA and what benefits could be applicable to their business needs. There was only one concern expressed by an area developer, that having a NRSA designation could be viewed as a negative impact on the area. (see Attachment C-Open House Invitations)

Any comments received during the 30-day public comment period and during the public hearing, including responses to those comments will be attached with this amendment.

VIII. Assessment of Economic Conditions

- There are a total of approximately 1,189 housing units in the designated NRSA area, of which 922 units (78%) is renter-occupied; and 267 units (22%) are owner-occupied, this is approximately a 20% higher rental occupancy than the city as a whole;
- The percent of vacant housing in the designated area is approximately 5%, which is slightly higher than the city at 3.70% as a whole;
- The unemployment rate in the area is 6.26%, which is slightly higher than the 4.10% for the city as a whole;
- The median household income in the area is \$32,222, which is lower as compared to \$42,373 for the city as a whole;
- The poverty status of individuals in the area is 44.5%, compared to 30% of the city as a whole;
- The education attainment of the residents beyond high school in the designated area is comparable to that of the residents in the city as a whole;
- Gross rent median dollars is \$731, is about \$43 dollars less, compared to \$774 for the city as a whole;
- The median family income in the area \$73,182, which is just over \$7,000 (10%) lower in comparison to that of \$80,977 for the city as a whole;
- The number of families earning less than \$50,000 is 42%, compared to 27% in the city overall
- Median owner-occupied housing values are \$120,900 within the area, compared to \$172,600 for the city as a whole;
- The percent of housing units built before 1980 is comparable to that in the city as a whole.
- The area contains approximately 145 (27%) units of Low-Income Tax Credit/Low-Income Tax Credit/Low-Income Subsidized Housing Units, compared to 545 units in the city as a whole;
- For the size of the area, it contains a good high number of commercial businesses, medical facilities, financial institutions, restaurants, parks and access to various transportation services;
- The 90% of overall condition of the housing stock is ranked Average to Excellent.

Strength/Opportunities

- Availability of vacant land owned by the City of Ames that can be re-developed into low and moderate cost housing for either owner and/or renter households;
- Strong commercial and retail businesses;
- Access to City-wide Cy-Ride Bus Service;
- A number of medical facilities, financial institutions, retail shops, & restaurants;
- Active Neighborhood Association;
- Updated Infrastructure Improvements have occurred within the last 3-5 years.

Weaknesses/Areas for Improvement

- Slightly higher unemployment rate
- Higher poverty rate compared to the city overall
- Higher percent of rental housing units compared to the city overall
- Higher percent of families earning less than \$30,000 compared to the city overall
- Neighborhood parks are underutilized, Franklin Park is not owned by the City

The proposed State Avenue NRSA is a strong and active area that contains good services and amenities for residents living in the area. The weaknesses can be attributed to the high number of the student rental population in the area, which will affect the poverty and unemployment rates and annual earnings of households.

XI. Opportunities for Economic Development and Empowerment

As documented in the City's 2013 Impediments to Fair Housing Study, there is a shortage of affordable housing units of both owner-occupied and rental units for households with incomes at 80% or less of the Ames Metropolitan Statistical Area (MSA). To address this issue, the City of Ames has adopted in its 2014-2018 CDBG Consolidated Plan the following goals and priorities:

1. Goal: Utilize and leverage CDBG Funds for Low and Moderate Income Persons through private and public partnerships as follows:

A1. Objective: To create, expand and maintain Affordable Housing for Homeless and Low-income persons.

Outcomes:

- i. Increase the supply of affordable rental housing
- ii. Improve the quality of affordable rental housing
- iii. Increase the availability of affordable owner-occupied housing
- iv. Maintain the supply of affordable owner-occupied housing
- v. Provide temporary rental assistance
- vi. Increase the supply of mixed-use development
- vii. Expand and maintain the supply of emergency shelter and transitional housing

A2. Objective: To maintain the Community Development Services of the Community

Outcomes:

- i. Continue provision of the Public Service Needs for homeless, special populations, and low-income households (utilities, rent, deposits, childcare, transportation, employment training, substance abuse, health services, legal services, other public service needs) and reduce duplication of services.
- ii. Continue provision of Public Facilities Needs for homeless, special populations and low-income households (senior centers, homeless facilities, child care centers, mental health facilities, neighborhood facilities, and other public facilities needs).
- iii. Continue provision of Public Infrastructure Needs in low-income census tracts (water, street, sidewalk improvements).

2. Goal: Utilize and leverage CDBG Funds for NON-Low and Moderate Income Persons through private and public partnerships as follows:

A1. Objective: Address Housing Needs in Non-Low and Moderate Income Census Tracts

Outcomes:

- i. Integrate affordable and market rate residential developments
- ii. Remove blight and deteriorated housing to reuse into new housing
- iii. Support and address code enforcement of deteriorated housing
- iv. Remove blight and deteriorated housing in flood plain and other hazardous areas.

The State Avenue NRSA is buoyed by private market activity in both the housing and commercial sectors. However, based on the data outlined in this document, the challenges ahead include the ability to: 1) create, through infrastructure and housing development, additional owner-occupied housing units for both low and moderate income households; 2) to improve the owner-occupied existing housing stock conditions; 3) to increase the public service program opportunities to very low income households find decent, safe and affordable rental housing units; and 4) Maintain a neighborhood park for use for families and households.

Emphasis on the above four areas will a positive impact and provide balance to this proposed NRSA.

X. Performance Measures

There are three years remaining in the City’s 2014-18 5-year Consolidated Plan in which, the opportunity to address the above four (4) challenges are available. In the City’s current 2016-17 Annual Action Plan, the following program activities and funding have been outlined to help address three of the four challenges that include the following:

- 1) Infrastructure Improvements Program for 321 State Avenue for housing production;
- 2) Home Improvement Rehabilitation Program for single-family owner-occupied housing units;
- 3) Homebuyer Assistance Program; and
- 4) Renter Affordability Programs

The fourth challenge is to maintain an existing neighborhood park in the NRSA. Neighborhood parks provide much needed open space, a recreational area for families to play and a place for the neighborhood to gather. Not securing this site may result in the one of two parks in the area to be redeveloped into another use.

Setting performance benchmarks in the proposed State Avenue NRSA will allow for more effective and efficient monitoring and evaluation strategies.

The tables below identify the goals and objectives for each performance measure that can be addressed over the next three remaining years of the 5-years of the Consolidated Plan:

Figure 4. Housing Performance Measurements

1. Objective: Increase the number of owner-occupied housing units for family households Incomes at 80% of the Ames MSA
Benchmark: Provide Homebuyer Assistance to 12-15 homebuyers in purchasing newly constructed or rehabilitated housing over the next 3 years
2. Objective: Maintain and sustain the current housing stock for owner-occupied households with incomes at 80% or less of the Ames MSA through a Home Improvement Rehabilitation type Programs.
Benchmark: Provide financial assistance to rehabilitate 5-10 owner-occupied homes over the next 3 years.
3. Objective: Increase the supply and affordability of rental housing for households at 60% or less of the Ames MSA in the NRSA
Benchmark: Assist 5-7 households with Deposit, 1 st Month Rent, Transportation and/or Childcare needs with incomes at 60% or less of the Ames MSA per year over the next 3 years

Figure 5. Infrastructure Performance Measurements

Objective: Install infrastructure improvements (streets, utilities, sidewalks) at 321 State Avenue to increase the stock of mixed affordable owner-occupied housing for the area.
Benchmark: Provide assistance to builders/developers for the construction of 40-50 lots for single family housing units over the next three years, of which at least 51% (20-26) would be available to family households with incomes at 80% or less of the Ames MSA.

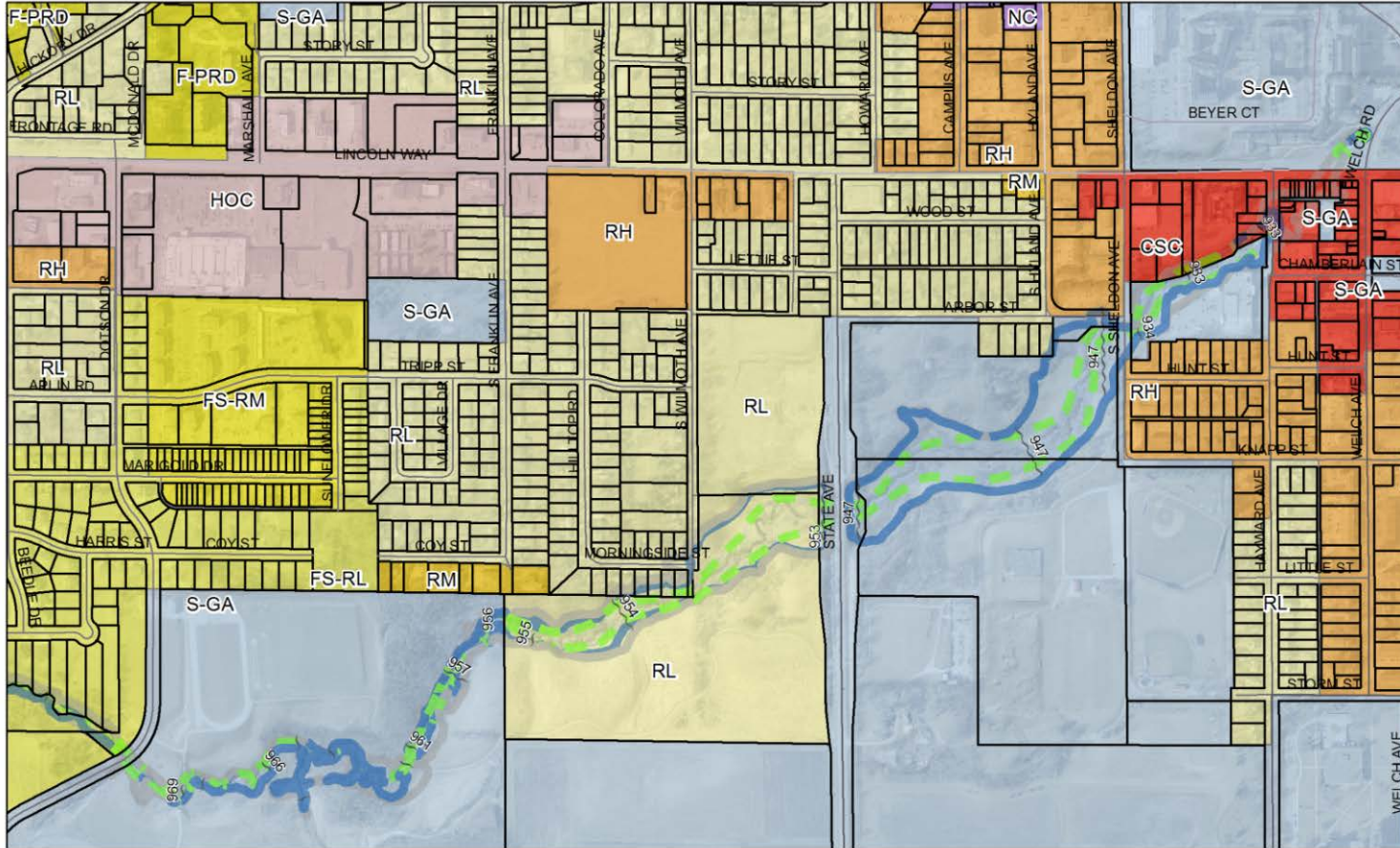
Figure 6. Acquisition of Land for Public Facilities and Improvement Performance Measurements

Objective: Assist in maintaining or in the development of recreational opportunities within NRSA, specifically that of a neighborhood park and/or open space areas.
Benchmark: Acquire available land to maintain or relocate a neighborhood park within the next year.

XI. Vision

The State Avenue NRSA is envisioned as a community where business, residential and recreational activities flourish. This area has good jobs that are accessible, can attain a balance of housing units affordable to and occupied by low-, middle- and high-income residents. Recreational opportunities that exist for area children and adults and attract people from the community. With the active neighborhood association, the physical environment of the State Avenue NRSA can be well maintained and free vacant and underutilized properties. The neighborhood commercial areas are well defined, vital and well connected by streets that accommodate people using public transportation, in cars, on foot and on bicycles in attractive, safe settings. Commercial districts provide the goods and services area residents want and need are integrated, as are area residents, into the larger economy of Ames.

Appendix D-321 State Avenue Zoning Map



321 State Zoning Map

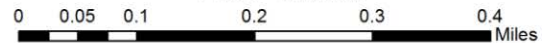
© City of Ames, Iowa makes no warranties, expressed or implied, including without limitation, any warranties of merchantability or fitness for a particular purpose. In no event shall the City of Ames be liable for lost profits or any consequential or incidental damages caused by the use of this map.

Date: 6/15/2016

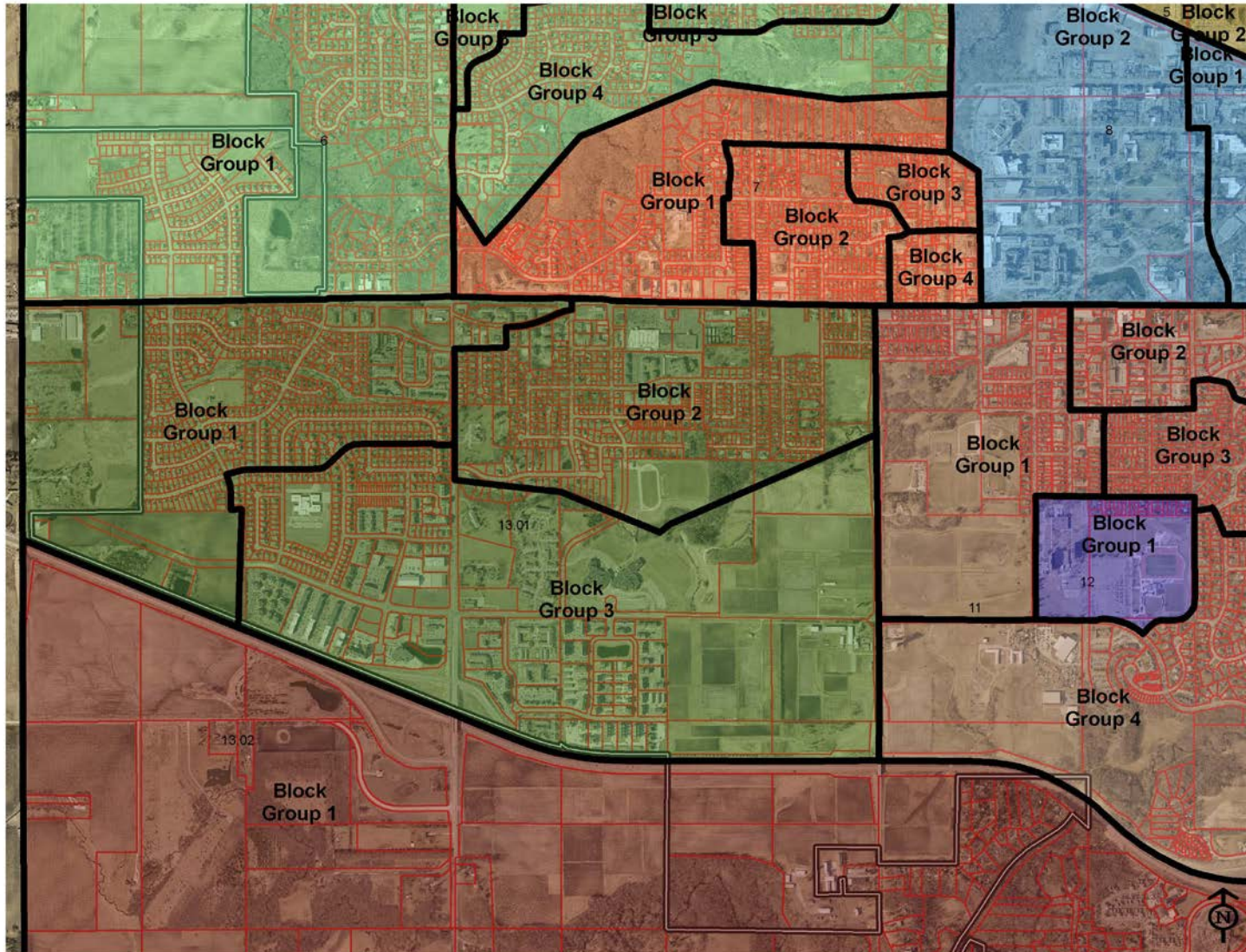


NORTH

1 inch = 600 feet



Appendix E-Census Tract 13.01 Map



COUNCIL ACTION FORM

SUBJECT: PUBLIC HEARING ON ACCEPTANCE & SUBMITTAL OF THE CITY'S 2015-16 CONSOLIDATED ANNUAL PERFORMANCE AND EVALUATION REPORT (CAPER) IN CONNECTION WITH THE CITY'S COMMUNITY DEVELOPMENT BLOCK GRANT PROGRAM (CDBG)

BACKGROUND:

The Community Development Block Grant (CDBG) regulations require that within 90 days from the end of its fiscal year, the City must prepare a Consolidated Annual Performance and Evaluation Report (CAPER). The 2015-16 CAPER reports accomplishments in relation to goals and objectives identified in the City's 2014-2018 Consolidated Plan and in the Annual Action Plan for fiscal year July 1, 2015, through June 30, 2016. Federal regulations require that the CAPER be available for a 15-day public review and comment period, which occurred September 8, 2016, through September 22, 2016. The approved CAPER is required to be submitted to the Department of Housing and Urban Development (HUD) on or before September 29 each year.

Attached for Council review and approval is a copy of the Executive Summary of the 2015-16 CAPER, which reports that of the \$1,047,965 budgeted (not including administration), \$699,765 was expensed on the following housing-related activities: Renter Affordability, Homebuyer Assistance, Operation/Repair, and Acquisition/Reuse of Properties for Affordable Housing Program Activities. Fifty-seven (57) low-income households were assisted. Additionally, approximately \$200,883 of program income was generated from the sale and repayments of single-family homes that were sold through the Homebuyer Assistance Program.

A full copy of the CAPER and attachments are available for review on the City's web site at: www.cityofames.org/housing.

ALTERNATIVES:

1. The City Council can adopt a resolution approving the submittal of the City's 2015-16 Consolidated Annual Performance and Evaluation Report (CAPER).
2. The City Council can deny adoption of a resolution approving the submittal of the City's 2015-16 Consolidated Annual Performance and Evaluation Report (CAPER).

MANAGER'S RECOMMENDED ACTION:

It is the recommendation of the City Manager that the City Council adopt Alternative #1. This action will adopt a resolution approving the submittal of the City's 2015-16 Consolidated Annual Performance and Evaluation Report (CAPER), which is to be submitted to HUD on or before September 29, 2016.



CITY OF AMES

**CONSOLIDATED ANNUAL PERFORMANCE AND
EVALUATION REPORTS (CAPER)**

**CITY OF AMES FISCAL YEAR
JULY 1, 2015 THROUGH JUNE 30, 2016**



**Public Comment Period:
September 8, 2016 thru September 22, 2016**

Table of Contents

CR-05-Goals and Outcomes	1
CR-10-Racial and Ethnic composition of families assisted	13
CR-15-Resources and Investments	14
CR-20-Affordable Housing	16
CR-25-Homeless and Other Special Needs	19
CR-30-Public Housing	21
CR-35-Other Actions	22
CR-40-Monitoring	26
CR-45-CDBG 91.520(c)	27
Other Documents:	28
Appendix I - Integrated Disbursement and Information System Reports (IDIS)	
Appendix II - Program Maps and Budgets	
Appendix III - ASSET Priorities & Funding, & Point in Time Count	

CR-05 - Goals and Outcomes

Progress the jurisdiction has made in carrying out its strategic plan and its action plan. 91.520(a)

This could be an overview that includes major initiatives and highlights that were proposed and executed throughout the program year.

With community input, the overall goals and outcomes of the City's 2014-18 Strategic Plan is to increase the supply of affordable housing for low and moderate income persons, and to continue to support and maintain the public service needs for special populations, homeless, and low income households. The major progress that the City made in carrying out these two overall strategies was through the implementation of the following project activities for 2015-16: Acquisition/Reuse for Affordable Housing, Renter Affordability Program, Disposition of Properties, Acquisition/Rehab of Properties, and Public Facilities Improvements for Non-profit Organizations.

The **Acquisition/Reuse Program** was designed to create, expand, and maintain affordable housing for homeless and low-income persons (80% or less of AMI) by: a. increasing the supply of affordable rental housing, b. improving the quality of affordable rental housing, c. increasing the availability of affordable owner-occupied housing or d. maintaining the supply of affordable owner-occupied housing. During the program year the acquisition of one 10-acre parcel (old Ames Middle School site) was completed. The purchase of this parcel can provide up to approximately 40-50 housing units for owner-occupied and/or rental purposes.

The **Renter Affordability Program** was designed to provide assistance to low-income households who are at or below 50% or less of the Story County median income limits, gain access to rental housing units that will improve their housing status, and help them to secure economic stability in order to obtain and/or remain in affordable housing units. The activities implemented were a Deposit, First Month's Rent and Transportation (Bus Passes and Fuel Vouchers) Assistance. During the program year, 71 households with incomes at 50% or below the AMI were assisted. Of the 71 households assisted, 36 received assistance with Deposit and/or First Month's Rent and 35 were received assistance with Transportation (21- fuel vouchers, 11-Cy-ride Bus Passes & 3-HIRTA Bus Passes).

The **Disposition of Properties** activity included the on-going maintenance of five lots that were purchased during previous program years (State Avenue, Sixth Street and Maxwell). State Avenue and the Sixth Street properties (three lots) are anticipated to be developed jointly within the remaining three program years of the 2014-18 5-Year Consolidated Plan. The Maxwell lot is anticipated to be sold to Habitat for Humanity in the 2016-17 program year and developed in program year 2017-18. The **Acquisition/Rehabilitation Activity** includes three properties (Roosevelt, Wellons and one duplex on Stafford).

During the 2015-16 program year, the Roosevelt property was sold to Habitat for Humanity and Habitat then sold the property to an eligible Habitat Homebuyer during the same program year. The emergency repairs on Wellons were completed, and the property is anticipated to be sold to Habitat for Humanity for major rehabilitation and selling of the property to an eligible Habitat home buyer will occur in program year 2016-17.

The rehabilitation of Stafford property has been completed. It is anticipated to be sold to an eligible non-profit organization in FY 2016-17, in which the property will be rented to low-income families.

Under the **Public Facilities Improvements Program for Non-Profits**, applications were solicited during the 15- 16 program year, and the awarding of projects and improvements to various non-profit facilities will occur in the 2016-17 program year.

The Neighborhood Housing Improvements and the Homebuyer Assistance Programs were not implemented during this program year. The opportunity to acquire a large parcel of land to expand the development of affordable housing for low income persons and households – which addresses the heart of the strategic plan – became the primary focus of all the activities that were implemented for the 2015-16 program year.

In addition to the outcomes listed below, a summary of accomplishments in attaining the goals and objectives for the reporting period can be found in Appendix I, along with a project map and budget in Appendix II.

Comparison of the proposed versus actual outcomes for each outcome measure submitted with the consolidated plan and explain, if applicable, why progress was not made toward meeting goals and objectives. 91.520(g)

Categories, priority levels, funding sources and amounts, outcomes/objectives, goal outcome indicators, units of measure, targets, actual outcomes/outputs, and percentage completed for each of the grantee's program year goals.

In the Non-LMI Category, there has not been a high need identified in the 5-Year Plan. Therefore, there are no goals or budget to address this category. For the rehabilitation of owner-occupied and/or rental housing units, and for home buyer assistance these programs have been put on hold in order to address the most urgent need to increase the supply of affordable housing through the acquisition of land and/or properties and the cost of public infrastructure to create lots for the development of affordable housing.

Goal	Category	Source/ Amount	Indicator	Unit of Measurement	Expected/ Strategic Plan	Actual/ Strategic Plan	Percent Complete	Expected Program Year	Actual Program Year	Percent Complete
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$700,000	Increase the supply of Affordable Housing through the acquisition of properties or Land for both rental and/or home ownership	Household Housing Units	10	6	60%	1	1	100%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$0.00	Installation of Public Infrastructure Improvements to create lots for affordable housing	Public Infrastructure (streets, water, sewer, sidewalks, etc.)	50	6	12%	0	0	0%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$0.00	Rental units constructed	Household Housing Units	10	10	0.00%	0	0	0.00%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$75,000	Rental units rehabilitated	Household Housing Units	15	2	13%	1	1	100%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$100,00	Homeowner Housing Rehabilitated	Household Housing Unit	60	60	0.00%	10	0	0.00%

CMB Control No: 2506-0117 (exp. 07/31/2015)

Goal	Category	Source/ Amount	Indicator	Unit of Measurement	Expected/ Strategic Plan	Actual/ Strategic Plan	Percent Complete	Expected Program Year	Actual Program Year	Percent Complete
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$50,000	Direct Financial Assistance to Homebuyers	Households Assisted	15	15	13.33%	2	1	50%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	ASSET: \$371,907	Overnight/Emergency Shelter/Transitional Housing Beds added	Beds	10	10	0.00%	10	10	100%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$22,000	Buildings Demolished	Buildings	5	1	20.00%	1	1	100.00%
Create & expand Affordable Housing for LMI Persons	Affordable Housing	CDBG: \$113,000	Housing Code Enforcement/Foreclosed Property Care	Household Housing Unit	10	6	60.00%	4	4	100.00%
Maintain Development Services in the Community	Affordable Housing Homeless Non-Homeless Special Needs	CDBG: \$100,000	Public Facility or Infrastructure Activities for Low/Moderate Income Housing Benefit	Households Assisted	6	6	-	6	0	0.00%

Goal	Category	Source/ Amount	Indicator	Unit of Measurement	Expected/ Strategic Plan	Actual/ Strategic Plan	Percent Complete	Expected Program Year	Actual Program Year	Percent Complete
Maintain Development Services in the Community	Affordable Housing Homeless Non-Homeless Special Needs	CDBG: \$40,000	Public service activities for Low/Moderate Income Housing Benefit (Renter Affordability Programs)	Households Assisted	300	300	--	50	55	100%
Maintain Development Services in the Community	Affordable Housing Homeless Non-Homeless Special Needs	ASSET: \$379,786	Homelessness Prevention	Persons Assisted	1,000	1,000	--	500	574	100%

Table 1 - Accomplishments – Program Year & Strategic Plan to Date

Assess how the jurisdiction's use of funds, particularly CDBG, addresses the priorities and specific objectives identified in the plan, giving special attention to the highest priority activities identified.

For the 2015-16 program year, approximately \$1,219,501 (not including administration) was allocated to implement the program activities listed under CR-05 Goals and Outcomes. Of that amount, approximately \$699,765 was expended on housing related activities that were specific objectives identified in the 5-year Consolidated Plan. The specific objectives are to create and expand affordable housing for low and moderate income households. The creation and expansion of affordable housing was accomplished primarily through the implementation of the Acquisition/Reuse for Affordable Housing, the Renter Affordability and the Acquisition/Rehabilitation of Properties Programs. Special attention was given to the highest priority activity, which was the implementation of the Acquisition/Reuse for Affordable Housing Program. Approximately \$564,275 was expended purchasing a 10-acre parcel of land for future development of affordable housing lots.

In addition to CDBG funds, the FY 15-16 ASSET funding recommendation was approximately \$3,615,916. Of that amount, the City's recommended share was approximately \$1,216,031. The City's share of ASSET funding expended for FY 15-16 was approximately \$1,153,364 towards addressing the goal to maintain important Development Services in the community that cover basic human needs. Of the \$1,153,364, approximately \$379,768 was spent to provide housing services to homeless, non-homeless, and special needs households in Ames by the following agencies: Youth and Shelter Services, Good Neighbor, Emergency Residence Project, Assault Care Center Extending Shelter & Support and The Salvation Army. Through the efforts of these agencies, approximately 574 households/persons were assisted.

ITEM # 41
DATE: 9-27-16

COUNCIL ACTION FORM

SUBJECT: AMENDMENT TO FISCAL YEAR 2016/17 ADOPTED BUDGET FOR CARRYOVERS FROM FISCAL YEAR 2015/16

BACKGROUND:

Each year the City has capital projects and specific operating projects that either span fiscal years or are delayed due to unforeseen circumstances. Until two years ago, the unspent budget for these approved projects was added as an amendment to the current year's adopted budget concurrent with the approval of the next fiscal year's budget. This meant that the amendment for unspent funds did not receive official City Council approval until just prior to the March 15 annual budget certification deadline.

The Code of Iowa requires that city spending by program not exceed Council approved budget amounts at any time during the year. The City of Ames monitors spending against the approved budget and has maintained compliance with Iowa Code. **However, delays in large construction projects such as the electric power plant fuel conversion and the new water treatment plant revealed situations where spending in excess of approved budget amounts could potentially occur if we wait to amend the budget for carryover amounts until March.**

For the last two years, a fall amendment to add the carryover projects from the prior year to the current adopted budget has been submitted to City Council for approval. A carryover amendment has been prepared again this year to carryover unspent funds from 2015/16. A summary is attached describing the carryovers, which total \$64,822,515.

Please note that all the projects and associated budgeted expenditures and funding sources were approved by City Council as part of the fiscal year 2015/16 budget, but were not completed during the year. The amendment provides formal Council authority to carry forward the appropriation for projects and other work that will not be spent until fiscal year 2016/17.

Amending the budget for carry over amounts at this time, rather than waiting until March, improves the ability of departments to monitor project spending and for Finance staff to track budget compliance.

ALTERNATIVES:

1. Adopt a resolution amending the fiscal year 2016/17 budget upwards by \$64,822,515 for carryover amounts from fiscal year 2015/16.

2. Refer this item back to staff for additional information or other adjustments to the amendments.

MANAGER'S RECOMMENDED ACTION:

Amending the FY 2016/17 budget for carryover amounts from the FY 2015/16 budget early in the fiscal year will provide for improved budget monitoring and tracking. It will also provide assurance that Council-approved projects and work not completed in the prior year will not be delayed for spending authority.

Therefore it is recommended that City Council approve Alternative No. 1, thereby adopting a resolution amending the fiscal year 2016/17 budget upwards by \$64,822,515 for carryover amounts from fiscal year 2015/16.

CITY OF AMES, IOWA

2015/16

CARRYOVER REPORT

TABLE OF CONTENTS

Expenditure Change Summaries.....	1-3
Explanation of Expenditure Changes.....	4
Fund Summary.....	7
Notice of Public Hearing Publication (Amendment of Current City Budget).....	9

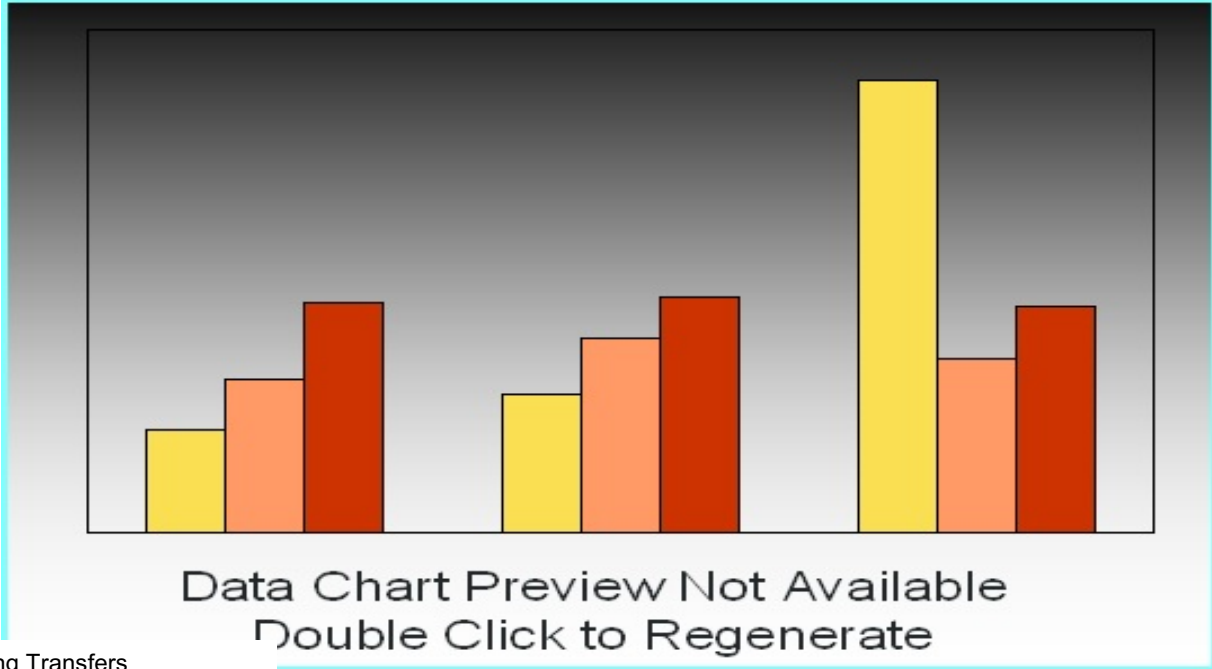
EXPENDITURE CHANGES BY PROGRAM

	2016/17 Adopted	2015/16 Carryover	2016/17 Adjusted	% Change From Adopted
<i>Public Safety:</i>				
Law Enforcement	9,129,677	64,000	9,193,677	0.7%
Fire Safety	6,810,174	21,113	6,831,287	0.3%
Building Safety	1,475,907	7,500	1,483,407	0.5%
Animal Control	387,981	28,685	416,666	7.4%
Other Public Safety	844,500	-	844,500	0.0%
Public Safety CIP	47,000	75,226	122,226	160.1%
<i>Total Public Safety</i>	<u>18,695,239</u>	<u>196,524</u>	<u>18,891,763</u>	<u>1.1%</u>
 <i>Utilities:</i>				
Electric Services	52,757,225	114,053	52,871,278	0.2%
Water and Pollution Control	7,550,450	74,134	7,624,584	1.0%
Water Distribution	1,303,194	1,050	1,304,244	0.1%
Sanitary Sewer System	893,282	6,550	899,832	0.7%
Storm Water Management	606,532	7,700	614,232	1.3%
Resource Recovery	4,035,755	-	4,035,755	0.0%
Utility Customer Service	1,517,603	8,000	1,525,603	0.5%
Utilities CIP	46,135,100	38,284,256	84,419,356	83.0%
<i>Total Utilities</i>	<u>114,799,141</u>	<u>38,495,743</u>	<u>153,294,884</u>	<u>33.5%</u>
 <i>Transportation:</i>				
Street System	5,363,169	49,200	5,412,369	0.9%
Transit System	10,619,036	-	10,619,036	0.0%
Public Parking	876,665	-	876,665	0.0%
Airport Operations	146,213	-	146,213	0.0%
Transportation CIP	14,736,040	19,203,555	33,939,595	130.3%
<i>Total Transportation</i>	<u>31,741,123</u>	<u>19,252,755</u>	<u>50,993,878</u>	<u>60.7%</u>
 <i>Community Enrichment:</i>				
Parks and Recreation	4,306,331	104,207	4,410,538	2.4%
Library Services	4,198,078	160,937	4,359,015	3.8%
Human Services	1,302,478	-	1,302,478	0.0%
Art Services	197,170	-	197,170	0.0%
Cemetery	169,795	-	169,795	0.0%
City-Wide Housing Programs	46,708	-	46,708	0.0%
CDBG Program	483,452	-	483,452	0.0%
Economic Development	1,882,857	-	1,882,857	0.0%
Cable TV	146,266	-	146,266	0.0%
Community Enrichment CIP	962,500	1,695,395	2,657,895	176.1%
<i>Total Community Enrichment</i>	<u>13,695,635</u>	<u>1,960,539</u>	<u>15,656,174</u>	<u>14.3%</u>

EXPENDITURE CHANGES BY PROGRAM, continued

% Change

	2016/17 Adopted	2015/16 Carryover	2016/17 Adjusted	From Adopted
<i>General Government:</i>				
City Council	345,643	46,389	392,032	13.4%
City Clerk	345,162	-	345,162	0.0%
City Manager	757,788	-	757,788	0.0%
Public Relations	192,357	153,293	345,650	79.7%
Planning Services	819,813	335,313	1,155,126	40.9%
Financial Services	1,769,105	-	1,769,105	0.0%
Legal Services	664,225	9,216	673,441	1.4%
Human Resources	466,950	-	466,950	0.0%
Facilities	434,698	127,203	561,901	29.3%
Merit Adjustment	187,595	-	187,595	0.0%
General Government CIP	50,000	1,832,134	1,882,134	3664.3%
Total General Government	6,033,336	2,503,548	8,536,884	41.5%
<i>Debt Service:</i>				
General Obligation Bonds	11,366,737	-	11,366,737	
Electric Revenue Bonds	968,556	-	968,556	0.0%
SRF Loan Payments	1,557,303	-	1,557,303	0.0%
Total Debt Service	13,892,596	-	13,892,596	0.0%
<i>Internal Services:</i>				
Fleet Services	3,170,334	1,427,295	4,597,629	45.0%
Information Technology	2,463,338	845,669	3,309,007	34.3%
Risk Management	2,402,295	-	2,402,295	0.0%
Health Insurance	8,986,528	-	8,986,528	0.0%
Internal Services CIP	135,000	140,442	275,442	104.0%
Total Internal Services	17,157,495	2,413,406	19,570,901	14.1%
<i>Total Expenditures</i>				
<i>Before Transfers</i>	216,014,565	64,822,515	280,837,080	30.0%
<i>Transfers</i>	19,034,642	-	19,034,642	0.0%
Total Expenditures	235,049,207	64,822,515	299,871,722	27.6%



EXPENDITURE CHANGES BY CATEGORY

	2016/17 Adopted	2015/16 Carryover	2016/17 Adjusted	% Change From Adopted
Personal Services	59,102,631	36,270	59,138,901	0.1%
Contractual	59,819,262	1,233,445	61,052,707	2.1%
Commodities	10,305,675	782,842	11,088,517	7.6%
Capital	62,629,959	62,769,958	125,399,917	100.2%
Debt	13,892,596	-	13,892,596	0.0%
Other (Refunds, Insurance Claims, etc.)	10,264,442	-	10,264,442	0.0%
Total Expenditures Before Transfers	216,014,565	64,822,515	280,837,080	30.0%
Transfers	19,034,642	-	19,034,642	0.0%
Total Expenditures	235,049,207	64,822,515	299,871,722	27.6%

- *Water distribution improvements* 1,094,019
- *N River Valley low head dam* 275,000
- *Other Water Utility CIP projects* 331,097
- Sewer Utility CIP projects (\$9,121,530):
 - *Sanitary sewer system improvements* 4,750,329
 - 848,380
 - 786,000
 - Bar screen improvements 744,907
 - 528,251
 - WPC Plant structural repairs 449,322
 - 1,014,341
 - Lift station improvements 2,071,660
 - 1,331,943
 - WPC mechanical/HVAC systems 1,690,887
 - 351,032
 - WPC Plant road repairs
 - Other WPC plant improvements
- Flood response/mitigation projects
- Teagarden area storm water improvements
- Other Storm Water Utility CIP projects
- Resource Recovery improvements

Transportation Program \$19,252,755

A total of \$49,200 is being carried forward in operating funding by the Public Works department. Of this amount, \$20,000 is allocated for downtown streetscape rehabilitation, with the remaining \$29,200 for delayed equipment purchases and miscellaneous projects.

Transportation CIP funding carryovers total \$19,203,555 and consist of the following programs and projects:

- Street construction projects (\$8,177,684):
 - *Asphalt street pavement improvements* \$2,052,971
 - 1,127,713
 - 1,084,072
 - 13th Street improvements 960,079
 - 947,135

- Concrete pavement improvements 849,773
- Seal coat pavement improvements 1,155,941
- Downtown street improvements 1,244,186
- ISU Research Park 2,083,181
- Other street improvement projects 1,926,259
- 3,770,850
- 2,001,395
- Shared use path projects
- Traffic engineering projects
- Street maintenance projects
- Transit system improvements
- Airport improvements

Community Enrichment Program \$1,960,539

Community Enrichment operating expenses of \$265,144 are being carried forward. Of this amount, \$104,207 is for Parks and Recreation projects and equipment, primarily the Emerald Ash Borer program (\$43,207) and a study of the City’s park system for ADA compliance (\$26,000). The Ames Public Library is also carrying over \$160,937 in unspent grant and donation funds, primarily for the Small Talk program and library building improvements.

A total of \$1,695,395 in funding is being carried over for the following Community Enrichment CIP projects:

- Parks and Recreation CIP projects (\$1,320,525):
 - *Park facility improvements* 275,390
 - *Inis Grove Park improvements* 275,000
 - *Municipal pool improvements* 138,930
 - *Furman Aquatic Center* 133,545
 - *Playground and park equipment* 95,395
 - *Bandshell improvements* 83,157

○ Brookside Park improvements	75,000
○ Sand volleyball complex	62,677
○ Other park and facility improvements	181,431
● Cemetery improvements	187,005
● Downtown Façade program	88,423
● Campustown Façade program	50,600
● Miscellaneous Community Enrichment projects	48,842

General Government Program \$2,503,548

Operating expenses of \$671,414 are being carried forward in the General Government program. \$317,313 of this amount is funding to allow the Planning department to hire outside professional assistance for projects such as the Land Use Policy Plan update. Planning is also carrying forward \$18,000 that is earmarked for their participation in the new software being purchased by the Inspections division. The Public Relations program is carrying forward \$153,293 in remaining funding earmarked for brand marketing, and, in City Council funding, \$46,389 of unspent contingency funds are being carried forward. The remaining balance of \$136,419 is for software updates and equipment for multiple departments, as well as funding for smaller City Hall facility projects, including \$40,000 for a video security system.

The General Government CIP carryover of \$1,832,134 is for the following projects:

● City Hall parking lot	\$1,053,596
● City Hall roof replacement	579,541
● City Hall improvements	198,997

Internal Services: \$2,413,406

Internal Services has \$2,272,964 in operating carryovers consisting of the following:

● Fleet equipment purchases	\$1,427,295
● Phone system upgrade	438,279
● Information Technology equipment	306,855
● Inspections software upgrade	100,535

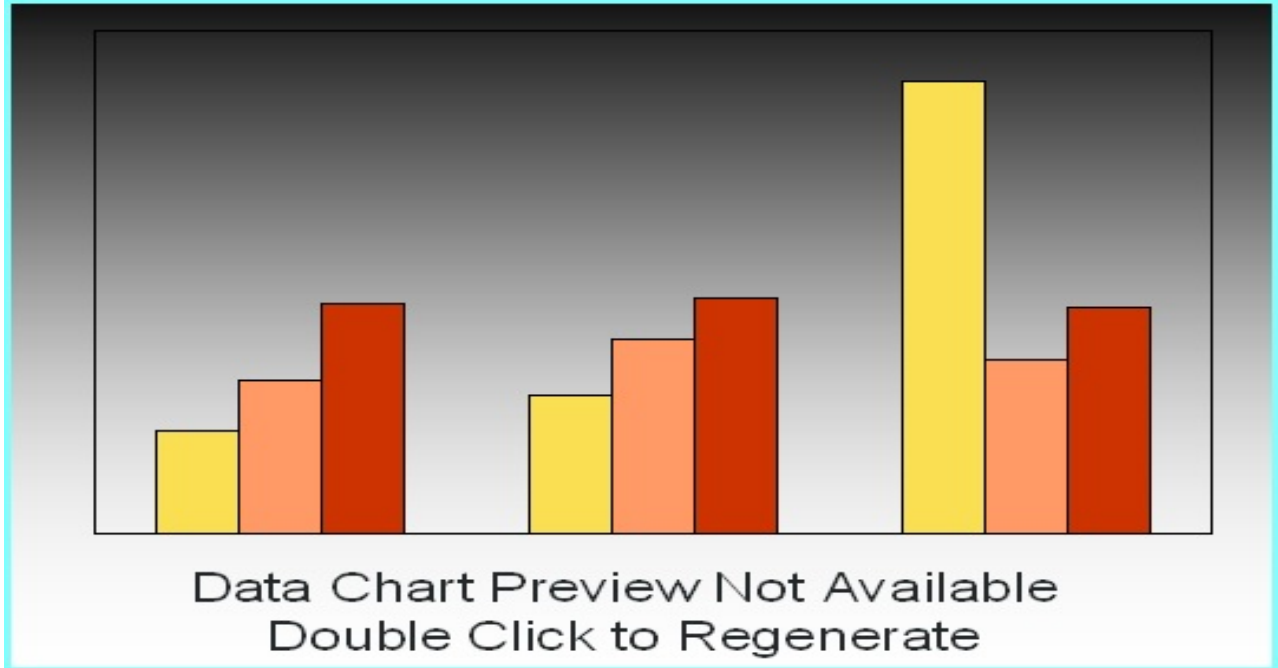
There is also an Internal Services CIP carryover of \$140,442 for improvements at the Fleet Maintenance facility.

Total Carryovers \$64,822,515

2015/16 CARRYOVERS BY FUND

	2016/17 Adopted	2015/16 Carryover	2016/17 Adjusted	% Change From Adopted
<i>General Fund</i>	34,542,430	1,986,970	36,529,400	5.8%
<i>Special Revenue Funds:</i>				
Local Option Sales Tax	8,432,876	2,667,189	11,100,065	31.6%
Hotel/Motel Tax	1,908,471	250,000	2,158,471	13.1%
Road Use Tax	8,000,651	2,192,338	10,192,989	27.4%
Other Special Revenue Funds	3,281,655	275,864	3,557,519	8.4%
<i>Total Special Revenue Funds</i>	21,623,653	5,385,391	27,009,044	24.9%
<i>Capital Project Funds</i>	9,706,264	13,629,944	23,336,208	140.4%
<i>Permanent Funds:</i>				
Cemetery Perpetual Care	-	-	-	
Furman Aquatic Center	-	-	-	
<i>Total Permanent Funds</i>	-	-	-	
<i>Enterprise Funds:</i>				
Water Utility	32,842,899	16,899,652	49,742,551	51.5%
Sewer Utility	17,987,019	9,167,410	27,154,429	51.0%
Electric Utility	66,227,620	7,224,191	73,451,811	10.9%
Parking	959,073	-	959,073	0.0%
Transit	13,979,076	3,770,850	17,749,926	27.0%
Storm Water Utility	2,820,219	3,919,287	6,739,506	139.0%
Ames/ISU Ice Arena	566,040	74,382	640,422	13.1%
Homewood Golf Course	262,053	-	262,053	0.0%
Resource Recovery	4,995,086	351,032	5,346,118	7.0%
<i>Total Enterprise Funds</i>	140,639,085	41,406,804	182,045,889	29.4%
<i>Debt Service Fund</i>	11,366,737	-	11,366,737	0.0%
<i>Internal Services:</i>				
Fleet Services	3,308,256	1,567,737	4,875,993	47.4%
Information Technology	2,471,585	845,669	3,317,254	34.2%
Risk Management	2,403,472	-	2,403,472	0.0%
Health Insurance	8,987,725	-	8,987,725	0.0%
<i>Total Internal Services</i>	17,171,038	2,413,406	19,584,444	14.1%
<i>Total Expenditures</i>	235,049,207	64,822,515	299,871,722	27.6%

2015/16 CARRYOVERS BY FUND, continued



**NOTICE OF PUBLIC HEARING
AMENDMENT OF FY2016-2017 CITY BUDGET**

Form 653.C1

The City Council of Ames in STORY County, Iowa
will meet at City Hall, 515 Clark Avenue, Ames, IA
at 6:00 PM on 02/27/2016
(hour) (Date)

for the purpose of amending the current budget of the city for the fiscal year ending June 30, 2017
(year)

by changing estimates of revenue and expenditure appropriations in the following programs for the reasons given. Additional detail is available at the city clerk's office showing revenues and expenditures by fund type and by activity.

		Total Budget as certified or last amended	Current Amendment	Total Budget after Current Amendment
Revenues & Other Financing Sources				
Taxes Levied on Property	1	26,989,155	0	26,989,155
Less: Uncollected Property Taxes-Levy Year	2	0	0	0
Net Current Property Taxes	3	26,989,155	0	26,989,155
Delinquent Property Taxes	4	0	0	0
TIF Revenues	5	245,892	0	245,892
Other City Taxes	6	9,913,236	0	9,913,236
Licenses & Permits	7	1,637,422	0	1,637,422
Use of Money and Property	8	9,732,777	0	9,732,777
Intergovernmental	9	23,556,609	0	23,556,609
Charges for Services	10	272,389,265	0	272,389,265
Special Assessments	11	457,082	0	457,082
Miscellaneous	12	1,904,474	0	1,904,474
Other Financing Sources	13	27,981,500	0	27,981,500
Transfers In	14	15,633,343	0	15,633,343
Total Revenues and Other Sources	15	390,440,755	0	390,440,755
Expenditures & Other Financing Uses				
Public Safety	16	17,818,239	121,298	17,939,537
Public Works	17	6,467,085	-69,043	6,398,042
Health and Social Services	18	1,302,478	0	1,302,478
Culture and Recreation	19	8,186,851	240,144	8,426,995
Community and Economic Development	20	3,379,096	335,313	3,714,409
General Government	21	2,530,591	454,344	2,984,935
Debt Service	22	11,366,737	0	11,366,737
Capital Projects	23	13,791,892	19,920,249	33,711,931
Total Government Activities Expenditures	24	64,842,508	21,002,305	85,844,813
Business Type / Enterprises	25	293,358,253	-43,820,210	337,178,463
Total Gov Activities & Business Expenditures	26	358,200,812	64,822,515	423,023,327
Transfers Out	27	15,633,343	0	15,633,343
Total Expenditures/Transfers Out	28	373,834,155	64,822,515	438,656,670
Excess Revenues & Other Sources Over (Under) Expenditures/Transfers Out for Fiscal Year	29	16,606,600	-64,822,515	-48,215,915
Beginning Fund Balance July 1	30	626,936,840	-1,034,948	625,901,892
Ending Fund Balance June 30	31	643,143,440	-65,857,463	577,285,977

Explanation of increases or decreases in revenue estimates, appropriations, or available cash:

This is the Council-approved amendment per the City Manager's recommendation.

There will be no increase in tax levies to be paid in the current fiscal year named above. Any increase in expenditures set out above will be met from the increased non-property tax revenues and cash balances not budgeted or considered in this current budget. This will provide for a balanced budget.


Diane R. Voss
City Clerk/Finance Officer